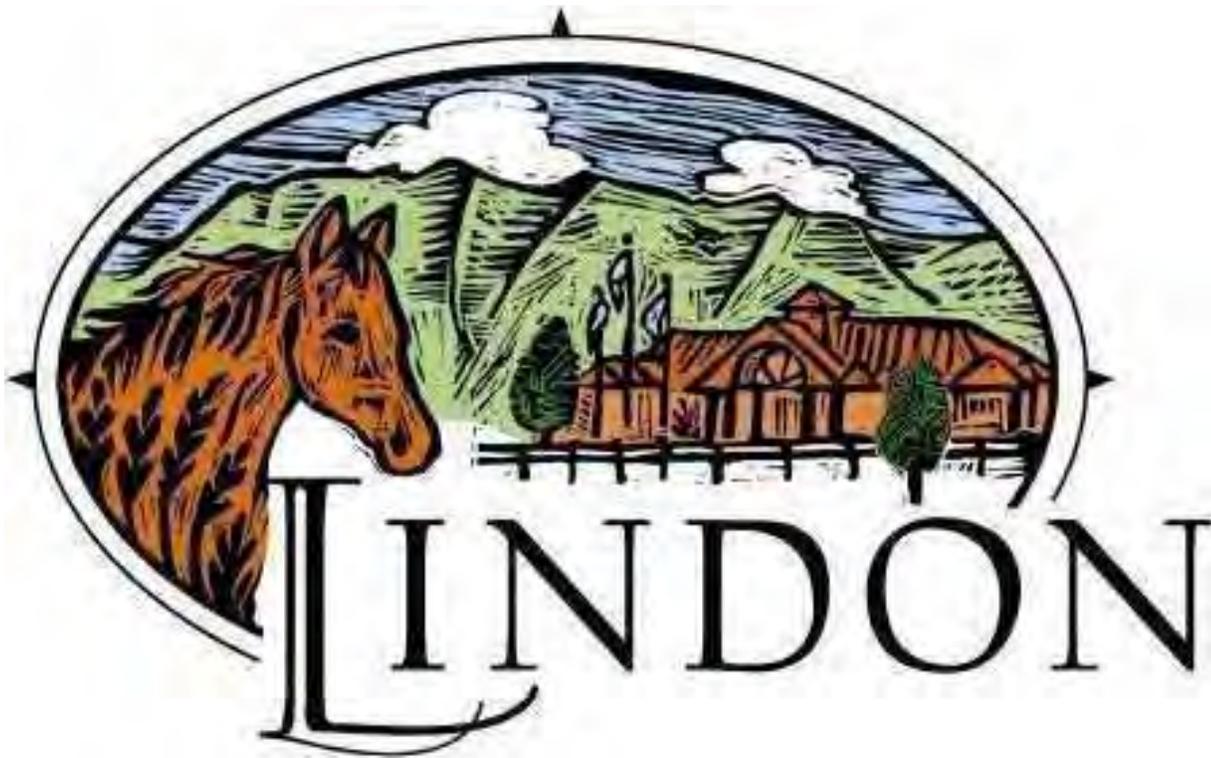


Lindon City Planning Commission Staff Report



February 25th, 2020

Notice of Meeting

Lindon City Planning Commission



The Lindon City Planning Commission will hold a regularly scheduled meeting on Tuesday, February 25, 2020, in the Council Room of Lindon City Hall, 100 North State Street, Lindon, Utah. The meeting will begin at 7:00 p.m. This meeting may be held electronically to allow a commissioner to participate by video or teleconference. The agenda will consist of the following items:

Agenda

Invocation: By Invitation

Pledge of Allegiance: By Invitation



Scan or click here for link to download agenda & staff report materials.

1. Call to Order
2. Approval of minutes
Planning Commission 2/11/2020
3. Public Comment
4. Continued public meeting for a Planning Commission recommendation to the Lindon City Council to amend Lindon City Code 17.47.040 to increase the allowable height in the Research and Business zone to four stories and sixty feet. Application is made by Mecca Holdings, LLC. *(20 minutes)*
5. Parks, Trails, and Recreation Master Plan Presentation *(20 minutes)*
6. Presentation and discussion on industrial building design standards *(20 minutes)*
7. New Business from Commissioners
8. Planning Director Report
- General City updates

Adjourn

Staff Reports and application materials for the agenda items above are available for review at the Lindon City Planning Department, located at 100 N. State Street, Lindon, UT. For specific questions on agenda items our Staff may be contacted directly at (801) 785-7687. City Codes and ordinances are available on the City web site found at www.lindoncity.org. The City of Lindon, in compliance with the Americans with Disabilities Act, provides accommodations and auxiliary communicative aids and services for all those citizens in need of assistance. Persons requesting these accommodations for City-sponsored public meetings, services programs or events should call Kathy Moosman at 785-5043, giving at least 24 hours notice.

The above notice/agenda was posted in three public places within Lindon City limits and on the State <http://www.utah.gov/pmn/index.html> and City www.lindoncity.org websites.

**The duration of each agenda item is approximate only*

Posted By: Kathryn Moosman, City Recorder

Date: 2/21/2020

Time: 5:00 pm

Place: Lindon City Center, Lindon Police Station, Lindon Community Center

Lindon City
100 North State Street
Lindon, UT 84042-1808



TEL 801-785-7687
FAX 801-785-4510
www.lindoncity.org

Item 1- Call to Order

Sharon Call
Mike Marchbanks
Rob Kallas
Steve Johnson
Scott Thompson
Jared Schauers
Renee Tribe

2 The Lindon City Planning Commission held a regularly scheduled meeting on **Tuesday,**
4 **February 11, 2020 beginning at 7:00 p.m.** at the Lindon City Center, City Council
Chambers, 100 North State Street, Lindon, Utah.

6 **REGULAR SESSION – 7:05 P.M.**

8 Conducting: Sharon Call, Chairperson
Invocation: Jared Schauers, Commissioner
10 Pledge of Allegiance: Scott Thompson, Commissioner

12 **PRESENT** **EXCUSED**

Sharon Call, Chairperson
14 Mike Marchbanks, Commissioner
Rob Kallas, Commissioner
16 Steven Johnson, Commissioner
Scott Thompson, Commissioner
18 Jared Schauers, Commissioner
Renee Tribe, Commissioner
20 Mike Florence, Planning Director
Anders Bake, Associate Planner
22 Kathryn Moosman, City Recorder

24 **Special Attendee:**
Councilmember Mike Vanchiere

26
28 1. **CALL TO ORDER** – The meeting was called to order at 7:00 p.m.

30 *At this time Chairperson Call made note that the zone change for property
located at 310 South 400 West has been pulled from the agenda.*

32 2. **APPROVAL OF MINUTES** –The minutes of the regular meeting of the
34 Planning Commission meeting of January 28, 2020 were reviewed.

36 COMMISSIONER MARCHBANKS MOVED TO APPROVE THE MINUTES
OF THE REGULAR MEETING OF JANUARY 28, 2020 AS PRESENTED.
38 COMMISSIONER THOMPSON SECONDED THE MOTION. ALL PRESENT
VOTED IN FAVOR. THE MOTION CARRIED.

40 3. **PUBLIC COMMENT** – Chairperson Call called for comments from any
42 audience member who wished to address any issue not listed as an agenda item.
There were no public comments.

44 **CURRENT BUSINESS** –

46 4. **Conditional Use Permit approval for Radical Classics – 122 N. 1800 W. Unit
#8.** Bret Madsen on behalf of Radical Classics is requesting conditional use

2 permit approval to operate an auto repair business located at 122 N. 1800 W Unit
#8.

4

Anders Bake, Associate Planner, led this agenda item by giving an overview
6 stating the applicant, Bret Madsen, who is in attendance, is requesting a conditional use
permit approval to operate an auto repair business at 122 North 1800 West, Unit #8. The
8 proposal requires a conditional use permit in the Light Industrial zone. The proposed
business, Radical Classics, will focus on modifying classic vehicles and will perform all
10 work indoors and within their unit. Notices were mailed on January 31, 2020 to
adjoining property owners in accordance with Lindon City Code and Staff has received
12 no public comments at this time.

Mr. Bake explained Radical Classics will be using a commercial unit in the
14 Lindon West Office/Warehouse Condominiums Subdivision. This subdivision was
recorded on November 29th 2004. The Condominium units were constructed in 1974.
16 The building and site comply with Lindon City Code requirements. The applicant is not
planning to do any construction on the building and will not be required to make any
18 changes to the building or site.

Mr. Bake further explained there are four reserved parking stalls available for Mr.
20 Madsen's business as well as additional parking spaces available to all customers of the
condominium subdivision. Outdoor vehicle storage is also prohibited as part of the
22 condominium's requirements. The business has two owners and no employees which will
allow the business to meet its parking needs. Mr. Bake noted based on the business
24 description that was provided, it is expected that this business will have a minimal impact
on surrounding properties and will be compatible with other uses in the Light Industrial
26 Zone.

Mr. Bake then presented the business description, building street view, aerial
28 photo of the site and surrounding area and building measurements followed by
discussion. He then turned the time over to the applicant for comment.

Mr. Madsen explained Radical Classics is a business that specifically deals with
30 the modifying aspects of classic vehicles. The work includes electrical, suspension,
brakes, drivetrain upgrades, taking a classic car and making it more reliable and fun car
32 to drive. Generally, the hours will be 8am to 5pm Monday through Friday. There will be
times in which we will be working earlier or later depending on deadlines for vehicles.
34 There are no employees, just two owners and all work will be done inside the warehouse.
The facility in which the shop is located does not allow any vehicles being worked on
36 outside. Parking will be only a few spots that are needed; the unit is under an HOA.

Mr. Madsen further explained Radical Classics has been in operation for a couple
38 years in Pleasant Grove, Utah and is currently being operated out of one of the owners'
personal shops at his house. There will be some noise associated with the daily
40 operations, but general normal conversation at normal speaking levels will be able to be
held at all times. He noted they have already talked with one of the neighbors and
42 introduced themselves and let them know what we will be doing and they have no issues
with our business.
44

Mr. Madsen noted they generally have a vehicle brought in and it will stay with
46 them for a little while and then they give it back to the owner and bring in the next
vehicle. They don't store the vehicles that are in line at the shop, they go and pick them
48 up at the owner's house when they are ready. The only water that will be used will be to

2 clean the floors at times and wash vehicles. He noted there will not be a big need or a
4 high usage of water and sewer capacity will not be an issue.

4 Following some general discussion, Chairperson Call stated this appears to be
pretty straightforward.

8 Chairperson Call called for any further comments or discussion from the
Commission. Hearing none she called for a motion.

10 COMMISSIONER KALLAS MOVED TO APPROVE THE APPLICANT'S
12 REQUEST FOR A CONDITIONAL USE PERMIT TO USE THE PROPERTY
LOCATED AT 122 NORTH 1800 WEST UNIT #8 FOR AN AUTO REPAIR
14 BUSINESS, WITH THE FOLLOWING CONDITIONS: 1. VEHICLES MAY NOT BE
STORED OUTDOORS FOR LONGER THAN 72 HOURS UNLESS THE OWNER IS
16 WAITING ON DELIVERY OF PARTS; 2. NO AUTOMOBILE PARTS WILL BE
STORED OUTSIDE OF THE BUILDING; 3. THE APPLICANT WILL COMPLY
18 WITH THE PROPOSED WASTE MANAGEMENT PLAN; 4. ALL ITEMS OF THE
STAFF REPORT. COMMISSIONER TRIBE SECONDED THE MOTION. THE
VOTE WAS RECORDED AS FOLLOWS:

20 CHAIRPERSON CALL	AYE
COMMISSIONER KALLAS	AYE
22 COMMISSIONER MARCHBANKS	AYE
COMMISSIONER JOHNSON	AYE
24 COMMISSIONER THOMPSON	AYE
COMMISSIONER SCHAUERS	AYE
26 COMMISSIONER TRIBE	AYE

THE MOTION CARRIED UNANIMOUSLY.

28

5. **Public Hearing** - Planning Commission recommendation to the Lindon City
30 Council to amend Lindon City Code 17.47.040 to increase the allowable height in
the Research and Business zone to four stories and sixty feet. Application is made
32 by Mecca Holdings, LLC.

34 COMMISSIONER JOHNSON MOVED TO OPEN THE PUBLIC HEARING.
COMMISSIONER MARCHBANKS SECONDED THE MOTION. ALL PRESENT
36 VOTED IN FAVOR. THE MOTION CARRIED.

38 Mike Florence, Planning Director, gave an overview of this item stating the
applicant, Mecca Holdings, LLC is proposing a new office building on the vacant lots at
40 345 South 800 West and 338 South 670 West. The applicant is petitioning the planning
commission for a recommendation to the city council to increase the allowable height in
42 the Research and Business zone from 48 feet and three stories to 60 feet and four stories.
He noted staff sent notices to all residential property owners who abut the property where
44 the applicant is proposing the office building.

46 Mr. Florence explained the purpose of the request is the applicant has a current
tenant, Bamboo HR, which leases another building in the Canopy Business Park and will
be expanding their operations and bringing on additional employees. He noted the
48 applicant would like to construct just one building instead of two smaller buildings to
accommodate the tenant. He noted the property is currently being used as a community

2 garden. Currently, the Research & Business zone has a combination of 2 and 3 story
buildings. This is the last remaining property to be developed in the Research and
4 Business zone. The applicant will be adding a large open space area for the use of the
tenant's employees that is adjacent to a number of single-family homes.

6 Mr. Florence commented when reviewing ordinance changes the planning
commission should also consider the goals of the general plan in maintaining and
8 enhancing the appearance and environmental quality of existing residential
neighborhoods by avoiding encroachment of land uses which would adversely impact
10 residential areas (i.e. increased traffic, noise, visual disharmony, etc.) and by providing
adequate screening and buffering of any adjacent commercial or industrial development
12 including parking and service areas. He noted it would also encourage development of
commercial facilities, such as hotels, restaurants and vehicle-related services at
14 transportation interchanges and carefully limit any negative impact of commercial
facilities on neighboring land use areas, particularly residential development.

16 Mr. Florence indicated the relationship of planned land uses should also reflect
consideration of existing development, environmental conditions, service and
18 transportation needs, and fiscal impacts. Developed areas should be protected and
revitalized by promoting new development and the adaptive reuse of existing community
20 resources and the transitions between different land uses and intensities should be made
gradually with compatible uses, particularly where natural or man-made buffers are not
22 available. The growth should be guided to locations contiguous to existing development
to provide city services and transportation in a cost-effective and efficient manner.
24 Commercial and industrial uses should be highly accessible, and developed compatibly
with the uses and character of surrounding districts.

26 Mr. Florence pointed out the adjacent Canopy Business Park has been a very
successful office development for Lindon City and the businesses there employ a
28 significant number of employees. Staff feels that one of the main items that the planning
commission needs to consider tonight is if the increase in height to 60' (4 stories) rather
30 than 48' (3 stories) will have a bigger impact on the adjacent residential properties. He
noted the Research and Business zone currently requires a 30' setback from all property
32 lines. The applicant's site plan shows the building setback approximately 65' from the
closest corner of the building to the residential zone and 183' from the furthest corner.
34 The Research and Business zone also requires 15' of landscaping on the commercial side
of the masonry wall when abutting residential. He pointed out there are three existing
36 buildings in the Canopy Business Park that are adjacent to residential uses. Those
buildings are two- and three-story buildings and have a setback range of approximately
38 57' to 92' feet from the residential zone. Mr. Florence indicated that staff has identified a
number of options for the planning commission to consider for this application:

- 40 • Move the proposed recommendation forward to the City Council. However, staff
recommends adding language which would require greater setbacks for a four-
42 story building. For example, a four-story building would have a minimum setback
for 60' instead of 30' which is required for a three-story building;
- 44 • Move the proposed recommendation forward to the City Council but require the
applicant to relocate the building on the site and recommend specific setback
46 requirements. For example, the building is proposed to be on an angle for better
views of Mount Timpanogos. The building could be moved so that it sits square
48 on the site and then adopt a larger setback requirements.

- 2 • Recommend denial of the application due to neighborhood impacts of increasing the height.

4 **Mr. Florence then referenced the Proposed Ordinance Amendment as follows:**

17.47.040 (4)

6 Building height. No building shall be constructed to a height exceeding three (3) four (4) stories with a maximum height of forty-eight (48) sixty (60) feet.

8 **Mr. Florence also presented the following Exhibits for discussion:**

- Zoning map of Research and Business zone area
- 10 • Picture of the existing site
- Aerial photo
- 12 • Aerial Photo showing proximity of other building in the business park to residential
- 14 • Existing buffering between office uses and residential
- Site plan
- 16 • Renderings
- Proposed buffering illustration
- 18 • Letter from the applicant
- Proposed ordinance language
- 20 • Zoning Map

22 The applicant, Mr. Willie Blocker addressed the Commission at this time. He explained he has been on this site for 20 years. He gave a brief history noting this
24 portion was going to be a large retail center in 1995, then Home Depot came in as the anchor tenant and opened for business in 1998, but that developer was unsuccessful in
26 making that a retail center and it didn't materialize. In 1999 the City created the R & B zone with residential up above. Mr. Blocker stated they came in 2000 and started
28 building Canopy Building #1 and acquired 40 acres and they have developed 20 acres of it to date. He noted they have had the opportunity to meet with some of the residents
30 either by phone and in person and he understands he is viewed as the "big bad developer" that wants to destroy Lindon and the peace and the views that they have and
32 that is not his intent. In the end he understands the citizens are taking the brunt of this proposal and they feel the city and developer are taking advantage of them.

34 Mr. Blocker stated he was asked if he would want one of these buildings in his back yard and his answer was "no" he would not want that in his backyard. The
36 residents have their rights as a homeowner but they also have their rights as a property owner too. They have looked at what they can do to mitigate the exposure to the
38 residents. Under the current code he can build the whole area into 3-story buildings and he can elongate that out and accommodate what their needs are or compress it into a
40 smaller footprint to impact fewer residents (he showed his current design). He noted this is not set in stone and can be modified. This will be contingent on what the commission
42 and council decides. A larger footprint impacts more residents or something more vertical that will impact half as many. All of these options will affect property values
44 and he hopes what they are attempting to do will mitigate some of these issues.

46 The architect addressed the commission at this time. He noted they are putting the building back to 72 ft away (more than double) and with the landscaping and fencing the building will virtually disappear and not nearly as intrusive. This is a good solution

2 and will have the least impact on the fewest people and will mitigate some of the
concerns. Mr. Blocker pointed out this will be a 100% solar energy building.

4 He also discussed with the commission the parking, required setbacks and re-
positioning and size of the proposed building and the circulation and campus needed.

6 Chairperson Call called for any comments from the public at this time. There
were several residents in attendance that addressed the Commission as follows:

8

Shawna Powell: Ms. Powell stated the next building from the proposed building is right
10 behind her home. Even with the current 3-story buildings and with trees there she can
see the people that are working in those buildings. This one is closer and if she can
12 visibly see in their building then they can see in her yard (day and night). Being a
homeowner in Lindon they have been told different things by the city over the years and
14 they are here trying to protect their properties. She pointed out this does not affect rental
properties as it does those who are homeowners that will devalue their property.

16

Ryan Allen: Mr. Allen would ask the developer to look at putting the building to the
18 east and west and implored the Commission to look at the setbacks and to allow that to
happen. He does not want to stare at a 4-story building and know that people are
20 looking into his yard and home. This will prohibit him from having private get-togethers
with his family and friends as there will be people there around the clock and this is
22 concerning. He has lived in Lindon for 12 years. This is NOT building a better
community. This is allowing the developer to pad his pockets at the expense of the
24 residents and homeowners and this would be a huge mistake. They already have light
and noise pollution with the 3-story buildings currently there and there will be more if
26 this building is built. He is imploring the Commission that this is wrong to allow this.
They need to move the building and address the setbacks if they are going ahead with it.
28 He is a homeowner and the value of his home will be affected the minute this building is
built. He acknowledges there could be a lot of value in meeting with the developer and
30 readdressing the east/west options and the setbacks. Mr. Allen pointed out we are talking
going from 48 ft. to 60 ft. so there is a huge difference. In regards to a 7 ft. masonry
32 fence; what is a 7 ft. fence or trees to a 60 ft. building; it leaves a large gaping hole.

34

Luke Gillman: Mr. Gillman stated he has lived in Lindon for 13 years. He noted there
are some SID (special improvement district) taxes that have been paid in past years. He
36 expressed his concerns of paying more to have something like this in our back yard.

38

Ted Lott: Mr. Lott told Mr. Gillman the Canopy Group came in and developed and that
is what the SID (special improvement district) is for. He noted the Hooley property was
40 sold to the Canopy Group also. He mentioned the item pulled from the agenda today
was his property. He pointed out if they bring it up again to change the zoning this will
42 affect his property as to be a buffer zone.

44

James Beadle: Mr. Beadle expressed his concerns with raising this building to the four
floors if it would be for all buildings. Mr. Beadle also commented that he would like
46 to see it on the west side.

2 **Jason Carling:** Mr. Carling stated this building would be right in his back yard. He also
4 agrees with what Mr. Allen said about the complete invasion of privacy and the fact that
it would lessen their home values and agrees it should be moved if possible.

6 **Erlene Lott:** Ms. Lott commented if this is moved to the west are they still talking
8 about four stories as they don't want something with four stories to come in behind
them. She is concerned about having four stories behind them as it would open up a can
of worms. Lindon is still a little bit of country and that's why they are here.

10
12 **Jamie Gillman:** Ms. Gillman stated she likes the idea of moving it to the west. She also
questioned what the likelihood is of the setbacks changing and how far that would be.
14 She pointed out that when she is in her back yard with her kids the current building
occupants can look right in her back yard and that is a three-story building.

16 **Meg Gillman:** Ms. Gillman stated she lives at the end of the property too and has lived
18 in Lindon for 13 years and it is her hope to live here the rest of her life. She pointed out
that all the neighbors moved to Lindon for the large lots and open space and if this 4-
20 story building goes up it will affect her property value.

22 **Monica Beadle:** Ms. Beadle commented that she lives a little further east. She stated
they can hear the air conditioning and heating all day long on the current buildings.
24 There is also light pollution 24/7 and if this bigger building goes in all these issues will
increase. She pointed out that the trees and fencing don't do anything to mediate these
26 issues and with a 4-story building it will all be more visible.

28 Chairperson Call called for any further public comments. Hearing none she called
for a motion to close the public hearing.

30 COMMISSIONER JOHNSON MOVED TO CLOSE THE PUBLIC HEARING.
32 COMMISSIONER SCHAUERS SECONDED THE MOTION. ALL PRESENT VOTED
IN FAVOR. THE MOTION CARRIED.

34 Commissioner Kallas stated he would like to see more analysis on building
36 location. He would suggest the developer and his architect meet with staff and the city
engineer to re-position the building to have less impact on the residents; he is leaning
38 towards continuation as to have more time for additional research. However, it appears
the developer is trying to mitigate some of the issues.

40 Commissioner Marchbanks stated the architect and owner have done a lot to
mitigate some of the impacts and circumstances, but he would like them to explore the
42 subterranean walkout (one story underground) so it is still a 48 ft. skyline; there is a way
there could be a win win for both developer and homeowner.

44 Chairperson Call stated one of the letters received from a resident talked about
how high the mechanical rooms are on top of the buildings. She asked the developer if
46 they can somehow change that or look at it so those are not so high on the buildings. She
feels we need to be careful with the residential areas and would suggest to the developer
48 to have a conversation with the residents to help mitigate some of these concerns as to
limit the negative impacts.

2 Commissioner Thompson commented it sounds like there is a consensus that the
4 developer is willing to work through some things to mitigate the issues and concerns of
the residents.

6 At this time Mr. Blocker asked what the Commission is looking for as they are
trying to be sensitive to the concerns of the residents. The Commission stated to let the
8 architect go to work to mitigate the issues more than what they already have and then
they may be open to changing the setbacks.

10 Commissioner Kallas would suggest the developer and architect meet with city
staff and engineer to think outside the box and then have a neighborhood meeting. The
12 existing cul-de-sac could be shortened that would then allow them to tweak the building a
little bit, and perhaps use a subterranean first floor. These are some things to look at and
14 discuss and then get the neighbors input, but the final decision will go to the city council
for approval.

16 Commissioner Marchbanks believes the consensus of the Planning Commission is
they would be a lot more inclined to not change the overall height as that can open doors
18 that may not be feasible to think about; he would suggest to explore going down rather
than up.

20 Commissioner Thompson commented we can't make everyone happy, but would
suggest the developer go out and explore some more options and then come back to the
22 Commission after it is vetted with the neighbors and work out some ideas and do his due
diligence.

24 Commissioner Johnson stated he appreciates the applicant's willingness to be
thoughtful and concerned of this and hopes the residents realize that. He pointed out the
Commission is bound by the ordinance and the developer is willing to work on this but
26 unfortunately property values are not a deciding factor. He does have concerns that if
this is re-zoned the whole zone is re-zoned and that needs to be considered and what it
28 creates further down the road. He would prefer the four stories but to set it further back.

30 Mr. Florence mentioned he will not be sending out another public notice before
the next meeting. Chairperson Call called for any further comments or discussion from
32 the Commission. Hearing none she called for a motion.

34 COMMISSIONER KALLAS MOVED TO RECOMMEND CONTINUANCE
OF ORDINANCE AMENDMENT 2020-3-O TO ALLOW THE DEVELOPER AND
STAFF AN OPPORTUNITY TO CONSIDER ALL OTHER POTENTIAL OPTIONS
36 TO INCLUDE SUBTERRANIAN, RELOCATION OF THE BUILDING,
REORIENTATION AND SHORTENING OF THE CUL-DE-SAC ROAD, ETC.
38 COMMISSIONER MARCHBANKS SECONDED THE MOTION. THE VOTE WAS
RECORDED AS FOLLOWS:

40 CHAIRPERSON CALL	AYE
COMMISSIONER KALLAS	AYE
42 COMMISSIONER MARCHBANKS	AYE
COMMISSIONER JOHNSON	AYE
44 COMMISSIONER THOMPSON	AYE
COMMISSIONER SCHAUERS	AYE
46 COMMISSIONER TRIBE	AYE

THE MOTION CARRIED UNANIMOUSLY.

2 **6. MS Properties Site Plan Approval – 430 N. 1200 W.** MS Industrial Properties,
3 LLC requests site plan approval for warehouse building located in the Light
4 Industrial zone. Parcel #'s 45:111:0009 and 45:111:0010.

6 Anders Bake, Associate Planner, opened this agenda item by giving an overview
7 stating for site plan approval, the planning commission will be evaluating whether the site
8 plan and building meet Title 17 development regulations. The subject property contains
9 an existing building and is used by MS properties for equipment storage. The proposed
10 project will include the construction of a larger building that will be attached to the back
11 end of the existing building. The building will mainly be used for equipment
12 maintenance.

13 Mr. Bake noted the proposed Site Plan also includes improvements to the
14 landscaping and parking to bring the site into compliance with current Light Industrial
15 requirements and covers two lots in the Lakeview Industrial Park subdivision. The
16 applicant has applied for a Plat Amendment to combine the two lots into one.

17 Mr. Bake explained the applicant representative, Buck Robinson, who is in
18 attendance, is proposing to use the parking standard for a manufacturing building and not
19 for an auto repair building. The reasoning for this is that the building will be used as a
20 fleet maintenance building and not open to the public. The auto repair parking standard is
21 one stall per 300 square feet of floor area excluding bay areas, plus five (5) stalls per
22 single bay/shop. Under the auto repair code, the building would require 79 parking
23 spaces. The proposed Site Plan identifies 13 striped parking stalls on concrete surfacing
24 and additional space on the property for up to 30 parking stalls.

25 Mr. Bake stated the parking code requires the site to have 23 parking stalls, based
26 on the floor area of the buildings. The parking stalls must be striped and surfaced with
27 asphalt, concrete or other binder pavement. City Code allows the city to consider
28 reducing the amount of required parking if they are provided evidence of similar uses that
29 accommodate their parking needs with fewer available spaces. The applicant was made
30 aware of this provision but has not provided the necessary documentation that would
31 allow for 10 less parking stalls than what is required.

32 Mr. Bake indicated the site provides adequate traffic circulation for customers and
33 trucks through the site. Vehicles will be able to enter and exit the site from two drive
34 approaches on 1200 West. City Staff is currently waiting for the applicant to provide
35 updated site and landscaping plans after the planning and engineering staff's first review
36 of these plans. Mainly, the applicant still needs to provide a landscape plan and update
37 the site plan to include interior landscaping. Staff made these conditions of approval of
38 the planning commission is comfortable with these items being approved at a staff level.

39 Mr. Bake stated the proposed building addition will have an exterior of painted
40 metal panel with plastered concrete the bottom six feet. The colors for the new building
41 will be earth tone. The East and West elevations of the new building will have numerous
42 metal panel roll-up doors to accommodate the fleet maintenance. The existing building
43 that will remain and has a metal exterior. The applicant will install stucco over the
44 existing metal panels to a height of six feet to match the plastered concrete portion of the
45 new building. Buildings in the Light Industrial zone are required to have 25% of the
46 exterior of all buildings covered with brick, decorative block, stucco, wood, or other
47 similar material. The Light Industrial Architectural Design requirements provide options
48 for the Planning Commission to consider other types of architecture.

2 Mr. Bake went on to say this provision may apply to the MS Properties site
because the applicant will be adding stucco to the existing building which will be the
4 most visible improvement from the street and match the design of the new building
behind it. Adding the stucco to the front elevation of the existing building appears to be
6 approximately 23% from staff’s measurements. The applicant is also adding stucco to the
north and south elevations of the existing building that the commission could consider.

8 Mr. Bake noted the minimum development size for the Light Industrial zone is
one acre. The lots contained in this Site Plan are an acre each but will be combined into
10 one lot through a Plat Amendment. The existing and new buildings meet the minimum
setback requirements for the light Industrial zone. The City Engineer is working through
12 technical issues related to the site plan and will conduct a final review if the planning
commission grants final site plan approval.

14 Mr. Bake commented the proposed site plan will bring improvements to this
property including additional landscaping and an organized parking lot. The proposed
16 architectural plans will improve the existing building and create a consistent architectural
pattern for the property. Staff will continue to work with the applicant and their engineers
18 to ensure that review comments are addressed and that the site meets city requirements.

20 Mr. Bake then presented the City Parking Code section, Aerial photo, Street view,
Site plan, building elevations, Colored building renderings, City Parking Code Section,
and the Comparative use reductions followed by discussion.

22 Mr. Bake pointed out a comparable reduction in the amount of required parking
may be considered if the City receives credible documentation of existing similar uses
24 that have fewer parking spaces which reasonably accommodates the similar use.
However, in order to accommodate the range of uses that may occur over time on the
26 property, any site which is given a reduction in the number of required parking spaces,
which reduction was allowed due to comparisons of other existing similar uses, shall
28 provide on the same or adjacent property (or through a shared parking agreement) the
area needed to accommodate the total amount of required parking spaces.

30 Mr. Bake indicated this “parking land-bank” shall be landscaped in a manner that
can reasonably be transitioned into future parking spaces without disruption to the uses or
32 circulation of traffic on the property. He noted if the ‘land-bank’ area is landscaped, said
landscaping shall not count towards other required perimeter or interior landscaping
34 requirements. Storm drainage and other engineering considerations on the site shall be
designed to accommodate the full number of required stalls. Following some general
36 discussion, the Commission was in agreement this will improve the area and all
requirements have been met.

38 Chairperson Call called for any further comments or discussion from the
Commission. Hearing none she called for a motion.

40
42 COMMISSIONER SCHAUERS MOVED TO APPROVE THE APPLICANT’S
REQUEST FOR SITE PLAN APPROVAL WITH THE FOLLOWING CONDITIONS:
44 1. THE APPLICANT WILL CONTINUE TO WORK WITH THE CITY ENGINEER
TO MAKE ALL FINAL CORRECTIONS TO THE ENGINEERING DOCUMENTS; 2.
46 THE PLANS WILL MEET DEVELOPMENT SPECIFICATIONS AS FOUND IN THE
LINDON CITY DEVELOPMENT MANUAL; 3. FINAL DESIGN WILL MEET THE
48 TITLE 17 STANDARDS FOR INDUSTRIAL BUILDINGS; 4. THE APPLICANT
WILL COMPLY WITH ALL BONDING REQUIREMENTS, IF APPLICABLE; 5. THE

2 PROJECT WILL MEET ALL GENERAL AND INTERIOR LANDSCAPING
REQUIREMENTS AS FOUND IN 17.49.060 AND 17.18.085;
4 6. A PLAT AMENDMENT TO COMBINE PARCELS 45:111:0010 AND 45:111:0009
WILL RECEIVE PLANNING COMMISSION APPROVAL AND BE RECORDED
6 WITH THE UTAH COUNTY RECORDER'S OFFICE; 7. ALL ITEMS OF THE
STAFF REPORT. COMMISSIONER MARCHBANKS SECONDED THE MOTION.
8 THE VOTE WAS RECORDED AS FOLLOWS:

CHAIRPERSON CALL	AYE
10 COMMISSIONER KALLAS	AYE
COMMISSIONER MARCHBANKS	AYE
12 COMMISSIONER JOHNSON	AYE
COMMISSIONER THOMPSON	AYE
14 COMMISSIONER SCHAUERS	AYE
COMMISSIONER TRIBE	AYE
16 THE MOTION CARRIED UNANIMOUSLY.	

18 7. **MS Properties Amended Plat – 430 N. 1200 W.** MS Industrial Properties, LLC
requests a plat amendment approval to consolidate two lots into one. Parcel #'s
20 45:111:0009 and 45:111:0010.

22 Mr. Bake gave an overview of this item stating the applicant, MS Industrial
Properties is petitioning to consolidate two existing parcels into one lot. These parcels are
24 currently lots nine and ten in the Lakeview Industrial Park subdivision. The applicant,
owns both parcels that will be amended as part of the application for one lot. The Plat
26 Amendment will allow the applicant to construct a new building on the lot. The proposed
plat amendment is located in the Light Industrial Zone (LI) zone and meets minimum lot
28 size and frontage requirements.

Mr. Bake stated Lindon City Code 17.32.00 references Utah Code for
30 requirements amending a subdivision plat. Under Utah Code 10-9a-608, an applicant may
petition the Land Use Authority (Planning Commission) to join two or more of the
32 petitioner fee owner's contiguous lots. He noted the City Engineer is working through
any technical issues related to the plat and will conduct a final review if the planning
34 commission approves the plat amendment. Following some general discussion, the
Commission was in agreement to approve this plat amendment request as presented.

36 Chairperson Call called for any further comments or discussion from the
Commission. Hearing none she called for a motion.

38
COMMISSIONER JOHNSON MOVED TO APPROVE THE APPLICANT'S
40 REQUEST FOR FINAL PLAT APPROVAL OF LAKEVIEW INDUSTRIAL PARK
PLAT "F" WITH THE FOLLOWING CONDITIONS: 1. PRIOR TO PLAT
42 RECORDING AND OCCUPANCY OF ANY NEW DEVELOPMENT WITHIN THIS
PLAT, THE APPLICANT MUST UPDATE THE FINAL PLAT MYLAR TO
44 INCLUDE NOTARIZED SIGNATURES OF OWNERS' CONSENT TO
DEDICATION CONSISTENT WITH ITEM ONE ABOVE; AND OBTAIN
46 SIGNATURES OF ALL ENTITIES INDICATED ON THE SUBDIVISION PLAT
ATTACHED HERETO; 2. ALL ITEMS OF THE STAFF REPORT. COMMISSIONER

2 THOMPSON SECONDED THE MOTION. THE VOTE WAS RECORDED AS
FOLLOWS:

4	CHAIRPERSON CALL	AYE
	COMMISSIONER KALLAS	AYE
6	COMMISSIONER MARCHBANKS	AYE
	COMMISSIONER JOHNSON	AYE
8	COMMISSIONER THOMPSON	AYE
	COMMISSIONER SCHAUERS	AYE
10	COMMISSIONER TRIBE	AYE

THE MOTION CARRIED UNANIMOUSLY.

12
14 8. **MS Properties Amended Site Plan – 1325 W. 500 N.** MS Industrial Properties,
LLC requests amended site plan approval for an outdoor construction storage
16 yard. Parcel # 14:063:0052.

18 Mr. Bake stated for amended site plan approval, the planning commission will be
evaluating whether the site plan and building meet Title 17 development regulations for
expanding a development. The purpose of the storage expansion is to allow the applicant
20 to more easily align the metal poles with the building to the east for manufacturing. The
existing building and site were approved by the Planning Commission in 2011 with the
22 following conditions:

- 24 1. That 10' tall evergreen trees be installed on the north and west property frontage
every 15' on center, excluding clear vision areas at drive entrances and on the
corner of the site as required by ordinance
- 26 2. That the 64 paved parking stalls shown on the site plan are adequate due to the
number of employees anticipated at the site and area to provide future expansion of
28 parking if necessary
- 30 3. That lighting be lowered, shielded and contained to the site on the north side of the
building
- 32 4. That the color of the accent stripe be changed from red to an earth tone
- 34 5. That windows on the north elevation be tinted
6. That the exception allowing all architectural treatments to be applied to the north
elevation is approved by the planning commission.

36 Mr. Bake stated the subject property contains two parcels with an existing
building on the North property that is used by MS properties for manufacturing. The
38 South portion of this property is used for outdoor storage. MS properties recently
purchased the south property and would like to expand their storage area into this
40 property. The project will include bringing in gravel surfacing and the installation of a
retaining wall on the South property line. The applicant will also be required to make
42 public improvements to Anderson Lane.

44 Mr. Bake noted the Amended Site Plan is considered a change in use and an
expansion of an existing development. This will require the applicant to install street
improvements to Anderson Lane along the South property. These improvements will
46 include expanding the pavement width of the road, extending the culinary water line, and
48 piping a ditch that runs along the East side of Anderson Lane. A section of right-of-way
for Anderson Lane will also need to be dedicated to Lindon City. These improvements

2 are not specified on the Amended Site Plan but the applicant has been made aware of
these requirements.

4 Mr. Bake indicated the lighting is not proposed to be extended into the proposed
storage yard. A twenty-foot-wide landscaping strip with trees planted every 30 feet is
6 required along public roads in the Light Industrial Zone. To reduce potential noise
issues, the planning commission in 2011 required that 10' evergreen trees be planted at
8 15' on center. Staff has recommended that this condition be continued through the site
with the conditional use permit. A landscaping strip and masonry fence was installed
10 along Anderson Lane on the North property when it was developed. The applicant will be
required to continue the landscaping strip and wall along Anderson Lane on the South
12 property as part of this Amended Site Plan.

Mr. Bake mentioned the Amended Site Plan includes gravel surfacing that will
14 cover the South property. A block retaining wall will be installed along the South
property line of the South property and an existing retaining wall on the north property
16 line will be removed. There are no new buildings proposed for this site or additional
changes to the North property. The minimum development size for the Light Industrial
18 zone is one acre. The lots contained in this Amended Site Plan are eight acres and five
acres. The existing building meets the minimum setback requirements for the light
20 Industrial zone.

Mr. Bake noted the City Engineer is working through technical issues related to
22 the site plan and will conduct a final review if the planning commission grants final site
plan approval. He mentioned the proposed Amended Site Plan will bring important
24 improvements to Anderson Lane that will link the existing improvements to the North
with future Improvements that Ivory homes will install to the South. The required
26 landscaping will also create a buffer between an industrial use and nearby residential
properties. It will also help visually connect the North and South properties and create a
28 consistent streetscape for Anderson Lane. He added staff will continue to work with the
applicant and their engineers to ensure that review comments are addressed and that the
30 site meets city requirements and can be worked through on a staff level.

Mr. Bake then presented the Aerial photo, Street view, Site plan, 2011 planning
32 commission meeting minutes and Aerial photo followed by discussion.

Chairperson Call called for any further comments or discussion from the
34 Commission. Hearing none she called for a motion.

36 COMMISSIONER THOMPSON MOVED TO APPROVE THE APPLICANT'S
REQUEST FOR AMENDED SITE PLAN APPROVAL WITH THE FOLLOWING
38 CONDITIONS: 1. THE APPLICANT WILL CONTINUE TO WORK WITH THE CITY
ENGINEER TO MAKE ALL FINAL CORRECTIONS TO THE ENGINEERING
40 DOCUMENTS; 2. THE PLANS WILL MEET DEVELOPMENT SPECIFICATIONS
AS FOUND IN THE LINDON CITY DEVELOPMENT MANUAL; 3. THE
42 APPLICANT WILL COMPLY WITH ALL BONDING REQUIREMENTS IF
NECESSARY; 4. ALL ITEMS OF THE STAFF REPORT. COMMISSIONER
44 MARCHBANKS SECONDED THE MOTION. THE VOTE WAS RECORDED AS
FOLLOWS:

46 CHAIRPERSON CALL	AYE
COMMISSIONER KALLAS	AYE
48 COMMISSIONER MARCHBANKS	AYE

2 COMMISSIONER JOHNSON AYE
COMMISSIONER THOMPSON AYE
4 COMMISSIONER SCHAUERS AYE
COMMISSIONER TRIBE AYE
6 THE MOTION CARRIED UNANIMOUSLY.

8 9. **MS Properties Conditional Use Permit – 1325 W. 500 N.** MS Industrial
Properties, LLC requests conditional use permit approval for an outdoor
10 construction storage yard. Parcel # 14:063:0052.

12 Mr. Bake explained MS Industrial Properties is requesting conditional use permit
approval to operate their business on the South property located at 1325 West 500 North.
14 The applicant plans to use the property for construction material storage which requires a
Conditional Use Permit in the Light Industrial Zone. He stated notices were mailed on
16 January 31, 2020 to adjoining property owners in accordance with Lindon City Code and
staff has received no public comments at this time. He noted the Special Provisions
18 section of Lindon City code, 17.49.090, will apply to this site. The ordinance outlines
practices which include items such as proper property maintenance, storage, fencing
20 height and excessive dust, odor, and noise.

Mr. Bake indicated the storage yard will have a gravel base and used for outdoor
22 storage of construction equipment. This will be an expansion of what is currently done on
the property to the North. The proposed use may increase truck traffic and noise in the
24 area. These impacts can be reasonably mitigated through the requirements outlined in the
Amended Site Plan for this site and Title 17.49.090. Amended Site Plan requirements
26 include widening the asphalt on Anderson Lane which will allow the road to handle
increased traffic from this business.

28 Mr. Bake stated the applicant will also be required to install a twenty-foot-wide
landscaping strip with trees every 15 feet along Anderson Lane and a 6' fence that will
30 create a noise and visual buffer from nearby residential properties. Mr. Bake stated every
site shall conform to the approved conditional use permit, site plan, or amended site plan.

32 Following some general discussion Chairperson Call called for any further
comments or discussion from the Commission. Hearing none she called for a motion.

34
COMMISSIONER TRIBE MOVED TO APPROVE THE APPLICANT'S
36 REQUEST FOR A CONDITIONAL USE PERMIT TO USE THE PROPERTY
LOCATED AT 1325 WEST 500 NORTH FOR CONSTRUCTION MATERIAL
38 STORAGE WITH THE FOLLOWING CONDITIONS: 1. THE APPLICANTS WILL
MAINTAIN THE PROPERTY AS APPROVED IN THE AMENDED SITE PLAN; 2.
40 THE APPLICANT WILL CONTINUALLY MEET THE SPECIAL PROVISIONS
REQUIREMENTS OF TITLE 17.49.090 FOR INDUSTRIAL PROPERTIES; 3. TEN
42 (10) FOOT TALL EVERGREEN TREES BE INSTALLED FIFTEEN (15) FEET ON
CENTER WITHIN THE TWENTY (20) FOOT LANDSCAPED AREA ALONG
44 ANDERSON LANE IN ORDER TO MITIGATE NOISE LEVELS AND FURTHER
SCREEN THE OUTDOOR STORAGE AREA; 4. ALL ITEMS OF THE STAFF
46 REPORT. COMMISSIONER SCHAUERS SECONDED THE MOTION. THE VOTE
WAS RECORDED AS FOLLOWS:

48 CHAIRPERSON CALL AYE

2 COMMISSIONER KALLAS AYE
COMMISSIONER MARCHBANKS AYE
4 COMMISSIONER JOHNSON AYE
COMMISSIONER THOMPSON AYE
6 COMMISSIONER SCHAUERS AYE
COMMISSIONER TRIBE AYE
8 THE MOTION CARRIED UNANIMOUSLY.

10 **10. New Business: Reports by Commissioners** – Chairperson Call called for any
new business or reports from the Commissioners.

12

Chairperson Call mentioned a follow-up on the Lindon Nursery and the
14 commercial property. She also mentioned the tour of the Walker Farms event center will
be held in a few weeks. Mr. Florence stated he will also be facilitating a tour of the new
16 doTerra warehouse in April. Commissioner Tribe asked if there has been a resolution on
the building on 4th North and State Street coming down as it is an eyesore and a safety
18 hazard. Councilmember Vanchiere said the Mayor is working on getting bids on
demolishing the building.

20 Chairperson Call called for any further comments or discussion from the
commission, hearing none she moved on to the next agenda item.

22

11. Planning Director Report –

24

- General City Updates
- Lindon Nursery and commercial depth setback discussion. He would like
26 to set up a committee to educate and look at other projects to talk about
planning principles and tour several properties and put recommendations
28 together to present to the council
- Light Industrial Zone – discussion on design items at a future meeting

30

Chairperson Call called for any further comments or discussion. Hearing none she
32 called for a motion to adjourn.

34 **ADJOURN** –

36 COMMISSIONER MARCHBANKS MADE A MOTION TO ADJOURN THE
MEETING AT 9:25 PM COMMISSIONER KALLAS SECONDED THE MOTION.
38 ALL PRESENT VOTED IN FAVOR. THE MOTION CARRIED.

40

Approved – February 25, 2020

42

44

Sharon Call, Chairperson

46

48 _____
Michael Florence, Planning Director

Lindon City
100 North State Street
Lindon, UT 84042-1808



TEL 801-785-7687
FAX 801-785-4510
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Item 4: Mecca Holdings ordinance amendment proposal to increase the allowable height from 48 feet to 60 feet.

Mecca Holdings LLC, represented by Willie Blocker, presented a plan on February 11th for a height increase. The planning commission continued this item to a further meeting to allow the applicant to review the planning commission feedback and to meet with the adjacent neighborhood. Mr. Blocker held a neighborhood meeting on February 20, 2020. Mr. Blocker would like to report back to the planning commission on his proposal and feedback from the neighborhood.

Lindon City
100 North State Street
Lindon, UT 84042-1808

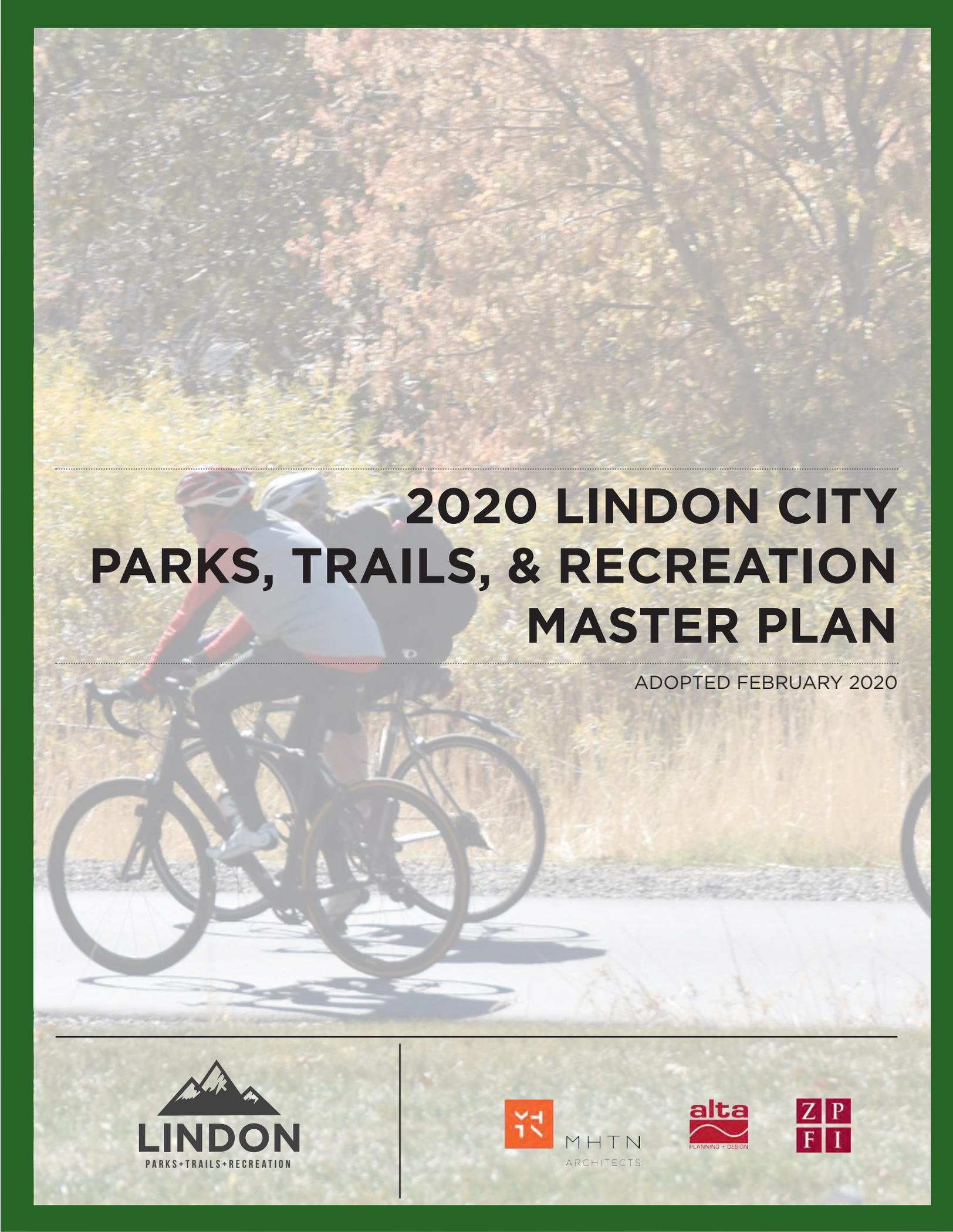


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Item 5: Lindon Parks, Trails, and Recreation Master Plan Draft

Lindon City has prepared a draft of the updated Parks, Trails, and Recreation Master Plan. The City will present highlights of the draft update for planning commission consideration and feedback.

No motion required, but feedback is requested. Input on future parks and trails identified on the maps will be the primary focus for which feedback is needed.



2020 LINDON CITY PARKS, TRAILS, & RECREATION MASTER PLAN

ADOPTED FEBRUARY 2020



ACKNOWLEDGMENTS

The following contributed to the Lindon City Parks, Trails, and Recreation Master Plan:

STEERING COMMITTEE

LINDON CITY

Mike Vanchiere - Lindon City Council
 Mike Florence - Lindon City Planning
 Anders Bake - Lindon City Planning
 Noah Gordan - Lindon City Engineer
 Adam Cowie - City Administrator
 Jake Hoyt - Lindon City Council

Heath Bateman - Parks and Recreation Director
 Jacob Woodcox - Parks Technician
 Wade Webb - Parks Superintendent
 Hannah Silvey - Recreation Supervisor
 Alan Walker - Aquatics Center Manager

Rebekah Mecham - Resident
 Dan Stone - Resident
 Allyson Omdahl - Resident

PLANNING TEAM

MHTN

Randy Boudrero - Landscape Architect
 Ryan Wallace - Planner & Project Manager
 Amanda Dillon - Community Planner
 Vince Olcott - Landscape Architect
 Dan Smith - Landscape Architect
 Allii Castillo - Community Planner

ALTA PLANNING

Dave Foster - Trails & Transportation Planner
 Emily Guffin - Transportation Planner

ZIONS PUBLIC FINANCE

Susie Becker- Economic Analyst
 Benj Becker- Economic Analyst

OVERVIEW

Section 1: Introduction

This section provides the purpose of this plan, background data on Lindon City, brief demographics, community involvement in this plan's development, and a summary of relevant sections from existing City plans and documents that bear influence on this plan.

Section 2: Existing Conditions

The second section of this document focuses on state of the existing City's parks, trails, and recreation facilities including maps, results from the consultant team's needs and gaps assessment and level of service analyses, and land values.

Section 3: Recommendations, Goals, and Policies

In this section, recommendations are made to improve and update the City's parks, trails, and recreation facilities. Proposed facility improvements are described and listed by investment priority and expected outcomes of the plan after implementation are described.

Section 4: Costs and Funding Opportunities

This section encapsulates the Impact Fees Facilities Plan and provides a cost analysis/estimate for recommended system improvements.

Section 5: Appendices

Appendix A- Impact Fee Analysis
 Appendix B- Public Engagement Results

2020 LINDON CITY PARKS, TRAILS, & RECREATION MASTER PLAN

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What do you wish Lindon's parks had? Think of things you like to do...things you

My name is (optional) Ruth Cox My age is (optional) 6



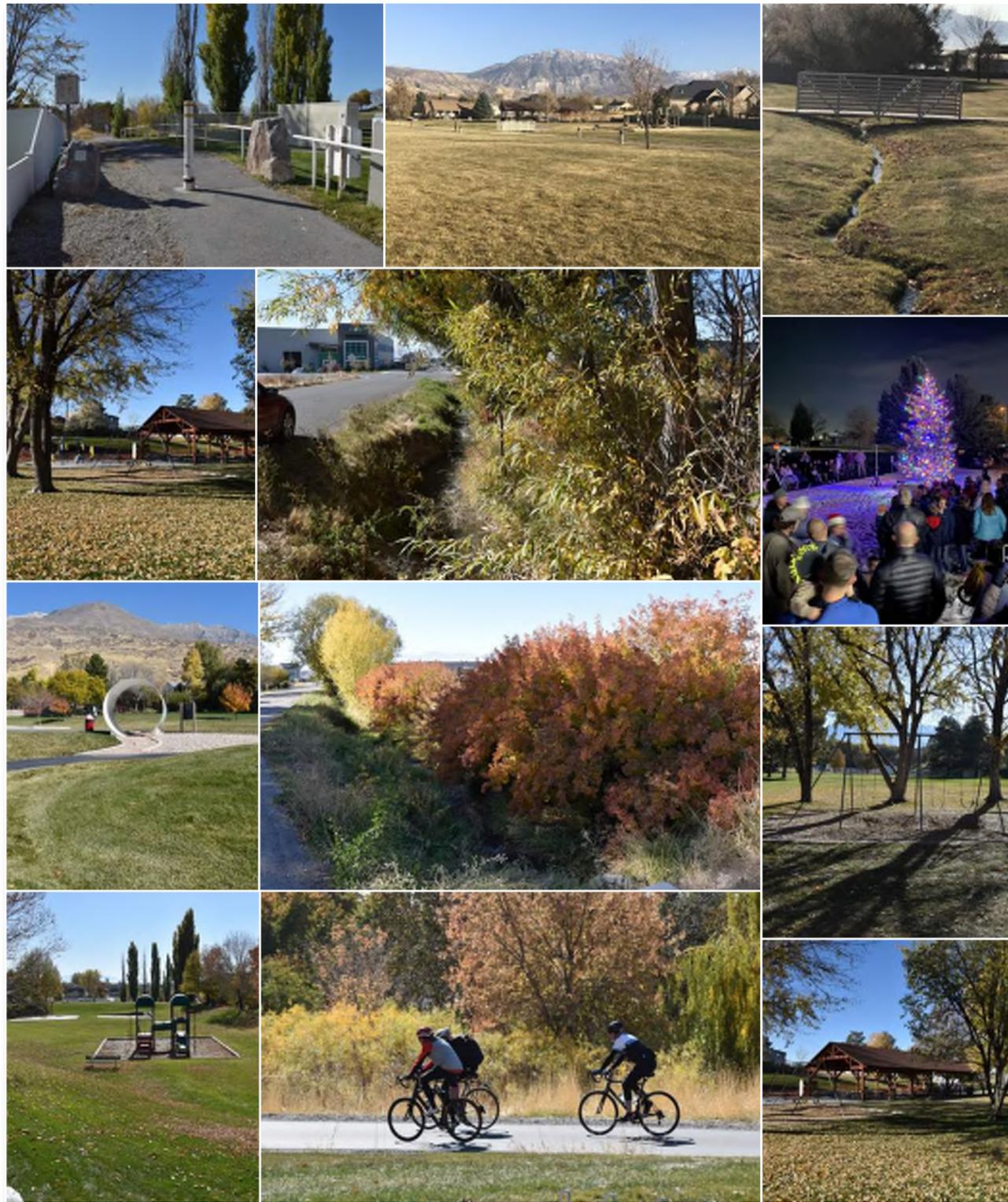
Swing set! Rainbow slides! Merry round!

Drawing by a child from the Parks, Trails and Recreation Master Plan Update Open House

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01 | INTRODUCTION



A. OVERVIEW

PURPOSE

This plan serves as an update to Lindon City's 2008 Parks, Trails, and Recreation Master Plan (PTRMP), which has been guiding the City for more than a decade. With a population of 9,900 in 2008, the City has grown by over 10 percent in the past 11 years. It is projected to continue growing, necessitating an update to the plan. This update will incorporate recommendations from the City's recently completed Bicycle and Pedestrian Master Plan (BPMP) to allow the PTRMP to continue to be the authoritative source of direction regarding parks, trails, and recreation in the community to 2030 and beyond. It is intended to also update the more general parks and recreation section of the City's General Plan.

CLASSIFICATION OF PARKS AND TRAILS

Parks in Lindon are separated into three main classifications: Community Parks, Neighborhood Parks and Trail head Facilities. Each of these facilities are defined in the 2011 Lindon City General Plan. Community Parks concentrate a broad range of recreational activities for major portions of the City. The General Plan defines the classifications as follows:

- **Community Parks** typically contain 'destination' amenities such as pavilions, ball fields, rodeo arenas, tennis courts, etc. and are usually 4 acres or larger in size. Community Parks should be located so as to promote accessibility from the entire community, but should be designed so as to not have adverse impacts to residential areas (i.e., lighting, noise, etc.).
- **Neighborhood Parks** provide basic recreational opportunities, such as grassy areas, picnic, and playground facilities easily accessible to residents.
- **Trail head Facilities** should provide year round water and trail access for trail users. Some parking locations at trail heads should be considered. Also, after considering the surroundings and proposed use of the facilities, amenities such as benches, location markers, drinking fountains, or a small restroom may be provided.

The goals outlined by the City for Trails system is to provide the means to accomplish safe, free and non- motorized movement throughout the community. Trails in Lindon are categorized as regional trails, local trails or US Forest Service Trails. Regional trails generally span communities, and connect to other regional trails. Local trails generally connect the places people live and work to destinations such as regional trails (the Bonneville Shoreline Trail, the Great Western Trail, the Utah Lake Trail, Lindon's Foothill Trail and the Lindon Heritage Trail), recreational facilities, schools, public facilities, retail establishments, etc.

They may also provide looped routes that provide opportunities for exercise and enjoyment not necessarily associated with a destination. Several US Forest Service trails exist in the foothills above the City.

PROJECT IMPROVEMENTS VS. SYSTEM IMPROVEMENTS CRITERIA

Parks, Trails and recreational facilities are categorized according to their function as either project improvements or system improvements as shown in table 2.2.

Project Improvements include facilities that benefit a small area and are generally of little benefit to the community as a whole. These facilities include private facilities that limit access to the general public, public facilities to which the general public does not have access, and public facilities that serve isolated or individual subdivisions or residential areas. This type of facility often lacks size, amenities, parking and connectivity to other system improvements. Project improvements are often placed within the community as an amenity to a particular subdivision, possibly related to receiving increased density considerations. Project improvements cannot be funded through impact fees, receive credit for costs against impact fees, or be considered in the impact fee level of service.

System improvements are intended to benefit the community as a whole and are allowed to be funded through impact fees. These types of facilities include improvements that are accessible to the general public and provide access, amenities, parking/connectivity and adequate facilities to meet needs throughout the City. Often these facilities are located outside the specific development and are regional in nature.

When park, trail or recreational facilities are provided as part of a land development for which there are density bonus considerations associated with dedication of land for or improvement of parks, trails or recreational facilities (even when they would otherwise meet the definition of system improvements), such dedication of land or improvements cannot also be used as a credit against impact fees.

This Master Plan and Capital Facilities Plan identifies the parks, trails and recreational facilities determined to be needed by the community. When land and improvements are dedicated to the public as part of a development, they must be included in this plan to be eligible for impact fee credits.

In the case of parks, except as noted in the preceding paragraph, a facility is defined as a system improvement if it contains (or is planned to contain in the Lindon Parks, Trails and Recreation Master Plan) two of the three following elements:

- It is at least 4 acres in size
- It contains 8 general amenity credits from the list below and adequate off- street parking:
 - Permanent restroom facilities (1 credit per facility)
 - Pavilions (1 credit each pavilion 936 SF or larger)
 - Picnic tables (outside of pavilions) (1 credit for 12 tables; max credit: 1)
 - Playgrounds (1 credit per 900 SF; max credit: 2)
 - Exercise equipment (1 credit if facilities are present)
 - Tennis, pickleball, or basketball courts (1 credit per court; max credit: 2)
 - Soccer fields (1 credit per field)
 - Baseball/softball diamonds (1 credit per diamond)
 - Sand volleyball pits (1 credit for 1800 SF; max credit: 1)
 - Riding arena (1 credit for 1/2 acre minimum arena; max credit: 1)
 - Lighted jogging path (1 credit per 1320 LF of path; max credit: 2)
 - Regional trail access (1 credit per trail; max credits: 2)
- It provides one of the specified unique functions listed below:
 - The park is adjacent to and provides access to the foothills or Dry Canyon
 - The park includes recreational access to a naturally flowing watercourse that flows more or less continuously or to Utah Lake
 - The park is a linear park along a regional trail; to qualify as a linear park, the park land must be at least 50 feet wide, it must be at least one half mile long, and it must be at least 4 acres in size.

- Other specific unique functions approved on a case-by-case basis by the Lindon City Council

Note that there is not a direct correlation between the designation of community parks (referred to in the General Plan) and system improvements.

Trail heads are a special subset of parks. System improvements are defined separately for trail head parks. A trail head park that is at least 2 acres in size, has off-street parking, and provides direct access to the foothills or Dry Canyon, or is adjacent to a regional trail is defined as a system improvement.

Parks that do not meet the definition of system improvements as described above are project improvements.

In the case of trails, the trails shown on the Lindon City Parks and Trails Master Plan Map comprise the system improvements in Lindon; this map contains the location, as well as the width and surface type standards for each trail. Trails not shown on the map are project improvements.

The planned Anderson Farms development will be a system improvement. Other privately owned recreational facilities are project improvements. Lindon City reserves the right to allow impact fee credits or make other similar arrangements in specific cases in which the City determines that land/ improvements associated with a development function as system improvements.

DETERMINATION OF PROJECT IMPROVEMENTS VS. SYSTEM IMPROVEMENTS FOR PARKS

Table 1 illustrates the determination of project vs. system improvements for trail head parks in Lindon according to the established criteria.

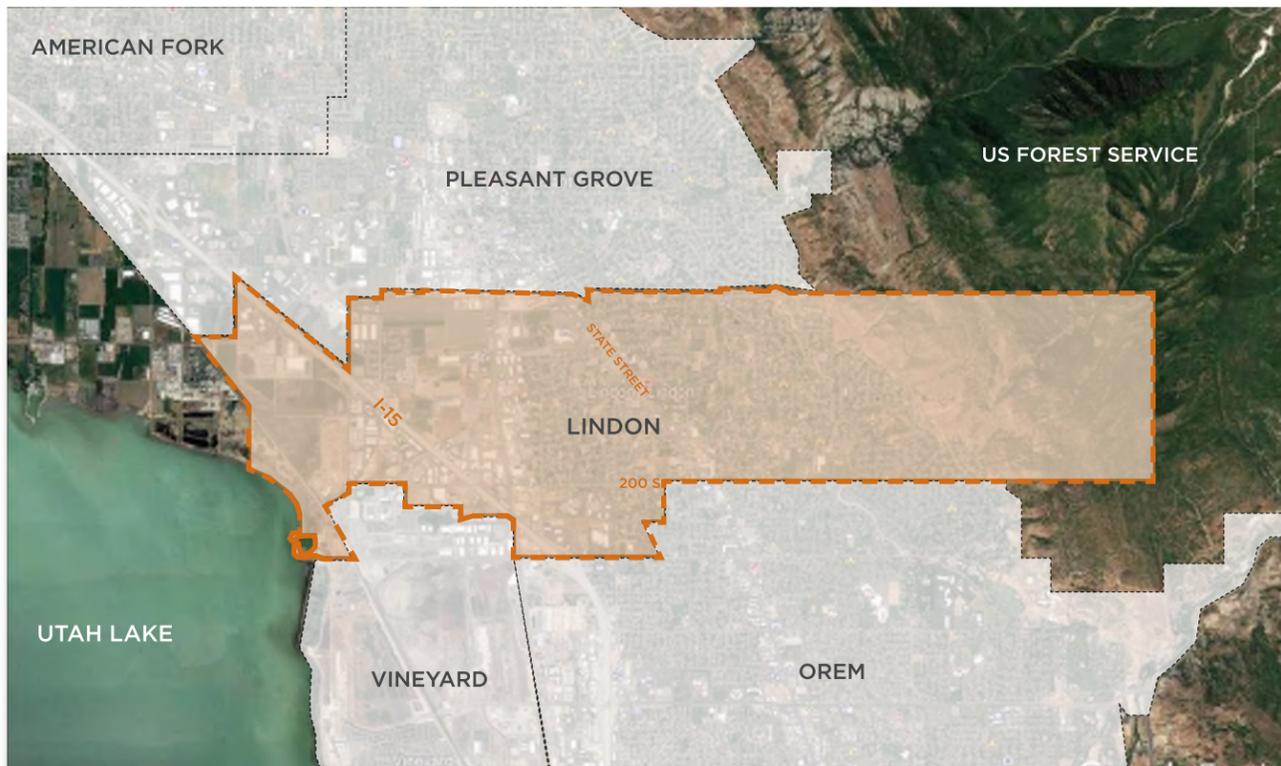
TABLE 1.1 DETERMINATION OF PROJECT VS. SYSTEM IMPROVEMENTS FOR TRAIL HEAD PARKS

	Name	Location	Type (in General Plan)	CRITERIA FOR TRAIL HEADS			RESULTS
				Size (2+ acres)	Off-Street Parking	Foothill or Dry Canyon Access	System or Project Improvement
							Type (must meet all criteria)
1	Dry Canyon Trail head	Mouth of Dry Canyon	Trail head	Yes	Yes	Yes	System
2	Equestrian Staging Area	185 North 1200 East	Trail head	No	Yes	Yes	Project
3	Lindon View Park	754 North 780 East	Trail head	No	Yes	Yes	Project

TABLE 1.2 DETERMINATION OF PROJECT VS. SYSTEM IMPROVEMENTS FOR TRAIL HEAD PARKS CONT'D.

Determination of Project vs. System Improvements for Parks (Based on Planned Improvements)																												
#	Name	Location	Type (in General Plan)	CRITERIA FOR PARKS																	RESULTS							
				Size (acres)	Credit	General Amenities														Provides Specified Unique Function		Type						
						Restrooms	Pavilions	Picnic Tables	Playgrounds	Exercise Equipment	Tennis or Basketball Courts	Soccer Fields	Ball Diamonds	Sand Volleyball	Riding Arena	Lighted Paths	Regional Trail Access	Off-Street Parking	No. of Amenities	Credit	Foothills Access or Dry Canyon Access	Water Course Access	Linear Park along a regional Trail	Credit	No. of Credits	Systems or Project Improvements		
				(4+ acres)		(8 required, plus parking)														(any one)		(2 of 3 Credits to be system improvement)						
1	Canal Bridge Park	140 North 800 East	Neighborhood	1.26						1							1	No	2							0	Project	
2	Citizenship	500 North 800 East	Neighborhood	2.67			1		1									Yes	2								0	Project
3	City Center Park	200 North State Street	Community	19.62		1	2		1	1								Yes	9	x							2	System
4	Creekside Park	100 South 600 West	Community	2		1	1		1		1							No	4				1		x	1	Project	
5	Geneva Resort Park	600 South 2000 West	Community	17.63		2	2	1	2	1								Yes	13	x			1		x	3	System	
6	Hillside Park	350 North 1200 East	Community	31.66													1	No	1			1			x	2	System	
7	Hollow Park	300 East 400 North	Community	4.46		1	2		1		2							Yes	8	x							2	System
8	Keeneland Park	1150 East 100 North	Community	4		1	3		2		2							Yes	10	x							2	System
9	Meadow Park	1700 West 500 North	Community	4.16			1		2									Yes	3				1		x	2	System	
10	Murdock Canal Park	Along Murdock Canal	Community	19.36													1	No	1					1	x	2	System	
11	Orchard Park	600 North Main Street	Community	4		1	2		2		2							Yes	8	x							2	System
12	Panorama park	900 East 140 North	Neighborhood	1.17					1		1							No	2								0	Project
13	Pheasant Brook Park	800 West 300 North	Community	9.78		2	3		2		2							Yes	16	x							2	System
14	Pioneer Park	500 East 150 South	Community	5.3		1	2		1		2	1						Yes	8	x							2	System
15	Water Tank Park	250 North 835 East	Neighborhood	1.44														No									0	Project
16	Willow Wood Park	800 West 600 North	Community	18.11		1	3	1	2									Yes	13	x							2	System

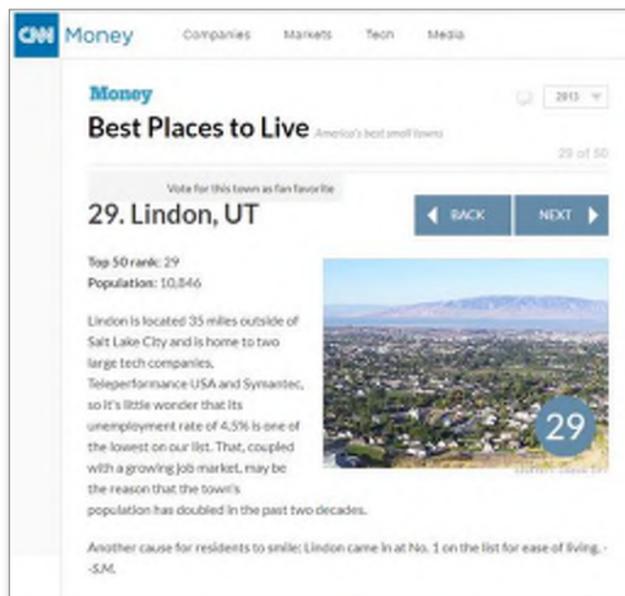
B. LINDON CITY PROFILE



Lindon community boundaries

Lindon City, located in Utah County, Utah, is a traditionally suburban community that was once home to large horse ranches and agricultural fields. It has been changing as lot sizes have been decreasing and additional residential and commercial development has entered the City. It is located 45 minutes south of Salt Lake City and is nestled below beautiful Mountain Timpanogos. Within its boundaries, it encompasses the Wasatch foothills to the east and Utah Lake to the west. With its several improved parks, trails, and amenities, it has ranked as one of CNN's 100 Best Small Cities to Live in America in 2009, 2011, and 2013.

Lindon enjoys strong demographic characteristics including population growth, education and household income. The median income of the average family is almost \$20,000 higher than the median income of the average Utah household and 80% of the population own their home creating stable, high quality neighborhoods throughout the community.



Lindon ranked 29th out of 50 towns considered for Best Places to live

<p>2020 POPULATION:</p> <p>11,512</p> <p>ANNUAL GROWTH RATE:</p> <p>1.27%</p> <p>POPULATION WITH ASSOCIATE'S DEGREE OR HIGHER:</p> <p>52.8%</p> <p>MEDIAN HOUSEHOLD INCOME:</p> <p>\$84,417</p>	<p>TOTAL HOUSEHOLDS:</p> <p>2,812</p> <p>MEDIAN HOUSEHOLD AGE:</p> <p>26.8</p> <p>OWNER OCCUPIED HOUSEHOLDS:</p> <p>2,207</p> <p>RENTER OCCUPIED HOUSEHOLDS:</p> <p>439</p>
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Source: 2018 edcUTAH Lindon City Profile and Lindon City Staff

C. COMMUNITY INVOLVEMENT IN THE PLANNING PROCESS

STEERING COMMITTEE

The City organized a group of steering committee members including city staff from planning, engineering, and parks and recreation. Residents and City Council members also participated. The group met on a bi-weekly basis with the consultant team to guide progress on the plan development and provide valuable input. In order to represent various perspectives of the community and the City Administration, the Steering Committee included engaged community members who care deeply about Lindon Parks and Recreation, representation from the City Council, City Administrative Staff members and the staff from the Parks and Recreation Department.

The Plan Steering Committee met on a bi-weekly basis at the Lindon Municipal Building, over a three month period from November 2019 to January 2020. At each of the six Steering Committee meetings, analysis and information were presented, informing the decision-making process of guiding the plan development. Beginning with formation of Guiding Principles of the Plan, the Steering Committee next considered existing conditions of parks, trails and recreation. After understanding community desires, existing needs and potential opportunities, recommendations for implementation were formed through careful consideration and alignment of City needs, available funds and desires by community members.



Steering Committee reviews existing conditions map

PUBLIC OPEN HOUSE

A public open house was held on Monday, December 2, 2019 to inform residents and community members of the project scope, status, and to gather comments and input. The open house was scheduled to coincide with the City's annual tree lighting ceremony and visit from Santa. More than 150 residents attended the event and participated by leaving comments in various forms. Residents identified issues, concerns, ideas, and opportunities on existing parks, trails, and recreation amenities in the City. Residents placed comments on maps of parks and trails in the City and also voted on potential new improvements the City might invest in for the future. All comments and votes were considered for the recommendations in this plan. A full list of comments and number of votes can be found in the Appendix.



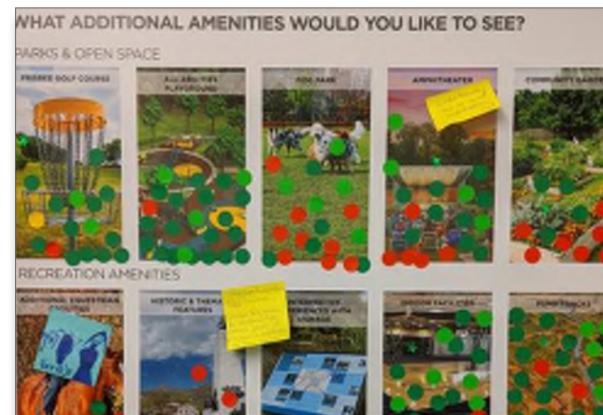
Residents of Lindon participating at the open house



Residents of all ages provided feedback



Boards provide information and maps of existing parks



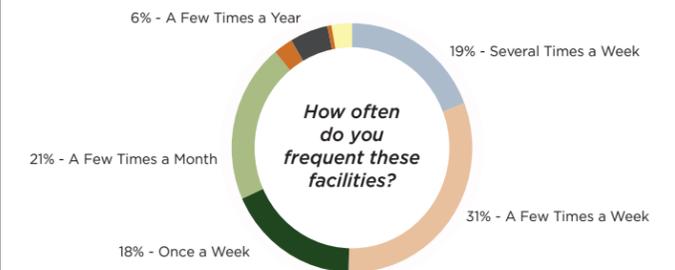
Residents placed dots to identify desired amenities



Children sharing comments in the form of sketches

ONLINE SURVEY RESULTS

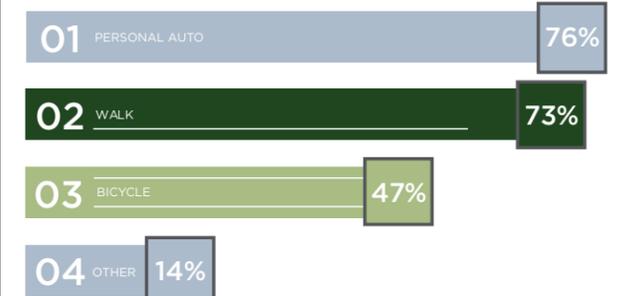
The consultant team published a survey to gather additional input from the City's residents and community members. The survey was open from late October to late December (2 months) and was promoted by the City through social media and in-person posters and flyers. Responses were analyzed and used to inform master plan recommendations. Highlights are provided in the next chapter and a summary of all the 213 responses can be found in the Appendix.



What are your favorite parks, trails, or recreation facilities outside of Lindon and why?



How do you travel to Lindon's parks, trails, and facilities?



D. COORDINATION WITH LINDON CITY PLANS

This Parks, Trails and Recreation Master Plan builds upon and integrates recommendations and guidelines identified in the City’s General Plan and the Bicycle and Pedestrian Master Plan.

2011 GENERAL PLAN

Lindon City’s 2011 General Plan identified a community vision “to provide for a strong, positive civic image and identify within a clean and attractive physical setting which seeks to preserve a high quality, open, rural living atmosphere which is also receptive to compatible services and amenities provided by some elements of urban living.”

One of the plan’s objectives for the vision includes “maintaining the quality of existing and future neighborhoods and land use areas within the City through community beautification and improved parks & trails.” The General Plan also has a section on Parks and Trails which states that “parks should be provided to allow for a variety of recreational opportunities to meet the needs of all areas of the community.”

The following ten parks and recreation guidelines are recommendations from the 2011 General Plan:

1. THE CITY SHOULD BE PROACTIVE IN EXPANDING, DEVELOPING, AND MAINTAINING ITS PARK SYSTEMS. THE CITY SHOULD PLAN FOR FOUR (4) ACRES OF PARKS AND TRAILS FOR EVERY 1000 RESIDENTS.

- Acquisition of land should occur as early as possible to help assure availability, affordability, and preservation.
- A committee or committees to promote park, pathway, and trail facilities should be established and on-going.
- Parks should be located to enhance unique landmarks, including historical sites and buildings, and environmentally significant areas. Sites with unique open space and scenic values should be a focus of public acquisition efforts.
- Natural areas, floodplains, forested areas, meadows, wetlands, and other important environmental features should be preserved as open space resources.
- Open space dedicated for safety and ecological purposes (such as detention areas) may be secondarily used as parks so long as the primary use is not diminished.

2. THE CHARACTER OF PARKS SHOULD REFLECT THE UNIQUE FEATURES AND LIFESTYLES OF THE SURROUNDING NEIGHBORHOODS.

3. PRIVATE DEVELOPMENT PARTICIPATES IN PARK, AND TRAIL DEVELOPMENT THROUGH PARK IMPACT FEES, DEDICATION OF LAND, AND/OR CONSTRUCTION OF FACILITIES.

4. THE CITY SHOULD ENCOURAGE NEIGHBORS TO COOPERATIVELY DEVELOP NEIGHBORHOOD PLAY PARKS WHICH MAY THEN BE TURNED OVER TO THE CITY TO BE MAINTAINED AS PART OF THE CITY PARK SYSTEM.

5. CONVENIENT ACCESS TO PUBLIC PARK SITES AND RECREATIONAL AREAS SHOULD BE ENSURED.

6. AN ACTIVE MUNICIPAL ROLE IN PROVIDING YOUTH-ORIENTED RECREATIONAL PROGRAMS AND SERVICES SHOULD BE MAINTAINED, TO INCLUDE AREAS FOR FIELD SPORTS (E.G., BASEBALL, SOCCER), MULTIPURPOSE COURTS, PICNIC AREAS, PLAYGROUNDS, AND PAVILIONS.

7. CLOSE COOPERATION BETWEEN THE CITY, PUBLIC AND PRIVATE SCHOOLS, PUBLIC AGENCIES, COMMUNITY GROUPS, VOLUNTEER ORGANIZATIONS, BUSINESS AND INDUSTRY SHOULD BE CONTINUED IN THE PROVISION OF RECREATIONAL SERVICES. FOR EXAMPLE, SUPPLEMENTAL RECREATION OPPORTUNITIES ARE AVAILABLE AT MOST SCHOOL SITES. JOINT USE OF PARK AND SCHOOL FACILITIES AND RECREATIONAL ACTIVITIES SHOULD BE ENCOURAGED.

8. LINEAR PARKS/TRAILS SHOULD BE MAINTAINED AND EXPANDED ALONG STREAMS, CREEKS, EASEMENTS, AND RIGHTS-OF-WAY, I.E. HOLLOW WATER SOURCE, PROCTOR DRAINAGE DITCH, BATTLE CREEK AND GROVE CREEK DRAINAGES, SALT LAKE AQUEDUCT, USBR AQUEDUCTS, NORTH UNION CANAL, MURDOCK CANAL, ETC.

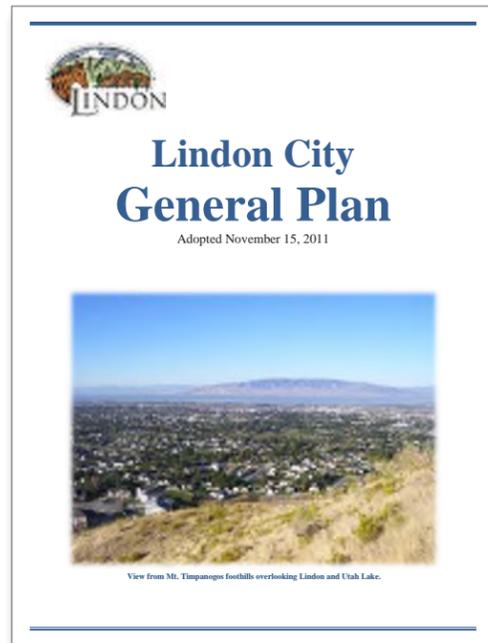
9. LANDSCAPING AND FORESTRY PROGRAMS SHOULD, IN COORDINATION WITH RELEVANT COMMITTEES OR BOARDS, BE CONTINUED AND EXPANDED ON PUBLIC PROPERTY AND ALONG ROADWAYS, INCLUDING THE CITY CEMETERY, SECONDARY WATER RESERVOIRS, CULINARY WATER STORAGE FACILITIES, GENEVA ROAD, AND STATE STREET.

10. IN ADDITION TO EXISTING CITY FUNDS, GRANTS AND OUTSIDE SOURCES OF FUNDING, THE CITY SHOULD EXPLORE ADDITIONAL OPTIONS FOR RAISING MONEY TO BUILD AND EXPAND THE PARKS AND RECREATION SYSTEM AND/OR CONSERVE OPEN SPACES, INCLUDING CITIZEN SUPPORTED BOND MEASURES.

2017 GENERAL PLAN LAND USE PLANNING SURVEY

The City conducted a General Plan survey in 2017 in which it gauged how its residents felt about future land use in the City. The survey revealed residents felt the following ways about parks, open space, and recreation:

- Parks and Open Space ranked as the top most desired future development type.
- The most important way to improve the aesthetic appearance of the City is to protect open spaces.
- 86% of survey respondents felt that open space is a defining characteristic of Lindon City.
- 67% of survey respondents said Lindon's traditional slogan, "little bit of country" still reflect the City today
- Open space and parks & recreation opportunities rank as 3rd and 5th, respectively for what makes Lindon unique to residents. (1st and 2nd were residential lot size and safety/crime rate, respectively.)
- 83% of residents favored the City trying to secure land for open space preservation.



Lindon City General Plan

2015 BICYCLE AND PEDESTRIAN MASTER PLAN

The City adopted a new Bicycle and Pedestrian Master Plan in July 2015. It set forth the following Vision and five goals:

VISION
THE VISION STATEMENT GUIDES LINDON'S DIRECTION FOR BICYCLE AND PEDESTRIAN FACILITIES, AND PROVIDES CLEAR DIRECTION FOR THE PROJECT. TO ENSURE CONSISTENCY WITH NEIGHBORING COMMUNITIES, THE STEERING COMMITTEE REVIEWED LANGUAGE FROM PREVIOUSLY DEVELOPED LOCAL BICYCLE AND PEDESTRIAN MASTER PLANS, INCLUDING THE AMERICAN FORK BICYCLE AND PEDESTRIAN MASTER PLAN (2013), THE LEHI BICYCLE AND PEDESTRIAN MASTER PLAN (2013), THE PLEASANT GROVE MASTER PLAN (2013), AND THE OREM BICYCLE AND PEDESTRIAN MASTER PLAN (2010), AS WELL AS NATIONAL EXAMPLES FROM ANCHORAGE, ALASKA; DAVIS, CALIFORNIA; MINNEAPOLIS, MINNESOTA; AND PORTLAND, OREGON. THE VISION STATEMENT OF THE LINDON BICYCLE AND PEDESTRIAN PLAN IS:

"LINDON WILL ENCOURAGE A COMMUNITY THAT VALUES HEALTHY MOBILITY OPTIONS AND A HIGH QUALITY OF LIFE THROUGH THE PROMOTION OF A SAFE AND WELL-CONNECTED BICYCLING AND PEDESTRIAN NETWORK."

GOAL 1: CREATE A COMPLETE BICYCLE AND PEDESTRIAN NETWORK TO INCREASE BICYCLE AND PEDESTRIAN MODE SHARE

GOAL 2: FOSTER A CULTURE OF BICYCLE AND PEDESTRIAN PLAN ADOPTION AND IMPLEMENTATION

GOAL 3: IMPLEMENT COMPREHENSIVE EDUCATION PROGRAMS

GOAL 4: IMPROVE SAFETY AND ENFORCEMENT ON LINDON STREETS, PATHS, AND BIKEWAYS

GOAL 5: MAINTENANCE: KEEP NON-MOTORIZED FACILITIES CLEAN, SAFE, AND ACCESSIBLE



Lindon Bicycle and Pedestrian Master Plan

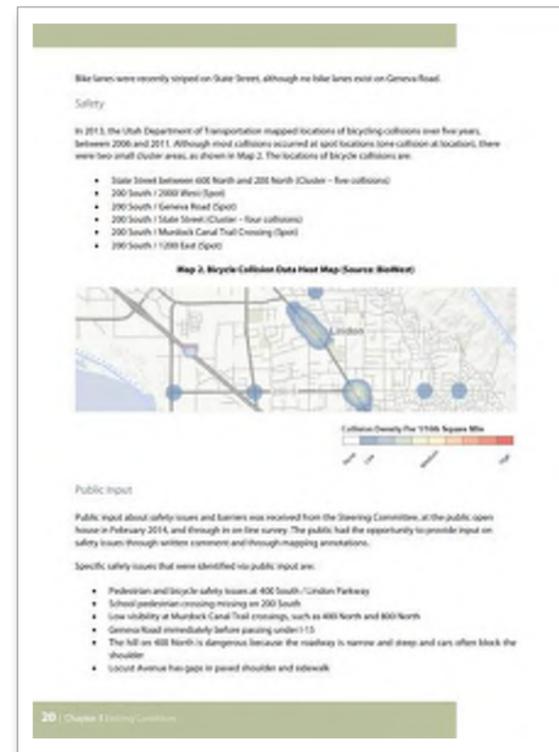
April 2015

FEHR + PEERS
 alta CRSA

Lindon Bicycle and Pedestrian Master Plan



Responses from the 2017 General Plan Survey



E. GUIDING PRINCIPLES



At the outset of the Master Plan update process the steering committee developed guiding principles which would be utilized to inform the decision-making that would need to occur. The diagram at right implies that no hierarchy exists, but rather that each of the principles are of equal importance with regards to implementing recommendations within this Master Plan.



03 | INTEGRATED WITH CITY DOCUMENTS & GOALS

The Parks, Trails, and Recreation Master Plan will reflect and inform the vision and goals set forth in the City's planning documents such as the General Plan and the Bicycle and Pedestrian Master Plan.



01 | A HOLISTIC APPROACH TO RECREATION

The Parks, Trails, and Recreation Master Plan will guide the orderly development and maintenance of a wide variety of recreational offerings and amenities including spaces and programming for indoor and outdoor activities, and will integrate its trails and parks with regional recreation plans.



04 | FILLS A NICHE FOR THE COMMUNITY

Focusing on Lindon's natural amenities and demographics, the City will provide active and passive recreational offerings to reinforce the community's identity, and compliment recreational opportunities already provided in neighboring communities.



02 | IMPROVED HEALTH AND WELL-BEING FOR RESIDENTS OF ALL ABILITIES

The City's parks, trails, and recreation areas will be accessible, inclusive and equitable for community members, both young and elderly, families and individuals, and residents with special needs.



05 | PRUDENT USE OF CITY FUNDS

Lindon City will maintain and expand the community's access to parks, trails, and recreation through the wise use of available funds and resources.



02 | EXISTING CONDITIONS

A. LEVEL OF SERVICE ANALYSIS

Level of service is a term used to describe the degree to which a service is provided to users. The community plans to provide facilities that meet or exceed the established level of service. As noted on page 52 of the 2011 Lindon City General Plan, under the heading of Park and Recreation Guidelines, "The City should plan for four (4) acres of parks and trails for every 1000 residents".

Parks, trails and other recreational facilities serve related functions- they are intended to help meet public recreation, leisure activity, exercise sports, gathering and cultural needs. The facilities provided should be suited to the needs, desires and lifestyles of the community. While a planning process attempts to predict what those needs, desires, and lifestyles of the community will be, it must be flexible enough to accommodate the actual needs, desires and lifestyle of the community in the future.

In order to afford this flexibility the level of service can be described on the basis of dollars per capita. This provides a unit of measurement that allows parks, trails and recreational facilities to be treated as a whole. It allows us to plan for a certain dollar level of investment that can be used to provide the facilities that are in demand at the time funding is available, rather than being constrained to provide predetermined facilities whether or not they are in demand in the future.

The impact fee level of service is lower than the functional level of service because some land has been donated and some improvements have been paid for using federal grants.

In order to identify costs that can be included in the impact fee level of service, tables in this document showing value of land and improvements include a separate column for impact fee eligible costs, which does not include the value of land or improvements paid for with federal funds.

In both the case of functional level of service and impact fee level of service, there will be an established threshold of performance, or an established level of service. The goal is to meet or exceed that threshold. There will be new and existing levels of service in both cases as well, which at any given time will likely be higher or lower than the established level of service.

The established functional level of service in Lindon is a minimum of 4 acres of parks, trails and recreation facilities for every 1000 residents, as suggested in the General Plan. As will be shown in this document, Lindon easily exceeds this level, so there will be little discussion of this measure of level of service.

The impact fee level of service in Lindon is a dollar value of land and improvements per resident of Lindon. It will be established at the level that currently exists. It is expected that Lindon City will use general funds to supplement impact fees in the construction of parks, trails and recreational facilities. By so doing, it is expected that the established impact fee level of service will increase over time. Furthermore, inflation will cause the established impact fee level of service to increase over time as well.

Since much of this document will relate to the impact fee level of service, this document will use "level of service" to mean the impact fee level of service. This document will use "functional level of service" when referring to the functional level of service.

B. EXISTING FACILITIES



Accessible, well paved trails can be enjoyed by people of all ages

Lindon City has an extensive park and trail system with a Functional Level of Service standard of four (4) acres per everyone 1,000 persons. Looking at a proximity map of the existing facilities, it is clear to see that Lindon City exceeds that number at about 10 acres per 1000 persons.

Two major trails exist within the city. The Heritage trail is a 10-foot-wide asphalt trail that runs from 1550 West, up Lakeview Road, into the city Center Park, along Main Street and east on Center Street to connect to the trail at Canal Drive. The Murdock Canal Trail, which runs north to south from the boundary with Orem on the south to Pleasant Grove on the north. Lindon city has provided neighborhood and community paths to its residents, with some already connected to the paths and trails that lead to other cities, US Forest trails, and other amenities.

The current LOS for Lindon City parks is approximately 10 acres per 1,000 persons. This is above the goals of the city therefore, it is recommended prioritization should be emphasized on the improvement of current amenities.

TABLE 2.1 LEVEL OF SERVICE IN NEIGHBORING CITIES

Level Of Service		
Location	Acres	Per Person
Payson	7	1,000
Saratoga Springs	2.29	1,000
Provo	4.04	1,000
Orem	2.76	1,000
Springville	7.67	1,000
American Fork	6.47	1,000
Highland	12.1	1,000
Lehi	7.53	1,000

PARKS EXISTING CONDITIONS

TABLE 2.2 EXISTING PARK FACILITIES FROM 2008 LINDON PARKS, TRAILS AND RECREATION MASTER PLAN AND CAPITAL FACILITIES PLAN

#	Name	Location	Type (in General Plan)	Type (System or Project Improvement)	Owned Land (acres)	Description
1	Lindon View Park	754 North 780 East	Neighborhood	Project	2.26	Land, some landscaping, fencing
2	Citizenship	500 North 800 East	Neighborhood	Project	2.67	Land, landscaping, playground, walking path, parking
3	Lindon City Center Park	200 North State Street	Community	System	11.3	Land, landscaping, large pavilion, restrooms, playground, riding arena, ball fields, drinking fountain, parking
4	Creekside Park	100 South 600 West	Community	Project	2	Land, landscaping, small pavilion, restrooms, picnic area, playground, fencing
6	Hillside Park	350 North 1200 East	Community	System	31.66	Land, hiking trails, native vegetation
7	Hollow Park	300 East 400 North	Community	System	4.46	Land, landscaping, small pavilion, restrooms, lighted path, parking
8	Canal Bridge Park	140 North 800 East	Neighborhood	Project	1.26	Open fields adjacent to trail
9	Meadow Park	1700 West 500 North	Community	System	4.16	Land
10	Murdock Canal Park	Along Murdock Canal	Community	System	NA	Vegetated area along the Murdock Canal Trail
11	Fryer Park	600 North Main Street	Community	System	4	Land
12	Panorama park	900 East 140 North	Neighborhood	Project	1.17	Land, landscaping, playground, basketball court
13	Pheasant Brook Park	800 West 300 North	Community	System	9.78	Land, landscaping, pavilion, restrooms, fencing, lighted path, parking
14	Pioneer Park	500 East 150 South	Community	System	5.3	Land, landscaping, small pavilion, restrooms, playground, fencing, soccer field, lighted path, drinking fountain, parking

RECREATION FACILITY



Photo Source: www.FlowRider.com

LINDON AQUATICS CENTER

In 2009 the City constructed a public aquatic recreation facility adjacent on City Property adjacent to the City Center Park. This facility provides a necessary recreation opportunity which is heavily used by the residents of the City and non-residents as well. This facility is available for use during the months of May to September.

The City also purchased property from the LDS church which has been remodeled into an indoor recreation center. This facility also houses the Senior Citizen and the Cultural Arts Center Facilities.

NEIGHBORHOOD PARKS



CITIZENSHIP PARK

Citizenship Park consists of 2.47 acres located at 500 north 800 east. The parks amenities consist of 1 Pavilion (25'x25'), 2 small playgrounds, 1 drinking fountain, 10 benches, asphalt path, flag pole, 1 half size basketball court with a single basketball standard, grass hills, detention basins and landscaping. There is a small paved parking lot with a capacity of six vehicles.



PANORAMA PARK

Panorama Park consists of 1.09 acres located at 900 east 140 North. The park amenities consist of 2 benches, a small playground a half-court basketball with a single basketball standard, access to Lindon heritage trail, large grass areas / detention basin.



CANAL BRIDGE PARK

Canal Bridge Park consists of 1.26 acres located at 140 north 800 east. The parks amenities consist of open grass fields/ detention basin, chain-link fencing, and a large retaining wall. The park is located adjacent to the Murdock Canal trail and the Lindon Heritage Trail.

TRAIL HEAD PARKS



LINDON VIEW

Lindon View is a regional trail head for the Murdock Canal trail. It's amenities include restroom building with an attached pavilion, 2 picnic tables, 3 bike racks, drinking fountain attached to the building, and landscaping. There is parking for approximately 13 vehicles. The park is jointly owned by Utah county parks and recreation.



DRY CANYON TRAIL HEAD

Dry canyon Trail is a popular trail. The trail head is a surfaced parking lot with approximately 30-40 parking stalls and a bathroom. The trail head also provides access to the Bonneville Shoreline Trail. It is maintained and owned by the US Forest Service.

Photo Source: www.AllTrails.com



EQUESTRIAN STAGING AREA

The Equestrian staging area is located in the foothills on the east edge of the city. It consists of gravel and asphalt paved access and parking areas allowing for loading and unloading of horses and equipment. All vegetation is native. Unpaved trails leave from this point into the foothills and mountains. Large boulders have been placed to restrict motorized vehicles from accessing the hill sides. The staging area is the Eastern terminus for the Heritage Trail.

COMMUNITY PARKS



PIONEER PARK

Pioneer Park consists of 5.3 acres located at 500 east 150 south. Park amenities consist of an existing pioneer era rock façade building, 2 Pavilions (25'x35'), playground, 6 trash cans, 1 bench, 1 trash enclosure, 1 drinking fountain at restroom building, large open grass fields surrounded with a lighted asphalt path, landscaping, access to back stop and field of adjacent Rocky Mountain Elementary school.

COMMUNITY PARKS



CREEKSIDE PARK

Creekside Park consists of 1.88 acres at 100 south 600 west. Park amenities consist of 1 pavilion (30'x40'), a monument sign, 2 bridges over the existing creek, 1 maintenance shed, 2 trash cans, asphalt path (not in good condition), 1 bench, and open grass areas. 2 pickle ball courts funded for construction in 2020



FRYAR PARK

Fryar Park consist of 4 acres located at 600 north Main Street. Park amenities consist of Large open grassy areas, a lighted asphalt path, 1 Pavilion (20'x30'), a playground, 1 trash enclosure, 4 benches, 2 trash cans and signage.



HILLSIDE PARK

Hillside Park consists of 31.66 acres located at 350 north 1200 east. This park consists of open native vegetation in the foothill above the city of Lindon. Parking consists of dirt/ gravel areas at the dead ends of pavements at 1200 east street and 380 north street. Unpaved and unmaintained gravel roads are located within the park boundaries. A buried city water tank and pump house are located within the park.



HOLLOW PARK

Hollow Park consists of 4.46 acres located at 300 east 400 north. Park amenities consist of 2 Pavilions (25'x35'), a Monument sign, Trash enclosure, 4 trash cans, 1 lighted tennis court, 2 lighted pickleball courts Lighted, an asphalt walking path, 5 benches, open grass fields and landscaping, and 1 drinking fountain at a restroom building.

COMMUNITY PARKS



MEADOW PARK

Meadow Park consist of 4.16 acres located at 1700 west 500 north. Park amenities consist of 1 Pavilion (25'x45'), a monument sign, 2 trash cans, a playground, large open grass fields with landscaping, 1 unmarked soccer field. The park consists of one main lawn, park areas on the north east corner of the road and smaller lawn areas on the south east, south west and north west corners of the intersection of 500 north and 1700 west. There is a small single loaded parking lot located in the south east quadrant of the park. A canal/ drainage ditch runs on the east side of 1700 west street.



PHEASANT BROOK PARK

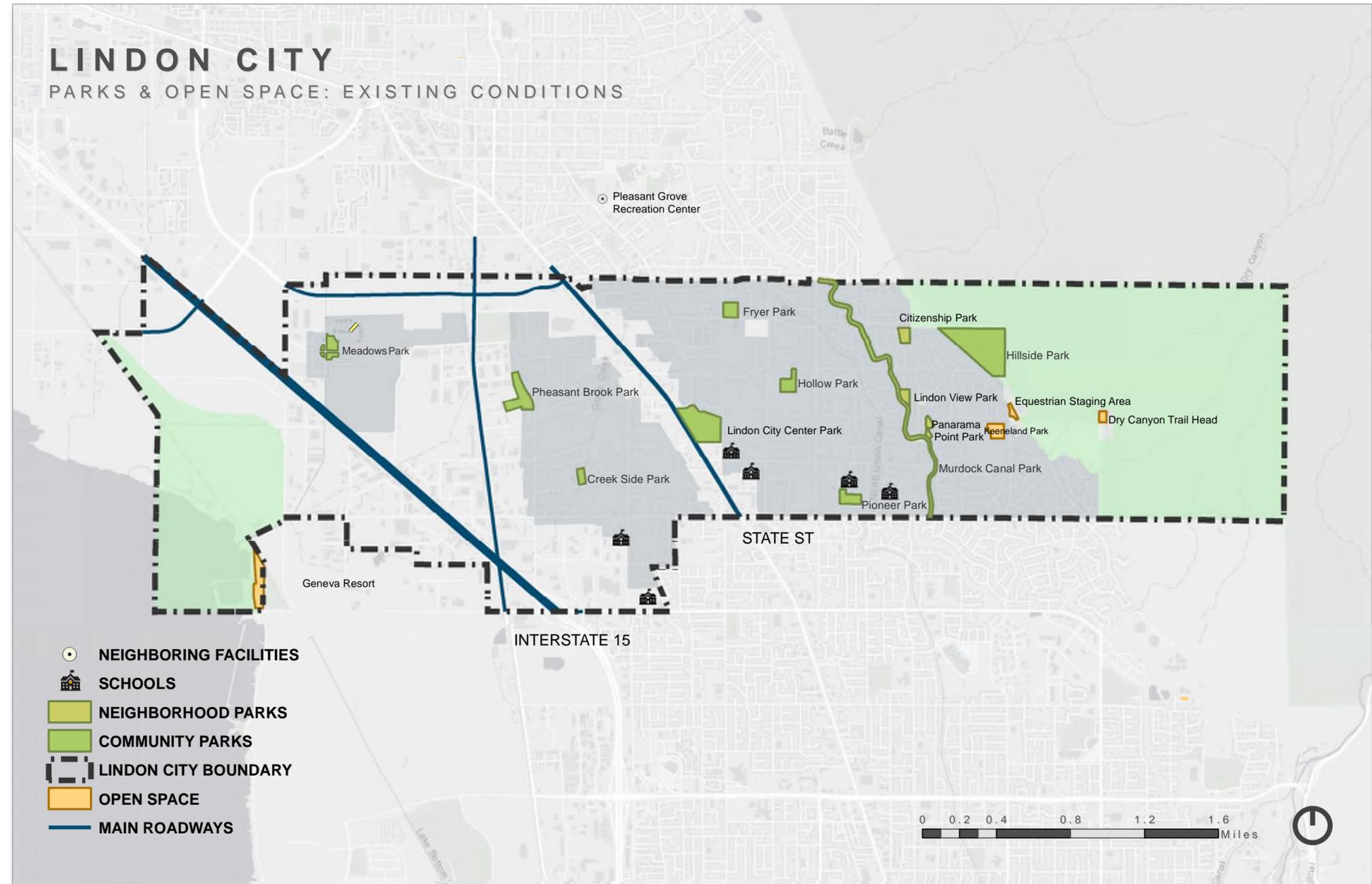
Pheasant Brook Park consists of 9.78 acres located at 800 west 600 north. The park amenities consist of 2 Pavilions (30'x60'), a playground, a restroom building, 2 baseball fields, 2 pickleball courts funded for construction in 2020, 3 soccer fields, a lighted asphalt path runs along the perimeter of the park and connects the parking lot to the other park amenities. A 35' flag pole is located adjacent to the parking lot and north pavilion. A bridge over a drainage swale connects the south pavilion to the main asphalt path. A parking lot with an approximate 80 car capacity and a trash enclosure is located on site



LINDON CITY CENTER PARK

Lindon City Center Park consists of 11.3 acres (18.6 including entire campus) located at 200 north State Street. The park is located adjacent to the Lindon Aquatic center, Lindon City Hall, Lindon Police and Fire Stations and the Lindon Community center. Park amenities consist of 1 pavilion (75'x35') and 1 pavilion (Small Octagon), 1 enclosed pavilion / building, 1 concession stand, 4 bleachers, 1 bench, trash cans, vinyl fence, chain-link fence, pool pump house, monument sign, metal pipe fencing, retaining walls, landscaping, trash enclosure, drinking fountains, 1 baseball field, 1 lighted baseball field, 1 rodeo arena and two playgrounds (one large all abilities playground and one smaller electronic playground). There are four parking lots with a capacity of 135 vehicles. The asphalt Heritage trail runs through the park and into a tunnel under state street on the west end of the park.

MAP 2.1 EXISTING CONDITIONS - PARKS & OPEN SPACE



FUTURE FACILITIES



GENEVA RESORT PARK

This 7.54 acre undeveloped parcel is conveniently located next to the Utah Lake Marina, which was once a vacation spot for Utahns. This park should complement not only the marina but should also provide needed sports fields and open space requested by the city residents during the Master Plan Open House.



ANDERSON FARMS PARK

Being developed by Ivory homes, this site will accommodate leisure, recreational and sports complex uses. According to Ivory Homes, the future park will offer pavilions, basketball courts, soccer fields, and baseball diamonds and will be a street crossing away from the newly developed homes.



KEENELAND PARK

The city in the near future desires to purchase a minimum of 4 acres of larger property on the southeast side of the city for a community park and additional open space.



LINDON VILLAGE

In 2020, Lindon City will be working with an urban design consultant to create a plan for a new character district, Lindon Village Center. This area will be developed through public and private funds to create a mid-rise development centered around a park and plaza public space. Well connected, this walkable, urban space will be easily accessed by numerous modes of transportation including walking, cycling, mass transit and private automobile. The gathering space will benefit, not only the users of the space, but the commercial area as it will foster a thriving and successful economic center by providing a pleasant and attractive space for users to visit and linger in.

TABLE 2.3 EXISTING PARK AMENITIES ANALYSIS

Name	Location	Type	Size (acres)	General Amenities																	Unique Function			
				Restrooms	Pavilions - small	Pavilions - Medium	Pavilions - Large	Picnic Tables	Playgrounds	Exercise Equipment	Pickle Ball Courts	Tennis Courts	Basketball Courts	Soccer Fields	Ball Diamonds	Sand Volleyball	Riding Arena	Lighted Paths	Regional Trail Access	Off-Street Parking	Other Considerations	FootHills Access or Dry Ca	Water Course Access	Linear Park Along a Regional Trail
Meadow Park	1700 West 500 North	Community	4.16	0	0	1	0	8	1	0	0	0	1	0	0	0	0	N	N	N	1 Pavilion (25'x45'), Monument sign, 2 trash cans, large open grass fields with landscaping, 1 unmarked soccer field	N	Y	N
Pheasant Brook Park	800 West 600 North	Community	9.78	1	0	0	2	20	1	0	2	0	3	2	0	0	0	Y	Y	0	2 Pavilions (30'x60'), 2 baseball fields, 2 pickleball courts funded for 2020, 3 soccer fields			
Creekside Park	100 South 600 West	Community	1.88	1	0	1	0	8	2	0	2	0	0	0	0	0	0	N	N	N	1 Pavilion (30'x40'), monument sign, 2 bridges, 1 maintenance shed, 2 trash cans, asphalt path (not in good condition), 1 bench, open grass areas, 2 pickleball courts funded for 2020	N	Y	N
Lindon City Center Park	200 North State St	Community	11.3	1	1	0	1	31	1	0	0	0	0	2	0	0	1	Y	Y	0	1 Pavilion (75'x35') and 1 pavilion (small octagon), 1 enclosed pavilion, 1 concession stand, 4 bleachers, 1 bench, trash cans, vinyl fence, chain-link fence, pool pump house, monument sign, metal pipe fencing, retaining walls, landscaping, trash enclosure, drinking fountains, 1 baseball field, 1 lighted baseball field	N	N	N
Aquatics Center				0	0	0	0	0	0	0	0	0	0	0	0	0	0							
Fryer Park	600 North Main St	Community	4	1	1	2	0	8	1	0	0	0	2	0	0	0	0	Y	N	Y	1 Pavilion (20'x30'), 1 trash enclosure, 4 benches, 2 trash cans	N	N	N
Hollow Park	300 East 400 North	Community	4.46	1	0	2	0	12	1	0	4	1	0	0	0	0	0	T	N	Y	2 Pavilions (25'x35'), monument sign, trash enclosure, 4 trash cans, 1 tennis court lighted, 2 pickleball courts lighted, asphalt walking path, 5 benches, open grass fields and landscaping, 1 drinking fountain at restroom.			
Pioneer Park	500 East 400 North	Community	5.3	1	0	0	0	12	1	0	0	0	1	0	0	0	0	T	N	Y	2 Pavilions (25'x35'), 6 trash cans, 1 bench, 1 trash enclosure, 1 drinking fountain at restroom, large open grass fields, landscaping, access to back stop and field of elementary school	N	N	N
Citizenship Park	500 North 800 East	Neighborhood	2.47	0	1	0	0	4	2	0	0	0	1	0	0	0	0	N	N	Y	1 Pavilion (25'x25'), 1 drinking fountain, 10 benches, asphalt path, flag pole, basketball court, grass hills and landscaping	N	N	N
Lindon View Park	280 North 780 East	Neighborhood	2.26	1	1	0	0	2	0	0	0	0	0	0	0	0	0	Y	Y	0	3 bike racks, drinking fountain attached to the building, pavilion attached to restroom (20'x45'), Landscaping, 2 picnic tables	N	N	Y
Panorama Park	900 East 140 North	Neighborhood	1.09	0	0	0	0	0	1	0	0	0	1	0	0	0	0	N	Y	N	2 benches, access to Lindon heritage trail, large grass areas, detention basin, small playground, half-court basketball	N	N	
Canal Bridge Park/Covered Bridge	140 North 800 East	Neighborhood	1.26	0	0	0	0	0	0	0	0	0	0	0	0	0	0	Y	N	N	Open fields adjacent to the trail	N	N	Y
Hillside Park	350 North 1200 East	Community	31.7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	N	N	N	Open native vegetation in foot hills	Y	N	N
Equestrian Staging Area	140 North 1200 East		1.25	0	0	0	0	0	0	0	0	0	0	0	0	0	0				Open native vegetation in foot hills	Y	N	N
Dry Canyon Trail Head			0.76	0	0	0	0	0	0	0	0	0	0	0	0	0	0				Open native vegetation in foot hills	Y	N	N
Geneva Resort Park	1700 West 500 North	Community	7.5	0	0	0	0	0	0	0	0	0	0	0	0	0	0				Was park (pre 1940's) will be a future park. Industrial area	N	N	N
Murdock Canal Park	Murdock Canal	Community	19.4	0	0	0	0	0	0	0	0	0	0	0	0	0	0				Open native vegetation along regional trail	N	N	Y
TOTALS			104	7	4	6	3	105	11	0	6	3	2	7	4	0	1							

TRAILS EXISTING CONDITIONS

In cities that are walkable and bikeable, people walk or bike for transportation and recreation because these are convenient, safe, and healthy ways to get around. Such cities make it possible for people of all ages and abilities to rely on active modes for their everyday trips to work, grocery stores, schools, parks, recreation, transit, and other civic destinations. The Lindon Parks, Trails, and Recreation Master Plan Update aspires to encourage and accommodate people who need or want to get around on foot, wheelchair, bike, horse, or other means that fall under the umbrella of active transportation, which is defined as personal transportation by which people move under their own power.

By prioritizing the Parks, Trails, and Recreation Master Plan Update, the City of Lindon is taking the first step needed to evaluate existing trail conditions and connections and establish a prioritized plan to develop infrastructure that makes walking, biking, or horseback riding feasible for both transportation and recreation. As the Plan is implemented, the expanded active transportation network will increase travel choice and make the City of Lindon a more attractive place to live, work, and play. The Parks, Trails, and Recreation Master Plan Update has the potential to impact many important aspects of life in the City of Lindon. Community health, environmental quality, equity, and economic growth, and quality of life could all be improved by the continued development of a thoughtfully planned trail system.

Community Health. Providing more opportunities for walking and biking allows people to incorporate physical activity into their daily routine and can increase access to existing recreational amenities such as regional trails, parks, and recreation centers.

Environmental Quality. A significant impact higher rates of active transportation have on the environment is improved air quality. The Wasatch Front historically rates poorly among U.S. cities for air quality levels, which puts residents at an increased risk of lung problems.

Equity. A transportation system that requires residents to rely on a car for their mobility needs is not an equitable one. Some people, due to age, ability, or economic status, are unable to drive or don't have access to a car.

Economic Growth. Walkable and bikeable communities promote property value appreciation and customer behavior that is advantageous for the local economy.

Quality of Life. More and more people prefer to live in walkable and bikeable communities. A safe, connected pedestrian and bicycle network gives people more transportation choices, promotes stronger place attachment, encourages more social interactions, and increases people's overall enjoyment of their community.



This section of the Murdock Canal Trail is heavily used and well-maintained. Over 17 miles in length, the trail connects communities throughout Utah County.

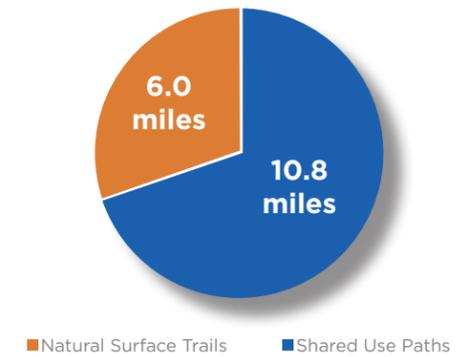
To understand the existing conditions of trails within the City of Lindon, the Plan update involved an intensive existing trail system analysis and public outreach process to determine the trail-related needs and desires of the community. Community members highlighted the importance for enhanced trail head amenities and access points, especially for the natural surface trails to the east of town and along the Lindon Heritage Trail and Murdock Canal Trail. Community members also highlighted the need for safer road crossings on the Murdock Canal Trail and the Lindon Heritage Trail.

Other community members desired better connections to neighboring communities, especially from the Equestrian Trail head to the Battle Creek Trail head in Pleasant Grove. Residents were also concerned about the lack of maintenance on several trails, desiring smoother trails and trails that accommodate all user groups, including equestrians, as well as improved winter maintenance on the Dry Canyon Access Road. This public process resulted in a better understanding of the current needs and desires of the community and will result in recommendations that are tailored to the needs, goals, and objectives of the community.

Existing Trail Network

As of Fall 2019, the City of Lindon's existing trail system includes approximately 10.75 miles of shared use trails that are designated to accommodate pedestrians and bicyclists. These trails include regional paths, such as the Murdock Canal Trail, the Utah Shoreline Trail, and the Lindon Heritage Trail, and short neighborhood connectors. There are also nearly 6 miles of natural surface trails within Lindon's limits, though many of these trails are maintained by the Forest Service. See Map 2.2 for the overall existing trail system.

Residents desire safe road crossings, more trails for ATVs and mountain bikers, a variety of trail surfaces to accommodate all user groups, better wayfinding signage, and improved trail maintenance (especially during winter months).



Lindon currently has **10.8 miles** of shared use paths and **6.0 miles** of natural surface trails.

TRAIL DEFINITIONS



SHARED USE PATHS

These are paved facilities that are designed to accommodate non-motorized users and are detached from primary vehicular roadways. They are intended to serve bicyclists, pedestrians, and other non-motorized user groups. These facilities are typically between 10ft and 12ft in width, and have shoulders of about 2ft on either side of the path. When large volumes of users are anticipated, a center line stripe is recommended to help organize traffic. If a mixed user environment is anticipated, signage should be used to help educate path users about proper yielding etiquette. When the path crosses a vehicular right of way, high-visibility crosswalks should be implemented to mitigate potential collisions. Lindon has implemented 13.0 miles of shared use paths, the most popular of which is the Murdock Canal Trail.



SIDEPATHS

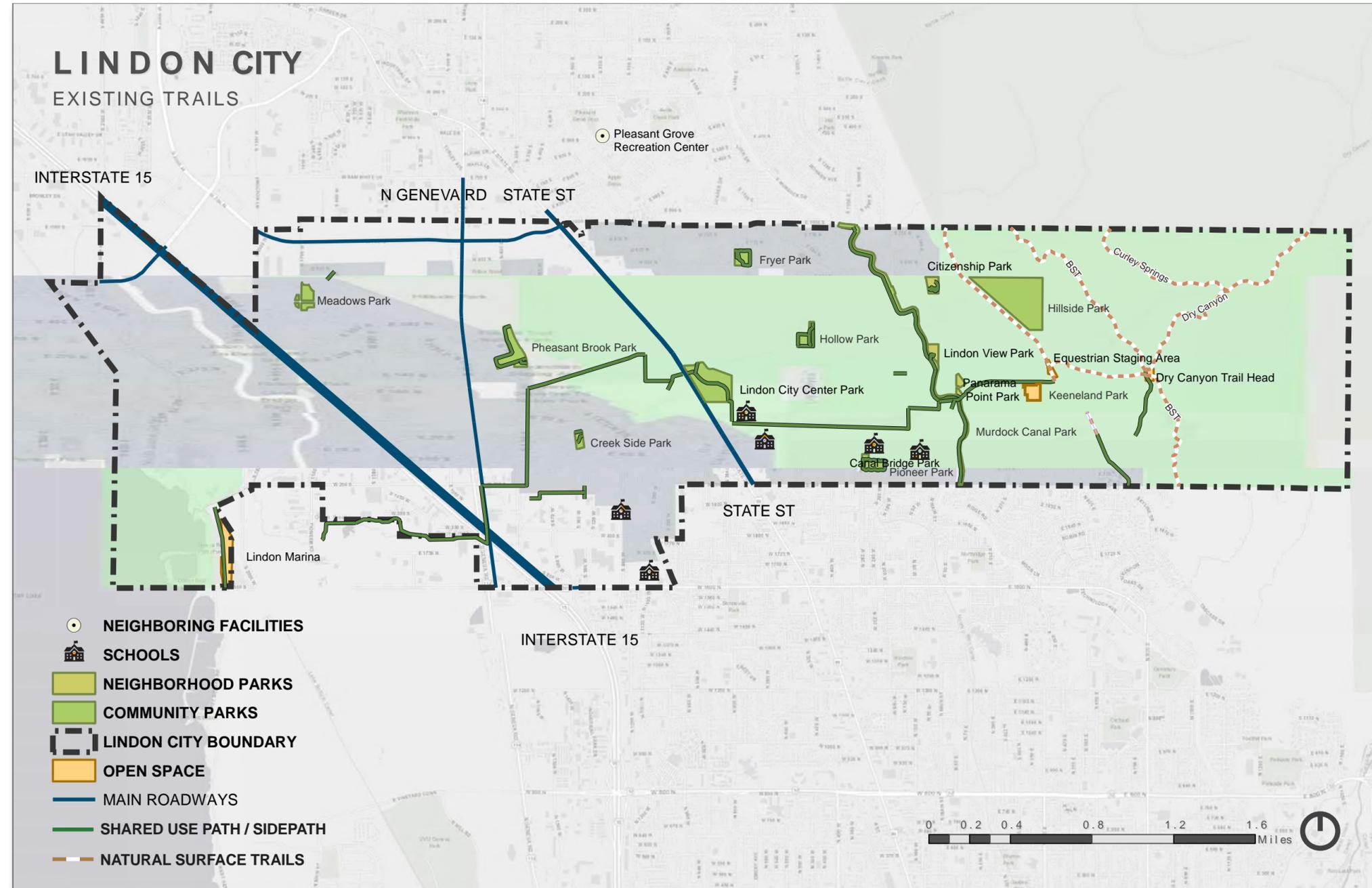
Sidepaths are shared use paths that run parallel to a road in a shared right-of-way. Sidepaths are similar to shared use paths but present challenges at roadway intersections. Many sections of the Heritage Trail are considered to be a sidepath due to their adjacency to the right of way. In areas where a shared use path is needed, but a concrete or asphalt surface is undesirable, crusher fine can be used instead of pavement. These facilities provide a high degree of comfort on long uninterrupted roadway segments, but have operational and safety concerns at driveways and intersections with secondary streets. Crossings should be designed to promote awareness, lower speeds, and facilitate proper yielding of motorists to bicyclists and pedestrians.



NATURAL SURFACE TRAILS

These are unpaved facilities that support a variety of activities, such as hiking, bicycling, running, horseback riding, and dog walking. These trails generally have a tread width of 36-72" and an overall running slope of 10% or less, though up to 15% is acceptable for short segments. The cross slope on a natural surface trail should not exceed 5% to ensure the safety of those using the trail. Natural surface trails can be designed to accommodate a broad or narrow range of users depending on the experience desired. Examples of different type of uses include shared use trails, bike optimized trails, hike optimized trails, service or access roads, and accessible trails. Trails may also be required to serve other utilitarian access functions depending on the underlying property ownership or access agreement.

MAP 2.2 EXISTING TRAIL FACILITIES



EXISTING SHARED USE PATHS

Lindon's two primary shared use paths – the Murdock Canal Trail and the Lindon Heritage Trail – are both paved trails that run on essential north-south and East-west corridors.



MURDOCK CANAL TRAIL

The Murdock Canal Trail runs for approximately 1.6 miles within Lindon's boundaries but for over 17 miles throughout all of Utah County. Built in 2013, it follows a now-enclosed canal. The trail accommodates year-around walking, jogging, equestrian use, cycling, and skateboarding and is well marked with signs approximately every quarter-mile. In addition, the trail can be accessed from numerous trail heads, some of which have restrooms facilities and running water, such as the Lindon View Trail head. Within Lindon, the trail crosses over three roads, each of which are marked with high visibility signage and cross walks.



LINDON HERITAGE TRAIL

The Lindon Heritage Trail runs for approximately 4.5 miles from the base of Mt Timpanogos to I-15. Ultimately, the trail will extend all of the way to Utah Lake. The trail connects neighborhoods to parks and schools throughout the community and gives residents the ability to walk, bike, or horseback ride safely without the presence of motorized traffic. The Heritage Trail also connects to the regional trail system, including the Bonneville Shoreline Trail, the Utah Lake Trail, and the Great Western Trail. The trail does cross over several roads, including Geneva Road, which presents some safety challenges.

EXISTING NATURAL SURFACE TRAILS

Lindon also has several shorter shared use paths that connect neighborhoods to schools and churches or from one cul-de-sac to another. To the east of the City are several natural surface trails that are maintained jointly by the Forest Service, Utah County, and the City of Lindon. These trails are popular outdoor recreation destinations for mountain bikers, hikers, and horseback riders.



Photo Source: www.AllTrails.com

BONNEVILLE SHORELINE TRAIL

The Bonneville Shoreline Trail extends for approximately 2 miles within Lindon's boundaries, but it connects all the way to Brigham City to the north and Spanish Fork to the south, providing essential connections to the entire region. Made up of dirt, gravel, and rock, the Bonneville Shoreline Trail presents a strong opportunity for helping people to access the outdoor recreation amenities that abound. Much of the Bonneville Shoreline Trail and associated natural surface trails are under management by the Forest Service and Utah County.



Photo Source: www.AllTrails.com

DRY CANYON TRAIL

The Dry Canyon Trail is a trail that features towering vertical canyon walls and beautiful meadows that leads to an overlook of Cascade Mountain, Mt Timpanogos, and all of Utah Valley. The trail is made up of dirt, gravel, and several rocky sections, making hiking and biking more treacherous than the Bonneville Shoreline Trail. Like other trails in the area, this trail is managed under the Forest Service, Utah County, and the City of Lindon.

TRAIL PAVEMENT CONDITIONS SCALE

Trail conditions were evaluated on a scale from 1 - 4, with "1" representing poor pavement quality and "4" representing great pavement quality. The pavement condition methodology was based on a visual survey of the number and types of distresses in a section of path. Severity of pavement condition was calculated by the type and extent of existing distresses and distress density was calculated for each type of distress. Through this, pavement condition was properly evaluated to show maintenance recommendations. The different types of pavement distress evaluated included:

- Cracking
- Bumps and sags
- Corrugations
- Depressions
- Edge cracking
- Joining reflections,
- Lane/shoulder drop-off
- Low ride quality
- Patching and utility cut patching
- Potholes
- Rutting
- Swelling
- Weathering

This pavement condition index yields a good indication of the existing pavement condition of a network and informs maintenance prioritization. While pavement conditions were evaluated on this scale, please note that some pavement conditions, such as dirt, gravel, or coarse pavement, are better suited for some user groups over others. Map 2.2, on the following page, shows the pavement conditions on Lindon's trails.



"GREAT" pavement conditions are marked by smooth, unobstructed pavement with none to minimal amount of distress.



"GOOD" pavement conditions are marked by relatively smooth, unobstructed pavement with minimal to minor amounts of distress.

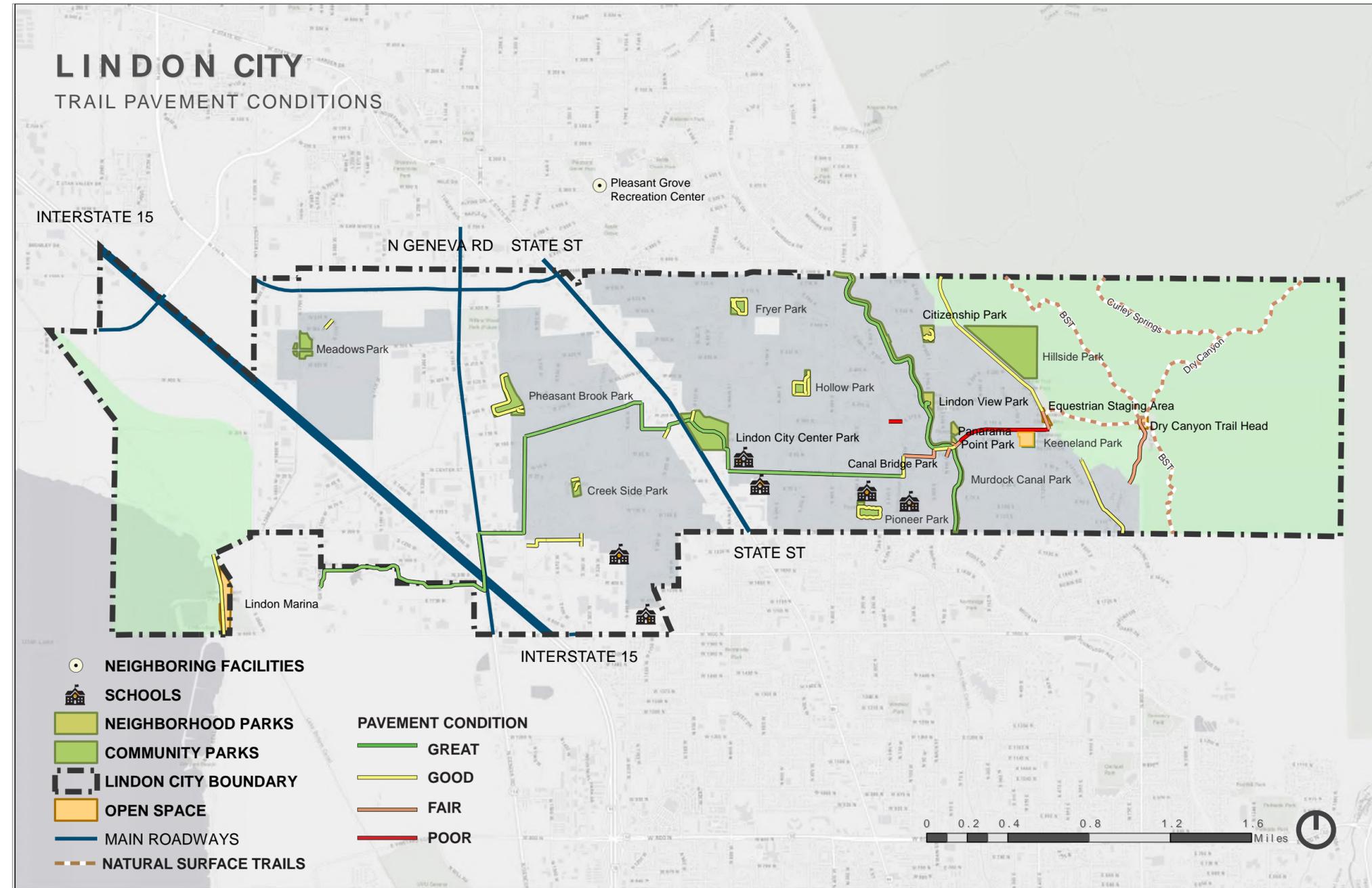


"FAIR" pavement conditions are marked by somewhat smooth and unobstructed pavement with minor to moderate amounts of distress.



"POOR" pavement conditions are marked by rough pavement quality with significant obstructions, such as branches, leaves, and other debris, with moderate to significant amounts of distress.

MAP 2.3: EXISTING TRAIL PAVEMENT CONDITIONS



C. NEEDS ASSESSMENT

To assess needs for new or improved amenities for the City, the consultant team worked with city staff to administer a survey, host an open house, and organize regular steering committee meetings. These activities were paired with a comparative level of service analysis and a coverage analysis using GIS data. Together, this information helped inform the proposed recommendations described in the next section. This section covers the results of each needs analyses.



SURVEY RESULTS

This section provides an overview of a parks, trails, and recreation perception online survey that was available for residents to fill out over the course of two months in late 2019. In total, 213 responses were received, thus making the results not statistically valid, but rather an indication of general thoughts and opinions from engaged residents and community members.

The most beloved recreation facility in the City is the Murdock Canal Trail. It was described as a great place to see neighbors and friends riding bikes, walking, or running. People liked it because it is a great non-auto connector to other neighborhoods and communities, and to the foothills and Provo Canyon. Respondents appreciated the fact that it felt connected to the City, but was also a place for recreating away from traffic. They liked how well it is maintained, allowing multi-season use. Some said their children use it to safely get to school.

Another amenity that proved to be a local favorite is the Lindon Aquatics Center and Pool, followed by Pheasant Brook Park, and Hollow Park. Respondents also expressed appreciation for the City's pickleball Courts.

When asked to rank the following in order of priority, respondents said that Lindon parks, trails, and recreation facilities should prioritize passive uses that are open to the whole community above other options provided.

What are your favorite facilities in Lindon?



1. Murdock Canal Trail - 22%
2. Lindon Aquatics Center - 11%
3. Pheasant Brook Park - 8%
4. Hollow Park - 8%
5. Pickleball Courts - 7%
6. Other - 44%

Rank the Following:



What would you like to improve regarding Lindon's parks, trails, and recreation facilities?



OPEN HOUSE

Under the direction of the Steering Committee, the Planning Team recommended a current best practice in community engagement by meeting "people where they are", to increase public engagement by members of the community not typically involved in the public process. This term refers to conflating public engagement processes with already existing community events to increase participation quantity and quality by engaging a wider cross section of the community demographic profile. After reviewing upcoming community events with City staff, the Tree lighting event held at the Community Center on Monday December 2nd, seemed to be the best opportunity for timely feedback to inform the PTRMP Plan Development. Careful coordination with the project Steering Committee and Lindon City Parks staff allowed for notifications to be sent out via social media, utility billing and the community non-emergency text line.



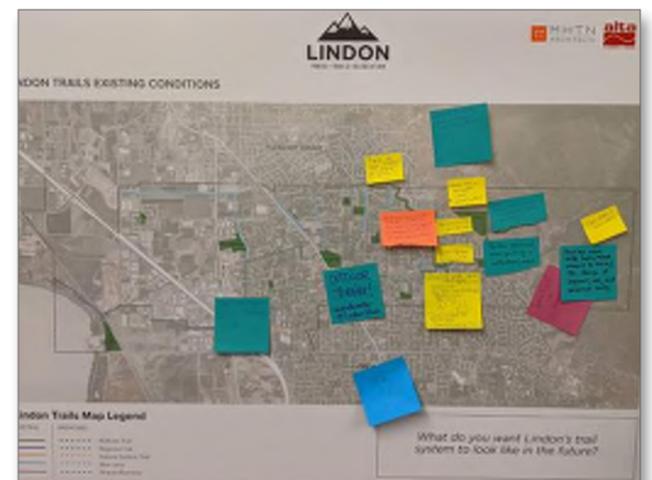
Children at open house drafting up their desired parks



Tree Lighting Festival during the open house

The result of this approach of "meeting people where they are" was highly successful and impactful on the outcomes of the PTRMP Plan. Eight stations were assembled inside the Community Center to engage with Lindon residents before and after the tree lighting ceremony. The stations included overview of the plan development process, draft guiding principles for the plan, existing conditions of parks and recreational opportunities, existing conditions and opportunities for community recreational trails, and several opportunities for open comments of future desires for the PTRMP plan.

By integrating public engagement into a previously existing community event - the Community Tree Lighting - the turnout was significantly higher including over 200 residents, which included representatives from groups who don't normally participate. The event also created the opportunity for significant involvement by children and youth utilizing coloring pages at youth-focused station.



Response from Lindon residents at the open house

There was an overwhelming response from Lindon residents for additional open space. There was an emphasis on the need for improvements and maintenance. Popular votes favored mountain bike and road bike facility improvements. There seemed to be an even split between those who did and did not want more equestrian facilities. Another request by several residents was an opportunity to have exercise opportunities besides walking and jogging.

E. DEFICIENCIES AND GAPS IN THE FACILITIES

The following analysis compares the current Lindon City supply of facilities and amenities and the City's desired supply to Metrics supplied by the National Parks and Receptions Association (NRPA) for comparable cities with a populations under 20,000 (Table 2.5). Lindon's desired supply of amenities and facilities exceeds the median supply in all categories with the exception of tennis courts. Deficiencies exist between the City's existing supply and the desired supply in all categories except Softball/ baseball fields, Swimming pools, Splash pads, Equestrian Arenas and Pavilions.

By the year 2030 and with the projected population increase, with no further development, the deficiencies either stay the same or increase in all categories that currently have deficiencies. By 2030 the category of pavilions will be added to the list of deficiencies. The categories of Pickleball courts and Trails (Paved miles) are not found in the NRPA metrics. The NRPA Metrics provide a good comparison of what other cities across the US provide in terms of parks and recreation facilities and amenities.

TABLE 2.5. EXISTING LINDON CITY RECREATION FACILITIES WITH CURRENT 2020 NEEDS AND PROJECTED 2030 DESIRED NEEDS

Facility/ Amenity	Existing Supply	Median Supply per the NRPA Survey for cities with a population of less than 20,000(1 Per Population of)	Lindon City Desired Supply (1 Per Pop of)	2020 Lindon City Desired Supply based on Current Population of 11,512	Deficiency or Surplus	2030 Lindon City Desired Supply based on Projected Population of 13,046	Deficiency or Surplus
Softball / Baseball Fields	4	3,378	2,500	5	-1	5	-1
Soccer / Football / Lacrosse	7	3,875	2,500	5	2	5	2
Indoor Recreation Facility	0	9,250	5,000	2	-2	3	-3
Basketball	2	3,975	3,000	4	-2	4	-2
Pickle Ball Courts	6		2,000	6	0	7	-1
Tennis	3	2,500	3,000	4	-1	4	-1
Volleyball (Sand)	0	5,000	10,000	1	-1	1	-1
Swimming Pools	1	8,586	10,000	1	0	1	0
Splash Pad	1		10000	1	0	1	0
Skate Park/ Pump Track	0	10,234	10,000	1	-1	1	-1
Trails (Paved Miles)					0		0
Equestrian Arena	1	8786	10000	1	0	1	0
Picnic Tables	105		105	110	-5	124	-19
Restrooms	7		1500	8	-1	9	-2
Fitness Equipment	0		5000	2	-2	3	-3
Playgrounds	11	2269	850	14	-3	15	-4
Pavilions	13		850	14	-1	15	-2
Amphitheater	0	10430	10000	1	-1	1	-1

FACILITIES THAT SURPASS THE 2020 SUPPLY

- Soccer/ Football/ Lacrosse

FACILITIES THAT MEET THE 2020 SUPPLY

- Pickle-ball Courts
- Swimming Pools
- Splash Pad
- Trails (Paved Miles)
- Equestrian Arena
- Pavilions

FACILITIES THAT DO NOT MEET THE 2020 SUPPLY

- Softball/ Baseball Fields
- Indoor Recreation Facilities
- Outdoor Basketball Courts
- Tennis Courts
- Volleyball courts (Sand)
- Skate Park/ Pump Track
- Picnic Tables
- Restrooms
- Fitness Equipment
- Playgrounds
- Amphitheater

FACILITIES THAT SURPASS THE 2030 DESIRED SUPPLY

- Soccer/ Football/ Lacrosse

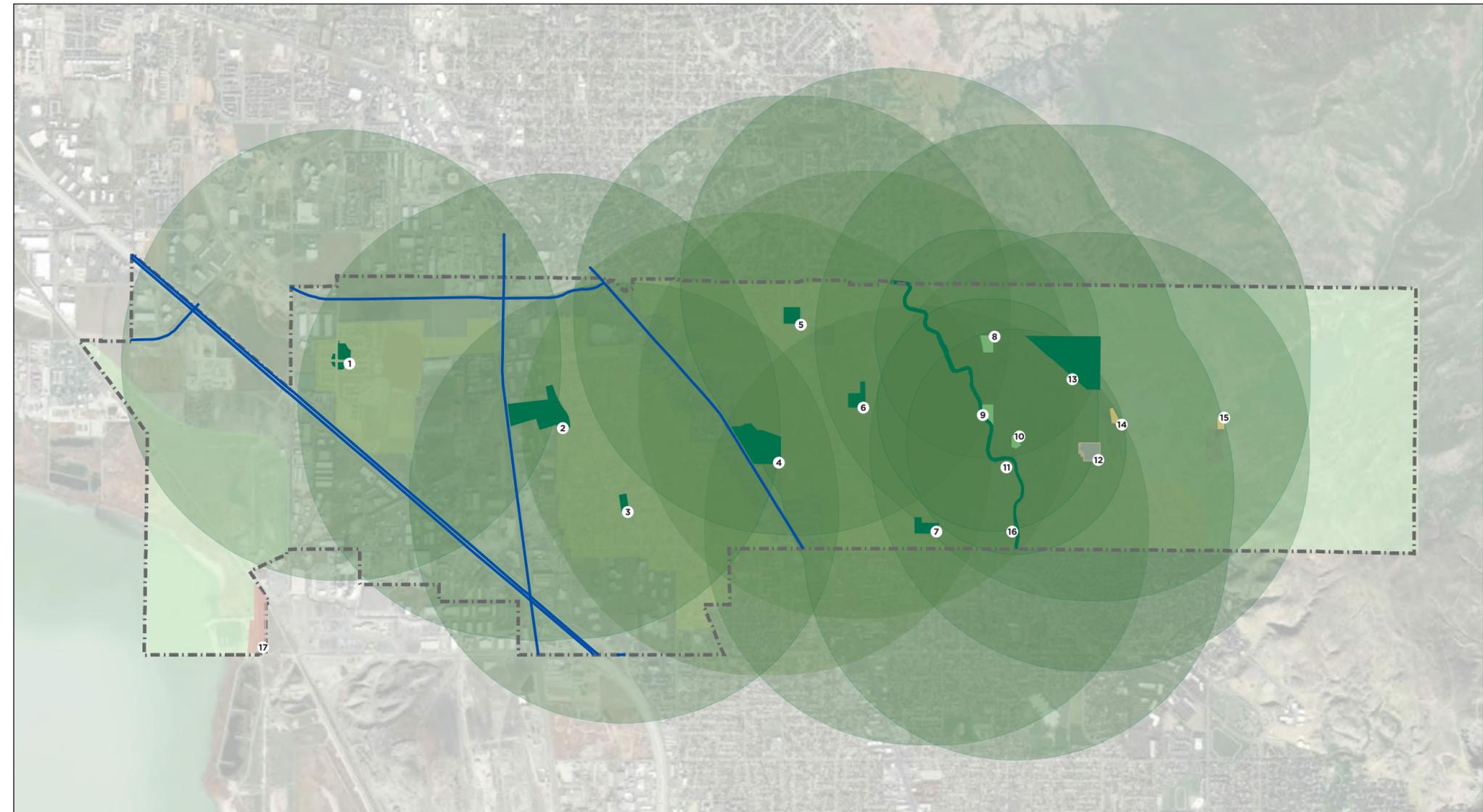
FACILITIES THAT MEET THE 2030 DESIRED SUPPLY

- Pickle-ball Courts
- Swimming Pools
- Splash Pad
- Trails (Paved Miles)
- Equestrian Arena

FACILITIES THAT DO NOT MEET THE 2030 SUPPLY

- Softball/ Baseball Fields
- Indoor Recreation Facilities
- Outdoor Basketball Courts
- Tennis Courts
- Volleyball courts (Sand)
- Skate Park/ Pump Track
- Picnic Tables
- Restrooms
- Fitness Equipment
- Playgrounds
- Pavilions
- Amphitheater

MAP 2.4 COVERAGE ANALYSIS



- NEIGHBORHOOD PARK
- COMMUNITY PARK
- Trail head
- FUTURE PROPOSED PARK
- LINDON CITY BOUNDARY
- ROADS & HIGHWAYS
- RESIDENTIAL LAND USE
- OPEN SPACE
- COMMUNITY PARK 1-MILE BUFFER
- NEIGHBORHOOD PARK 1/2 MILE BUFFER

- 1 MEADOW PARK
- 2 PHEASANT BROOK PARK
- 3 CREEKSIDE PARK
- 4 LINDON CITY CENTER PARK
- 5 FRYER PARK

- 6 HOLLOW PARK
- 7 PIONEER PARK
- 8 CITIZENSHIP PARK
- 9 LINDON VIEW PARK
- 10 PANORAMA PARK

- 11 CANAL BRIDGE PARK
- 12 KEENELAND PARK
- 13 HILLSIDE PARK
- 14 EQUESTRIAN STAGING

- 15 DRY CANYON TRAIL HEAD
- 16 MURDOCK CANAL PARK



03 | RECOMMENDATIONS, GOALS, AND POLICIES

A. PROPOSED IMPROVEMENTS

PARKS AND OPEN SPACE

An analysis of the existing park system and the City's desired level of service, this chapter presents proposed additions and improvements to the City's Parks, Trails and Recreation Facilities.

The established functional level of service is a minimum of 4 acres of parks, trails and recreation facilities for every 1000 residents as suggested in the 2011 General Plan. The City of Lindon easily exceeds this level of service at approximately 10 acres per 1000 residents. The minimum level of service is based upon the improvements to the existing city owned land, land under a development agreement and a future park acquisitions on the eastern bench.

Future Community Parks

Geneva Resort Park - The City owns 7.54 acres of property adjacent to the Lindon Marina. This property will be improved at some point in the future to accommodate a sports complex.

Anderson Farms Park - The construction of this future park is imminent and is part of a development agreement with the Land Developer. The size of the agreed upon park parcel is 10.2 acres and the park will provide the following amenities agreed upon in the development agreement including soccer and softball fields, pickleball and basketball courts, pavilions play structures and swings and generous landscaping, lighting and other site furnishings.

Keeneland Park - The City has identified the need to create a 3 to 5 acre park facility in the large open space on the east bench above the Murdock Canal Trail to accommodate the recreational needs of the residents in this area of town. Lindon will continue to work with the property owners to acquire a parcel of acreage for the future park.

Cook Property- The city has identified the need to purchase 4.58 acres of property along Lake View road to accommodate the recreational needs of the residents in this area of town. Lindon will continue to work with the property owners to acquire this parcel of acreage for a future park.

Hutchinson Family Property- The City has identified the need to purchase 4.62 acres of property adjacent to Hollow Park to accommodate the recreational needs of the residents in this area of town. Lindon will continue to work with the property owners to acquire this parcel of acreage for addition to Hollow Park.

Lindon Village Center- In 2020, Lindon City will be working with an urban design consultant to create a new character district, Lindon Village Center. This area will be developed through public and private funds to create a mid-rise development centered around a park and plaza public space. Well connected, this walkable, urban space will be accessible through private and public, alternative modes of transportation. The gathering space will benefit, not only the users of the space, but the commercial area as it will foster a thriving and successful economic center by providing a pleasant and attractive space for users to visit and linger in.

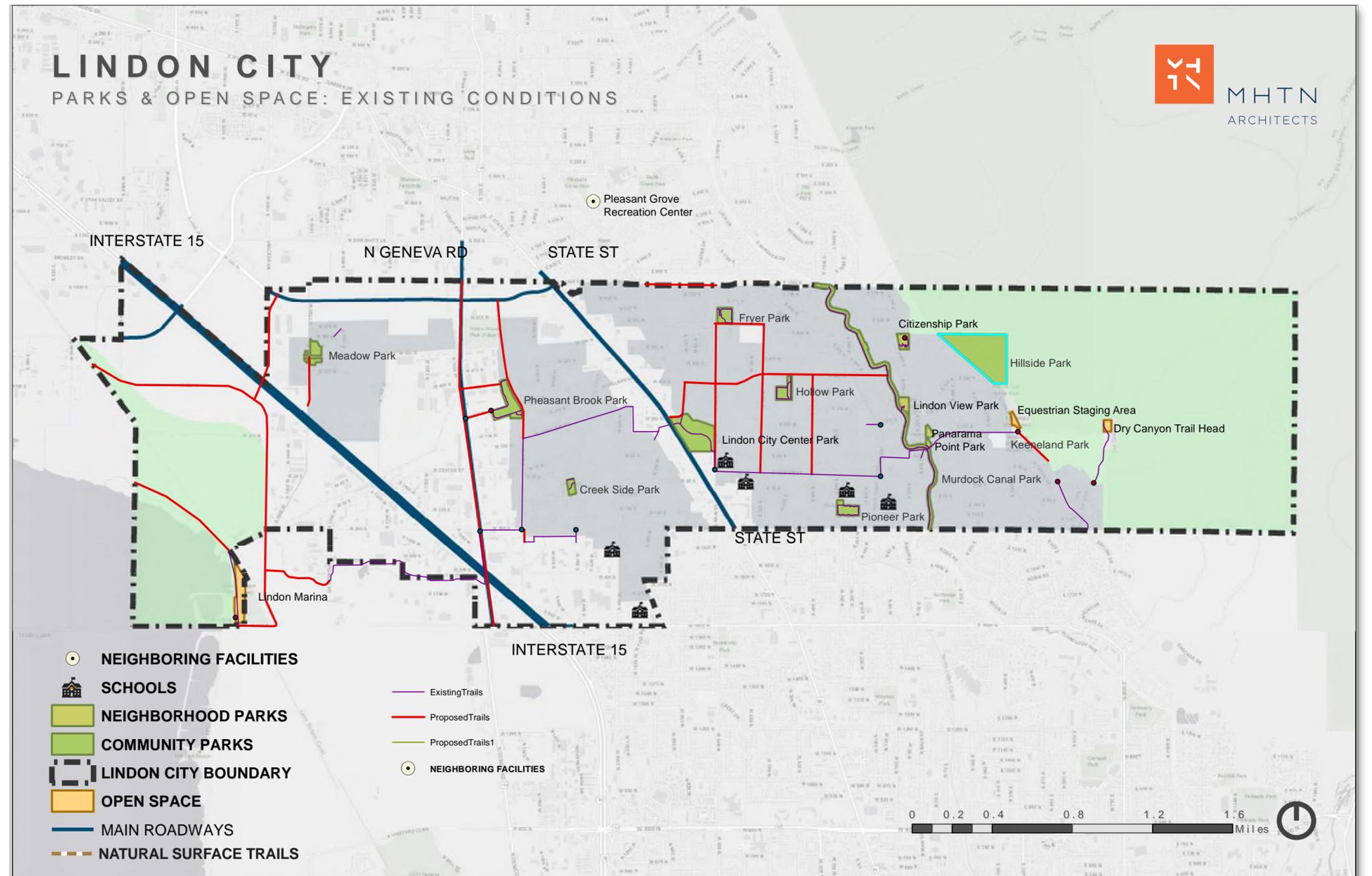
The addition of these parks meets the city's desire to maintain the minimum level of service for the future projected population size of 12,491 residents in the year 2029.

PARKS AND RECREATION AMENITY ENHANCEMENTS

During the PTRMP update, over 250 responses were received. Comments received during the Community Open House confirmed many of the survey results. Due to the level of service provided to the City Residents by the existing and proposed park and trail facilities, the City focus should consist of providing additional amenities and possible niche type amenities in the proposed and existing parks.

The park survey found that residents will travel short distances to the existing neighborhood and community parks in the City of Lindon and will also travel to community and regional parks within the County searching for specific amenities not provided by Lindon City due to the city size and cost factors. The niche type amenity is described as a desired use not found in the City and adjacent communities that will provide recreational opportunities and uses that keep Lindon citizens in the City confines. The opportunities may include recreational facilities including an indoor field-house to support additional year-round recreational programming and a pump track to expand cycling opportunities within the City.

MAP 3.1 EXISTING AND FUTURE PARKS & TRAILS



PARKS RECOMMENDED FACILITY TYPES

SHADE STRUCTURES/TREES



The community residents overwhelmingly request additional shade thru the use of both shade structures and additional large canopy shade trees. The use of shade structure over playground structures will protect users from excess heat on the structures themselves on hot summer days when the use of playgrounds may be at its peak level.

The use of shade structures should be limited to the high use park playgrounds due to cost impacts. The addition of large shade trees should be considered in the near future due to low material costs.

PARKING



Additional parking may provide additional use of all system parks. Parking improvements at Hillside Park can improve access to the Bonneville Shoreline Trail system.

A LARGE ALL ABILITIES PLAYGROUND



An all abilities playground is defined as a playground is a space that provide inclusive play opportunities for children of all ages and abilities. These types of playgrounds are designed to promote the healthy development of all children’s physical, social cognitive and sensory abilities.

It is proposed that these playgrounds be constructed in Hollow Park, Pheasant Brook Park and Pioneer Park in order to provide this amenity in several strategic locations across the city.

SIGNAGE



Signage opportunities will assist City residents in locating all of the City Parks. Some City survey respondents indicated that they were not aware of all the City Parks in the Park System. Signage costs can be prioritized over several years and should be provided at Hillside, Citizenship and the future Geneva Resort Park.

ALL AGES PLAYGROUND/EXERCISE EQUIP.



Another request by a majority of the mature residents at the open house was the installation of simple inground exercise equipment that could be used all ages. This equipment will accentuate additional fitness in conjunction with the existing park trails. Possible park locations for this equipment are Lindon View Park, Pheasant Brook Park and the City Center Park with trail access.

ADA ACCESSIBILITY



A high Priority for the City is to provide accessibility to existing Park Amenities. In many instances, the cost of providing ADA accessibility is reasonably inexpensive and provides the greatest potential amount of use by all city residents in a fair and equitable way. Potential Park locations already noted by the City are: Hollow Park, playground access; City Center Park, playground, pavilion and equestrian facilities; Meadow Park, playground access; Panorama Park, playground access; Pheasant Brook Park, playground access; Pioneer Park, playground access and Citizenship Park, playground access.

UPDATE OLD PLAYGROUNDS



Several parks have older or small outdated playground equipment. While useful, this equipment does not provide the more social active playground activities provided by more modern equipment. Possible park locations may include Citizenship Park, Panorama Park and Creekside Park.

FRISBEE GOLF COURSE

Another low cost amenity requested by residents in the City-wide Survey and the City Open House was frisbee golf. These courses could be provided along the existing trails in Hillside Park and Citizenship Park.

PUMP TRACKS

A potential unique amenity that could be provided by the City is a Pump track. A pump track was request by several residents. A pump track is a circuit of rollers, bank turns and features to be ridden by riders using on mountain bikes. A pump track could be as small as one third of acre to one half acre. Several potential locations for pump tracks are along the Murdock Canal Trail or in Lindon View Park or Hillside Park.

PICKLEBALL COURTS

Pickleball is becoming a very popular sport as of late and is increasing in popularity around the state and country. The City is planning to construct new Pickleball Courts in Creekside Park and Pheasant Brook Park within the next two years.

TRAILS RECOMMENDED FACILITY TYPES

SHARED USE PATHS



Shared use paths provide a travel area separate from motorized traffic for bicyclists, pedestrians, skaters, wheelchair users, joggers, and other users. Shared use paths are desirable for bicyclists of all skill levels preferring separation from traffic. Shared use paths should generally provide directional travel opportunities not provided by existing roadways. Most shared use paths are designed for two-way travel. Shared use paths along roadways are called sidepaths.

SIDEPATHS



Sidepaths provide a high degree of comfort on long uninterrupted roadway segments, but have operational and safety concerns at driveways and intersections with secondary streets. Crossings should be designed to promote awareness, lower speeds, and facilitate proper yielding of motorists to bicyclists and pedestrians.

WAYFINDING



Wayfinding gives trail users direction and understanding of where they are. Wayfinding signage will point out where a person is, where useful destinations are, and it provides a sense of place for the community.

CROSSWALK IMPROVEMENTS



Improving crosswalks is key to encourage non-motorized travel. Safer crosswalks for sidewalks and trails improve the connectivity throughout the city, allows the user more ease and enjoyment when using the facilities, and provides a safer place for all the residents of the community. Improving the crosswalk safety can be done with the addition of light, (re) striping the crosswalk, and pulling out the curb to increase pedestrian visibility.

TRAIL HEAD ENHANCEMENTS



Having good trail heads can increase the trail usage. Good facilities such as lighted parking, bathrooms, and drinking fountains will invite the public to use the trail head more.

B. PRIORITIZATION OF PROJECTS

To ensure the most efficient use of funds, project prioritization is prudent. This is meant to facilitate selecting which parks need facility upgrades the most and avoids overlap in existing conditions. The suggested projects are broken down into four main topics which are listed below:

1. FOCUS FUNDING ON PARKS AND RECREATION AMENITY ENHANCEMENTS
2. PROVIDE FACILITY SERVICES YEAR-ROUND
3. INVEST IN IMPROVED TRAIL CONNECTIVITY
4. PROMOTE FULL EXTENT OF CITY PARKS, TRAILS AND RECREATION OPPORTUNITIES

1. FOCUS FUNDING ON PARKS AND RECREATION AMENITY ENHANCEMENTS

SHADE STRUCTURES/TREES



- Fryer Park
- Hollow Park
- Meadow Park
- City Center Park
- Creekside Park
- Pheasant Brook Park
- Pioneer Park
- Citizenship Park

ALL ABILITIES PLAYGROUND



- Hollow Park
- Pheasant Brook Park
- Pioneer Park

ALL AGES PLAYGROUND/EXERCISE EQUIPMENT



- City Center Park
- Lindon View Park
- Pheasant Brook Park

UPDATE OLD PLAYGROUNDS



- Citizenship Park
- Creekside Park
- Panorama park

ADA ACCESSIBLITY



- Hollow Park
- City Center Park
- Meadow park
- Pheasant Brook Park
- Pioneer Park
- Citizenship Park

SIGNAGE



- Hillside Park
- Citizenship Park

PARKING



- Hillside Park

FRISBEE GOLF COURSE



- Citizenship Park
- Hillside Park

PUMP TRACKS



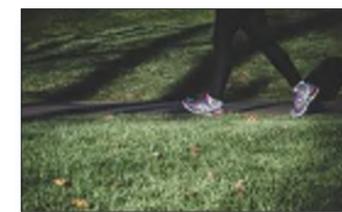
- Hillside Park
- Lindon View Park

PICKLEBALL COURTS



- Creekside Park
- Pheasant Brook Park

RUNNING PATH/PARK TRAIL



- Meadow Park

2. PROVIDE FACILITY SERVICES YEAR-ROUND

KEEP SELECT RESTROOMS OPEN ALL YEAR



- City Center Park
- Lindon View Park
- Others adjacent to trails

CONSIDER KEEPING DRY CANYON TRAIL ROAD OPEN



CONSIDER PROVIDING ADDITIONAL INDOOR FACILITIES LIKE A FIELD HOUSE



3. INVEST IN IMPROVED TRAIL CONNECTIVITY



Photo Source: www.AllTrails.com

Improving trail connectivity enhances the residents' ability to get the facilities with more ease. This would increase park usage.

4. PROMOTE FULL EXTENT OF CITY PARKS, TRAILS, AND RECREATION OPPORTUNITIES



ON WEBSITE
Updating the website to include the most recent information will help the residents navigate and be up to date with what is available to them.



SOCIAL MEDIA CAMPAIGN
Social media campaigns are a fun and modern way to reach audiences of all ages about recreational amenity offerings, as well as events and activities.



ACTIVITIES CAMPAIGN
An activities campaign will get the residents of Lindon to the parks and using them, thus, raising public knowledge and interaction with the facilities.

DIVERSIFICATION OF OFFERINGS

The core of the recommendations of this Plan is to take a holistic approach to the concept of recreation, by offering a wide variety of recreational offerings throughout Lindon City. The diagram below depicts this concept by showing that the Parks and Recreational offerings in Lindon should seek to provide a variety of active and passive uses, as well as educational and enjoyment driven for Lindon residents across all ages, interests and abilities.

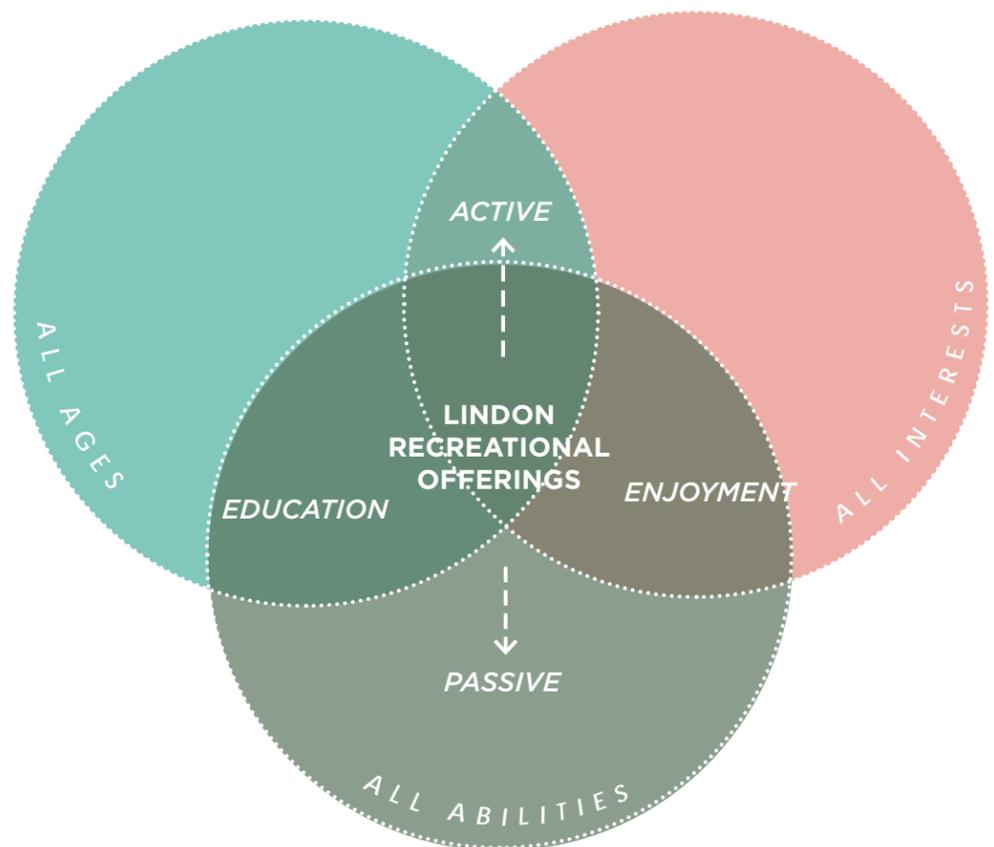


TABLE 3.1: PROPOSED PARK RECOMMENDATIONS

Park Type	Park Name	Recommendations	Acres	Estimated Cost
Community	Lindon City Center Park	Large Shade Structure, ADA Walks, Ramps, Seating	11.3	\$287,00
Neighborhood	Creekside Park	Trees, Medium Upgraded Play Structure, Small Upgraded Play Structure, 620 lineal ft of Upgraded Trail Paving	2	\$214,900
Community	Hillside Park	1 Tree, Frisbee Golf Set (9 pieces), 850 lineal ft of Un-lit Paved Trail, 4 Signs, Restrooms, 10 Parking Stalls, Small Pavilion, Picnic Tables	31.66	\$370,00
Community	Hollow Park	Trees, Frisbee Golf Set (4), ADA Access to Playground (2)	4.46	\$724,000
Community	Meadow Park	Large Shade Structure, 20 Trees, Frisbee Golf Set (9), 1,050 lineal ft of Lit Paved Trail, ADA Access to Playground, Fitness Equipment (3 Small), Medium Equipment	4.16	\$231,100
Community	Lindon View Park	Pump Track (Dirt), Fitness Equipment (1 Large)	2.26	\$272,000
Community	Fryer Park	Medium Shade Structure, 2 Signs, Basketball Court	4	\$84,000
Neighborhood	Panorama Park	Small Upgraded Play Structure, 380 lineal ft of Un-lit Paved Trail, Small Pavilion	1.17	\$82,600
Neighborhood	Citizenship Park	2 Medium Shade Structures, Trees, Frisbee Golf Set (7), 2 Small Upgraded Play Structure, ADA Walks, Ramps and Seating (2), 2 Signs, ADA Access to Playground (2), Fitness Equipment (Medium)	2.67	\$284,500
Community	Pheasant Brook Park	2 Medium Shade Structure, Trees, ADA Access to Playground (1), Fitness Equipment (Large), Update Parking Lot (106 Stalls), All Abilities Playground, Baseball outfield fencing, Baseball field Lighting, Drinking Fountain, Bleachers, Picnic Tables	9.78	\$855,500
Community	Pioneer Park	1 Large Shade Structure, Frisbee Golf Set (3), 1,510 lineal ft of Upgraded Trail Paving, ADA Walks, Ramps and Seating (2), ADA access to Playground, Fitness Equipment (Large), Update Park Lot (68 Stalls), All Abilities Playgorund	5.3	\$367,200

TABLE 3.2: UNIT COSTS

Unit Costs			
Name	Units	Cost Per Unit	Total Investment
Size (acres)	91.4	201,435	\$18,411,159.00
Restrooms	6	150,000	\$900,000.00
Pavilions- small	4	30,000	\$120,000.00
Pavilions- Medium	6	75,000	\$450,000.00
Pavilions- Large	3	100,000	\$300,000.00
Picnic Tables	105	1,000	\$105,000.00
Playgrounds	11	100,000	\$1,100,000.00
Exercise Equipment	0	9,000	\$0.00
Pickle Ball Courts	6	50,000	\$300,000.00
Tennis Courts	3	75,000	\$225,000.00
Basketball Courts	2	20,000	\$40,000.00
Soccer Fields	7	500	\$3,500.00
Ball Diamonds	4	250,000	\$1,000,000.00
Sand Volleyball	0	25,000	\$0.00
Riding Arena- sf	27,243	2.25	\$61,296.75
Lighted Paths	3,850	50	\$192,500.00
Paved Paths	1,884	47	\$88,548.00
Regional Trail Access	0	200,000	\$0.00
Asphalt (parking) sf	63,530	5	\$317,650.00
Mowed Acres ***	1,770,833	2	\$3,541,666.00
TOTAL PARKS			\$27,156,319.75

TABLE 3.3 PROPOSED TRAIL RECOMMENDATIONS

Facility Type	Location	Facility Notes	Distance (mi)	Estimated Cost
Shared Use Path	Heritage Trail from Pioneer Lane to Lindon Marina	Finish last section of the Lindon Heritage Trail.	0.9	\$638,680
Shared Use Path	Geneva Rd Rail-with-Trail from northern to southern city lines	Shared use path along the Geneva Road rail corridor. Once entirely completed, this project will provide regional connectivity through Utah County.	1.8	\$1,331,960
Shared Use Path	Utah Lake Shoreline Trail from Lindon Marina to city line	Continuation of the Utah Lake Shoreline Trail.	0.6	\$448,560
Sidepath	2000 W from Heritage Trail to 700 N	Shared use path along 2000 W from the Heritage Trail to 700 N.	1.8	\$939,120
Sidepath	800 W from 700 N to Lakeview Rd	Shared use path along 800 W from 700 N to Lakeview Rd.	0.8	\$584,520
Shared Use Path	Park connector from Geneva Rd to Pheasant Brook Park	Shared use path connecting the future Geneva Road Rail-with-Trail to Pheasant Brook Park.	0.2	\$116,480
Shared Use Path	Park connector from Geneva Rd to Pheasant Brook Park	Shared use path connecting the future Geneva Road Rail-with-Trail to Pheasant Brook Park at UDOT's future traffic signal.	0.2	\$147,840
Sidepath	Anderson Farms	Neighborhood sidepaths in Ivory Home's new Anderson Farm Development.	0.2	\$147,840
Sidepath	135 W from City Center Park to 400 N	Sidepath connecting the City Center Park to northern neighborhoods.	0.2	\$147,840
Sidepath	Main St from Center St to 600 N	Sidepath connecting the City Center to northern neighborhoods and Fryer Park.	0.8	\$591,360
Sidepath	400 N from 135 W to the Murdock Canal Trail	Sidepath connecting western neighborhoods to the Murdock Canal Trail.	1.1	\$813,120
Sidepath	200 E from Center St to 400 N	Sidepath connecting southern neighborhoods to northern neighborhoods.	0.8	\$591,360
Sidepath	400 E from Center St to 400 N	Sidepath connecting southern neighborhoods to northern neighborhoods.	0.5	\$369,600

Cost per 10' wide asphalt shared used trail.....\$140 per linear ft.
 Cost per 10' wide asphalt side path.....\$140 per linear ft.

TABLE 3.4 PROPOSED SPOT IMPROVEMENTS

ID	Facility Type	Location	Facility Notes	Estimated Cost
1	Trail Crossing	Heritage Trail and Geneva Road	Add curb extensions and crosswalk with warning signage and yield lines	\$15,328
2	Trail Crossing	Heritage Trail and 200 South	Install crosswalk with warning signage and yield lines. Look into possibility of intersection reconfiguration, including implementation of roundabout.	Varies.
3	Enhanced Crosswalk	Neighborhood Connector Trail and 200 South	Add curb extensions to shorten crossing and install high visibility crosswalk	\$15,328
4	Trail Crossing	Pheasant Brook Park and future Rails-with-Trail facility	Install hybrid beacon or traffic signal in conjunction with future trail development	\$120,500
5	Trail Crossing	Heritage Trail and Center Street (at City Center Park)	Add curb extensions to shorten crossing	\$14,000
6	Trail Crossing	Heritage Trail and Center Street (at Canal Drive)	Add curb extensions to shorten crossing	\$14,000
7	Trail Crossing	Neighborhood Connector Trail and Canal Drive	Add curb extensions and crosswalk with warning signage and yield lines	\$121,828
8	Regional Trail head	Lindon Marina / Geneva Resort	Enhance existing amenities, including improving parking area's connection to the trail and adding maps and signage.	\$1,328
9	Regional Trail head	Pheasant Brook Park	Enhance existing amenities, including improving parking area's connection to the trail and adding maps and signage.	\$1,328
10	Trail Access	Citizenship Park	Develop natural surface connection from park to the Bonneville Shoreline Trail. Expand parking by 2 additional spots.	\$11,920
11	Trail head	Dry Canyon Winter Gate	Develop 2-5 parking spaces for winter access. Implement trail signage, including maps and directional signs	\$8,660
12	Trail Access	Bonneville Shoreline Trail Neighborhood Access	Develop 2-5 parking spaces for trail access.	\$10,000
13	Trail head	Equestrian Staging Area	Enhance existing trail head amenities, including adding restrooms, signage, and increased equestrian amenities.	\$151,328

Cost per pedestrian hybrid beacon..... \$25,000 each
 Cost per thermoplastic x-walk marking..... \$30 per sq. ft.
 Cost per curb extension..... \$4,000 per corner
 Cost per restrooms..... \$150,000 per building
 Cost per parking..... \$2,000 per stall
 Cost per sign panel..... \$21 per sq. ft.
 Cost per steel sign post..... \$20 per linear ft.

C. STANDARDIZED DEFINITION, CLASSIFICATIONS, AND GUIDELINES FOR ALL PARK AND TRAIL TYPES

IMPLEMENTATION

Implementation strategies for trails projects require a blend of careful planning and opportunistic decision making. On-street projects, like sidepaths, can often be implemented quickly and efficiently when coordinated with planned roadway projects. Conversely, shared use path projects may require more extensive easement negotiations, permitting, or fundraising to reach construction. This section outlines a brief, planning-level analysis of trail design standards, project cost estimates, and potential funding sources and strategies for developing the recommending network.

Design Guidelines

Trails are one of the primary ways in which people can truly experience a city. If carefully planned and sustainably constructed, these recommended trails will promote an enjoyable user experience and minimize future maintenance requirements. These design guidelines specify how trails and supporting facilities should be designed and constructed within the City of Lindon. The following standards and guidelines are referred in this guide:

- The Federal Highway Administration's (FHWA) Manual on Uniform Traffic Control Devices (MUTCD) defines the standards to install and maintain traffic control devices on all public streets, highways, bikeways, and private roads open to public traffic.
- FHWA's Small Town and Rural Multimodal Networks (2016) document is a design resource and idea book to help small towns and rural communities support safe, accessible, comfortable, and active travel for people of all ages and abilities.

Shared Use Paths Design Guidelines

Shared use paths provide a travel area separate from motorized traffic for bicyclists, pedestrians, skaters, wheelchair users, joggers, and other users. Shared use paths are desirable for bicyclists of all skill levels preferring separation from traffic. Shared use paths should generally provide directional travel opportunities not provided by existing roadways. Most shared use paths are designed for two-way travel. Shared use paths along roadways are called sidepaths.

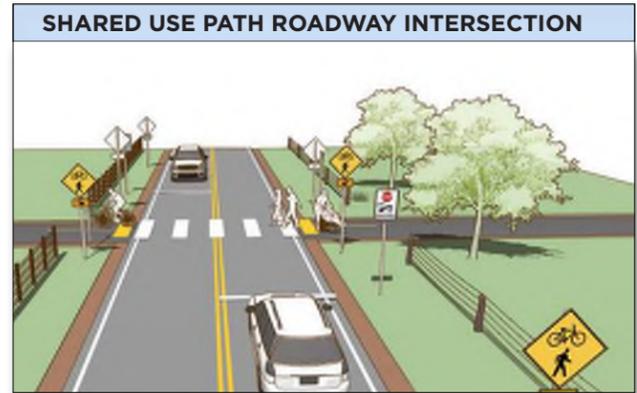
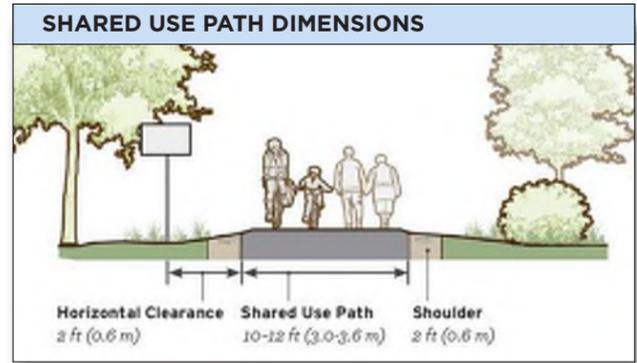
Typical application

- Shared use paths are typically located in independent rights of way, separate from roadways
- In utility corridors, such as powerline and sewer corridors
- In waterway corridors, such as along ditches, drains, canals, streams, and rivers

Design Features

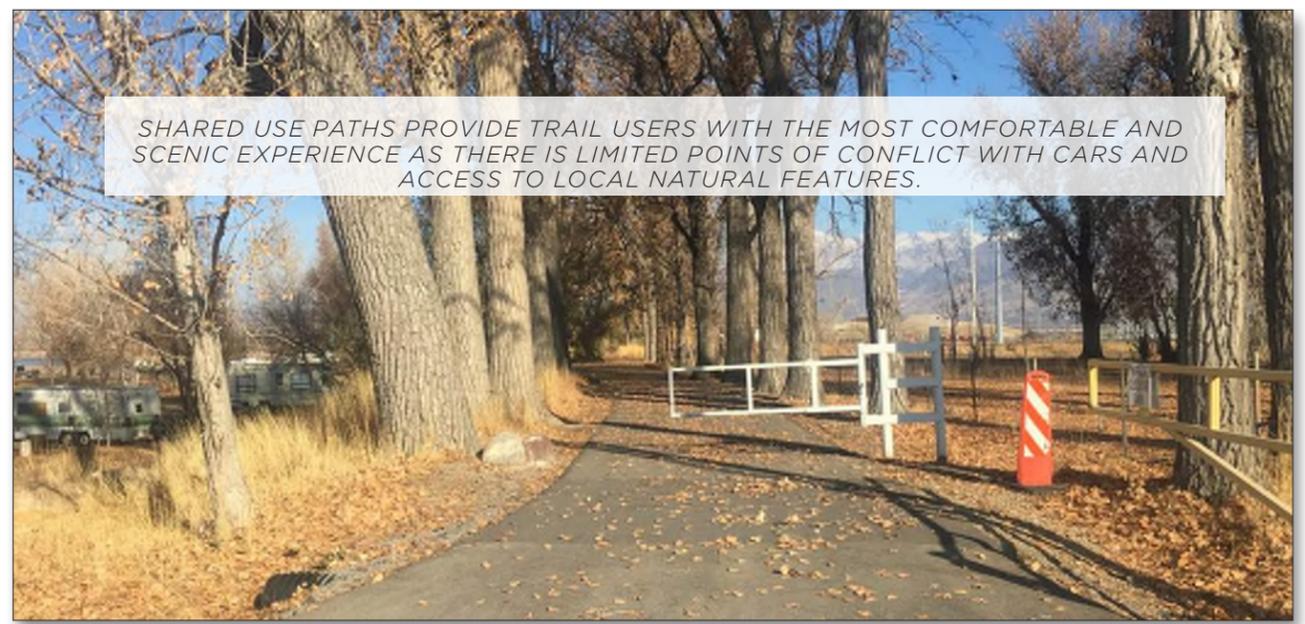
- Recommended minimum 10' width to accommodate moderate usage (14' preferred for heavy use). Minimum 8' width for low volume solutions only.
- A 2'ft or greater shoulder on both sides of the path should be provided free of obstacles. An additional foot of lateral clearance, for a total of 3 ft, is required by the MUTCD for the installation of signage or other furnishings.
- Standard clearance to overhead obstructions should be 10'

For estimated shared use path costs, see Table 3.5 on page 20.



Further Considerations

- Under most conditions, centerline markings are not necessary. Centerline markings should only be used for clarifying user positioning or preferred operating procedure.
- Where there is a sharp blind curve, painting a solid yellow line with directional arrows reduces the risk of head-on collisions.
- Short sections of centerline are recommended upon the approach to street crossings to channelize path users.
- Small scale signs should be used in path environments (MUTCD 9B.02).
- Terminate the path where it is easily accessible to and from the street system, preferably at a trail head, controlled intersection or at the beginning of a dead-end street.
- Use of bollards should be avoided as standard practice and only used if a history of motorized access violations is present. If bollards are used at intersections and access points, they should be colored brightly and/or supplemented with reflective materials to be visible at night.



SHARED USE PATHS PROVIDE TRAIL USERS WITH THE MOST COMFORTABLE AND SCENIC EXPERIENCE AS THERE IS LIMITED POINTS OF CONFLICT WITH CARS AND ACCESS TO LOCAL NATURAL FEATURES.

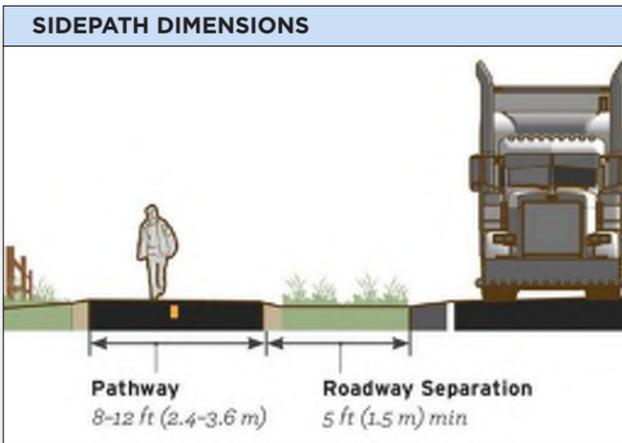
Sidepath Design Guidelines

Sidepaths provide a high degree of comfort on long uninterrupted roadway segments, but have operational and safety concerns at driveways and intersections with secondary streets. Crossings should be designed to promote awareness, lower speeds, and facilitate proper yielding of motorists to bicyclists and pedestrians.

Typical application

- The preferred minimum roadway separation width is 6.5 - 16.5 ft. Minimum separation width is 5 ft.
- Separation narrower than 5 ft is not recommended, though it may be accommodated in constrained circumstances with the use of a physical barrier between the sidepath and the roadway. Barriers should prevent path users from moving into the roadway. Refer to the AASHTO Roadside Design Guide (2011) for additional guidance.
- In extremely constrained situations, rumble strips may be used as separation for short distances.
- It is important to keep approaches to intersections and major driveways clear of obstructions due to parked vehicles, shrubs, and signs on public or private property.
- Maximum cross slope of 2%. Design for a 1.5% cross slope to account for tolerance in construction.
- Running slopes should be below 5%. However, because sidepaths are located within a roadway right of way, the running slope may match the general grade established for the adjacent roadway.

For estimated sidepath costs, see Table 3.5 on page 20.



The Lindon Heritage Trail is considered a sidepath.

Sidepath Crossing Design Guidelines

Typical Application

- At controlled and uncontrolled sidepath crossings of driveways or minor streets.
- Used to provide for visibility and awareness of the crossing by motorist in advance of the crossing.
- Increases the predictability of sidepath and road user behavior through clear, unambiguous right of way priority.

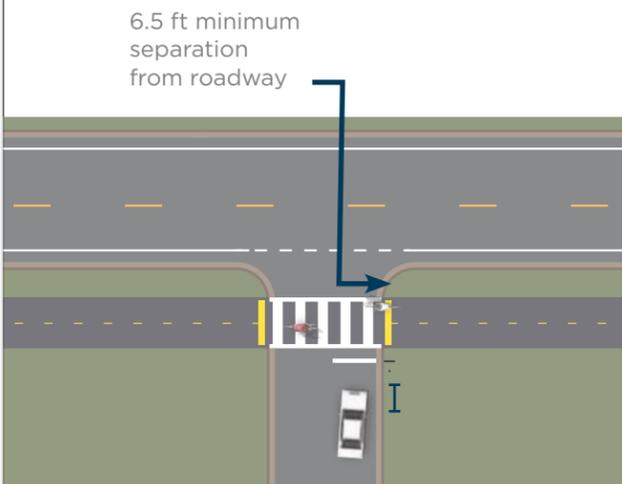
Design Features

- The sidepath should be given the same priority as the parallel roadway at all crossings.
- Provide clear sight triangles for all approaches of the crossing.
- Maintain physical separation to the crossing of 6.5 to 20 ft. As speeds on the parallel roadway increase, so does the preference for wider separation distance. Set back crossings of at least 15 feet allow for a vehicle to cross the path in a separate decision process from the merging maneuver with vehicle traffic.
- Use high visibility crosswalk markings to indicate the through area of the crosswalk.

Further Considerations

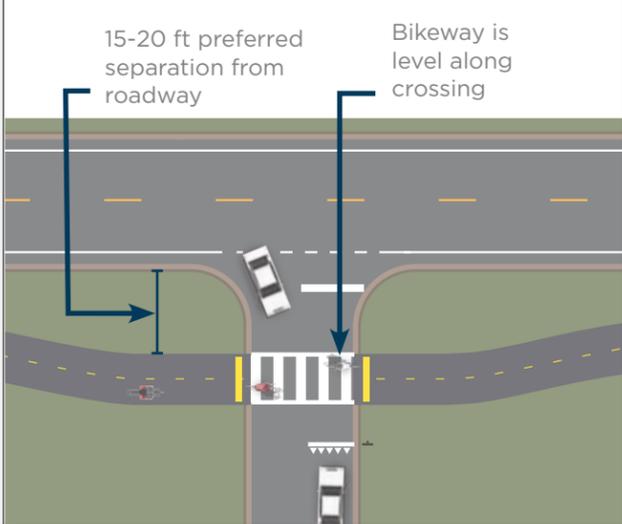
- Sidepaths running for long distances with many driveways or street crossings can create operational concerns. Attempt to limit or consolidate driveways along sidepaths.
- Along roadways, these facilities create a situation where a portion of the bicycle traffic rides against the normal flow of motor vehicle traffic and can result in wrong-way riding where bicyclists enter or leave the path.

ADJACENT SIDEPATH CROSSING



Where space is constrained or sight distance is limited, an adjacent crossing can promote visibility of path users.

SEPARATED SIDEPATH CROSSING



Where space is available, a separated crossing provides room for most motorists to yield to path users outside of the flow of through traffic.

TABLE 3.5 GENERAL COST ESTIMATES

Facility Type	Unit	Unit Cost	Assumptions
Shared Use Path	LF	\$140	Asphalt
10' wide path - asphalt	LF	\$140	
10' wide path - concrete	LF	\$160	8" concrete, saw cut joints
Sidepath	LF	\$160	
10' wide asphalt path	LF	\$140	
Natural Surface Trail	LF	\$6	
6' wide natural surface trail, native soil	LF	\$6	
Crossings and Traffic Calming			
Install RRFB with ped refuge island	EACH	\$25,000	Mast arm mounted
Install pedestrian hybrid beacon	EACH	\$113,000	Mast arm mounted
Curb extensions (per corner)	EACH	\$4,000	
Thermoplastic pavement marking	SQ	\$30	
Parking	Stall	\$2,000	
Restrooms	Building	\$150,000	
Class I Sign and Steel Sign Post			
Sign Panel	SQ	\$21	Class 1
Steel Sign Post	LF	\$20	2x2 tubing

Note that these are estimated costs. Planning Level Costs do not account for permitting, land acquisition, or design. Site-specific issues or constraints may result in higher costs. Actual costs may vary.



A less well maintained section of trail along Lindon's Heritage Trail.

TRAIL MAINTENANCE

Regular maintenance is a critical component of a high-quality trail system. Without proper and timely maintenance, trails are at risk of erosion, overgrowth, and degradation, which can have a negative impact on both safety and the user experience. People are more likely to walk or bike for transportation and recreation when they have access to well-maintained trails.

Trail maintenance is also crucial for minimizing impact on the natural environment, and wildlife; it also preserves the aesthetic beauty of the landscape. Ultimately, maintenance protects the investments made in building trails, and ensures that trails will continue to be assets to their community long into the future.

During the winter months, regular plowing and/or grooming of certain trails and paths is necessary to provide access, protect user safety, and reduce liability. Trail grooming can also increase opportunities for wintertime use such as cross-country skiing and fat biking.

The following section provides a brief overview of typical trail maintenance tasks and includes some general best practices.

Tree and Brush Trimming: Tree branches should be trimmed in a manner that leaves a one- to five-foot minimum horizontal clearance from the shoulder of the path and an eight- to twelve-foot vertical clearance. Any branches that appear to be dying, broken, or loose should be removed. Larger trees can be trimmed beyond the recommended clearance and trimmed less often. Trees should not be trimmed or pruned in a manner that thins out the branch cover and eliminates the shade it produces. Because natural surface trails are often less accessible than other types of trails and on-street facilities, a popular strategy is to trim trees and brush beyond the minimum clearances to reduce maintenance frequency.

Weed Abatement: In the case of landscaped buffers adjacent to sidepaths or other planted areas near trails, weeds should be removed regularly to preserve the setting's aesthetic features. Native vegetation along trails in open space and wooded areas can typically be left untended (with the exception of trimming), and will contribute to the natural aesthetic. However, invasive plant species should be removed.



Mowing and Landscaping: Maintaining vegetation on path shoulders (in open space) and in sidepath buffers is important for preserving the integrity of the soil, preventing encroachment, and enhancing the character of the trails. The frequency of mowing and other landscaping activities will depend on the time of year and weather conditions. Grass or vegetation patches that wither or die should be replaced by seeding the patches, placing mulch, and watering them. If erosion occurs in the patch before the new grass is grown, grading the area may be necessary.

Debris Removal: Debris on paved paths can range from natural tree and plant droppings, such as leaves and twigs, to human-produced garbage and litter. Debris should be swept or blown off of the path to prevent tripping hazards and to preserve the paths' aesthetic features. Debris removal may be required more frequently at different times of year.

Snow Removal: For trails where snow removal is desirable, removal should occur immediately following winter weather events. On-street pedestrian and bicycle facilities can be plowed and/or de-iced concurrently with travel lanes. Paved paths can be cleared of snow using plows, shovels, snow blowers, or mechanical snowbrushes.

Sign Repair and Replacement: Trail signage is not only critical for navigation and orientation, but also serves as a "brand" for the trail system. Keeping signage in good condition is therefore vital for maintaining a usable and appealing trail system. Trail signage should be inspected annually and replaced or repaired if damaged or defaced.

Restriping: Striping on paved paths should be inspected annually. Spring is typically the best time to inspect and restripe paths, as salt and winter weather can remove it. Restripe any areas where the striping has faded or been removed. Restriping on-street facilities such as shoulder lines or advisory shoulders should be done annually given Lindon's climate .

Crack Sealing and Repair: Sealing cracks in asphalt pavement is a cost-effective technique for extending the life of the asphalt surface. Crack sealing uses a flexible material that adheres to the crack edges but moves with the asphalt as it contracts and expands with changes in temperature. Identifying and sealing cracks as soon as possible can reduce the rate at which potholes form. Seal cracks that are 1/8 of an inch or greater to prevent further deterioration.

Sealcoating: Exposure to water, sunshine, and other elements degrades the binder that holds the aggregate in asphalt together over time. Sealcoat is a material that provides protection from this type of damage. Regular sealcoating will extend the life of asphalt, and will also replenish the color and appearance of the pavement.

Pavement Overlay: An overlay consists of adding new asphalt material over the existing surface assuming the base services is still sound enough. Overlay is distinct from total replacement, less expensive and extends the life of the pathway. Asphalt overlays are required around 20 - 30 years after the initial installation if sealcoating is done periodically.

The costs estimates for these maintenance priorities are listed in Table 3.4 on the opposite page.

TABLE 3.6 SHARED USE PATH AND SIDEPATH ROUTINE MAINTENANCE

Maintenance Activity	Function	Frequency	Est. Annual Cost (per mi.)
Path sweeping	Keep paved surfaces debris free	Twice annually (once in spring and once in fall)	\$140 (x2)
Litter and trash removal	Keep path clean and maintain consistent quality of experience for users	Annually, or as needed	\$70
Mowing path shoulders (native opens space areas)	Increases the effective width of the path corridor and helps prevent encroachment	Twice annually, in late spring and mid- to late-spring	\$100 (x2)
Tree and brush trimming	Eliminate encroachments into path corridor and open up sight lines	Annually, or less frequently as needed	\$100
Weed abatement	Manage existence and/or spread of noxious weeds, if present	Twice annually, in late spring and mid to late summer	\$140 (x2)
Safety Inspections	Inspect path tread, slope stability, and bridges or other structures	Annually	\$20
Snow removal/grooming	Limited to sections of the path where year-round access is desired	As needed (assume 20 events)	\$480
Sign and other amenity inspection/replacement	Identify and replace damaged infrastructure	Annually (assume 2 sign replacements)	\$100
Crack sealing and repair	Seal cracks in asphalt to reduce long term damage	Annually	\$250

Sweeping Guidance

- » Establish a seasonal sweeping schedule that prioritizes popular trail facilities.
- » Sweep on-street facilities whenever there is an accumulation of debris.
- » Pave gravel driveway approaches to minimize loose gravel on paved roadway shoulders.

Snow Removal Guidance

- » Employ a proactive snow removal strategy, and have a plan for the removal of de-icing surface material debris that accumulates in and around trail facilities.
- » Create a prioritization schedule for snow removal that focuses on primary routes and destinations

Pavement Surface Guidance

- » Inspect the pavement 2 to 4 months after trenching construction activities are completed to ensure that excessive settlement has not occurred.
- » Maintain a smooth pothole-free surface with minimal variations in pavement condition.

Pavement Overlay Guidance

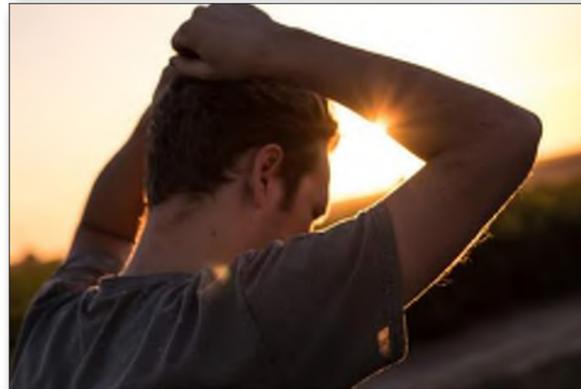
- » Extend the overlay over the entire roadway surface to avoid leaving an abrupt edge.
- » Ensure that inlet grates, and manhole and valve covers are within 1/4 inch of the finished pavement surface and are made or treated with slip-resistant materials.

D. OUTCOMES OF PLANS AFTER IMPLEMENTATION

The implementation of Lindon Parks, Trails and Recreation Master Plan and Capitol Facilities Plan Update is intended to provide the following benefits and outcomes:

Community Benefits

- Families will have increased opportunities to recreate together.
- A stronger sense of community through increased opportunities for individuals and families to interact.
- Reduced senses of alienation, loneliness and anti social behavior due to increase community recreation.
- Increased opportunities for community involvement, and shared management and ownership of resources.
- The new and updated facilities and their associated opportunities and quality of the local environment will provide a greater sense of community.



Individual Benefits

The residents who make use of the recreational facilities and activities will have greater overall well-being and health. The facilities will enhance the quality of the residents lives by:

- Providing increased opportunities to experience and observe nature.
- Providing opportunities for learning and living a more balanced life.
- Residents will have increased facilities for stress reduction, rest, relaxation, and revitalization through recreation.



Economic Benefits

- Increased Property values
- Attracting New Investments



Environmental Benefits

- Reduction in Flooding through increased storm water detention and storage.
- Enhanced Air quality through the installation of trees and vegetation that assist in filtering out pollutants in the air.

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04 | COSTS AND FUNDING OPPORTUNITIES



MAP 4.1 RECOMMENDATIONS FOR SPOT IMPROVEMENTS

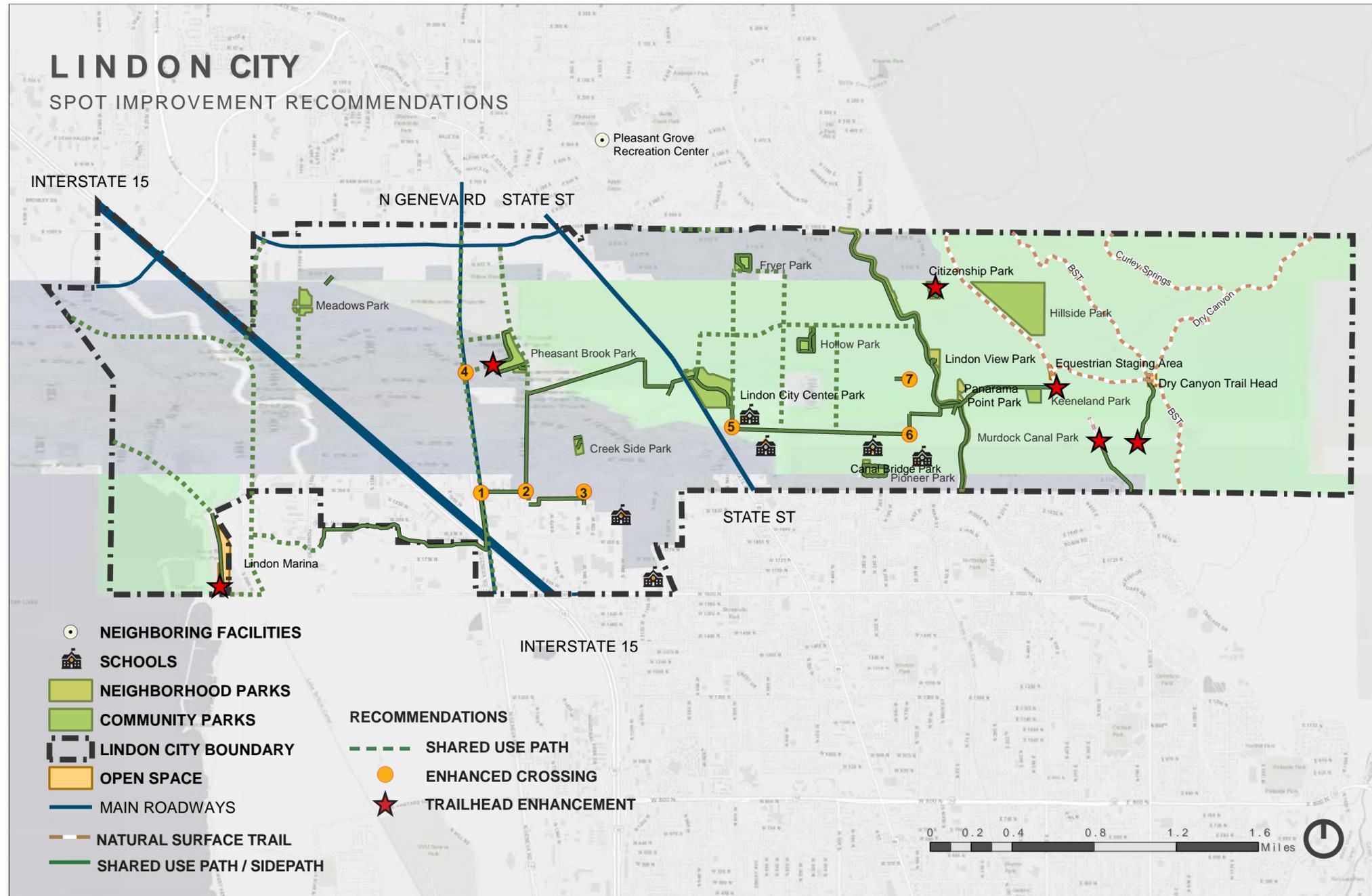


TABLE 4.2 PROPOSED TRAIL RECOMMENDATIONS

Facility Type	Location	Facility Notes	Estimated Cost
Shared Use Path	Heritage Trail from Pioneer Lane to Lindon Marina	Finish last section of the Lindon Heritage Trail. This project is shown in both the 2008 and 2015 Plans.	\$638,680
Regional Trail	Geneva Rd Rail-with-Trail from northern to southern city lines	Shared use path along the Geneva Road rail corridor. Once entirely completed, this project will provide regional connectivity through Utah County. This project is shown in the 2008 Plan.	\$1,331,960
Regional Trail	Utah Lake Shoreline Trail from Lindon Marina to city line	Continuation of the Utah Lake Shoreline Trail. This project is shown in both the 2008 and 2015 Plans.	\$448,560
Shared Use Path	2000 W from Heritage Trail to 700 N	Shared use path along 2000 W from the Heritage Trail to 700 N. This project is shown in the 2015 Plan.	\$939,120
Shared Use Path	Southern Pacific/FrontRunner Rail-with-Trail from Utah Lake Shoreline Trail to 600 N	Shared use path along the Southern Pacific/FrontRunner line, connecting to 600 N. This project is shown in the 2008 Plan.	\$934,640
Shared Use Path	800 W from 700 N to Lakeview Rd	Shared use path along 800 W from 700 N to Lakeview Rd. This project is shown in both the 2008 and 2015 Plans.	\$584,520
Shared Use Path	Park connector from Geneva Rd to Pheasant Brook Park	Shared use path connecting the future Geneva Road Rail-with-Trail to Pheasant Brook Park. This project is shown in both the 2008 and 2015 Plans.	\$116,480

Cost per 10' wide asphalt shared use trail.....\$140 per linear ft.
 Cost per 10' wide concrete shared use trail.....\$160 per linear ft.

TABLE 4.3 PROPOSED TRAIL HEAD RECOMMENDATIONS

Facility Type	Location	Facility Notes	Estimated Cost
Regional Trailhead	Lindon Marina	Enhance existing marina amenities, including improving parking area's connection to the trail and adding maps and signage.	\$1,328
Regional Trailhead	Pheasant Brook Park	Enhance existing park amenities, including improving parking area's connection to the trail and adding maps and signage.	\$1,328
Trail Access	Citizenship Park	Develop natural surface connection from park to the Bonneville Shoreline Trail. Consider expanding parking (2 additional spots).	\$11,920
Trailhead	Dry Canyon Winter Gate	Develop 2-5 parking spaces for winter access. Implement trail signage, including maps and directional signs.	\$8660
Trail Access	Bonneville Shoreline Trail Neighborhood Access	Develop 2-5 parking spaces for trail access.	\$10,000
Trailhead	Equestrian Staging Area	Enhance existing trailhead amenities, including adding restrooms, signage, and increased equestrian amenities.	\$151,328

Cost per restrooms.....\$150,000 per building
 Cost per parking stalls.....\$2,000 per stall
 Cost per sign panel.....\$21 per sq. ft.
 Cost per steel sign post.....\$20 per linear ft.
 Cost per natural surface trail.....\$6 per linear ft.

TABLE 4.4 PROPOSED ENHANCED CROSSINGS

ID	Facility Type	Location	Facility Notes	Estimated Cost
1	Trail Crossing	Heritage Trail and Geneva Road	Add curb extensions and crosswalk with warning signage and yield lines	\$15,328
2	Trail Crossing	Heritage Trail and 200 South	Install crosswalk with warning signage and yield lines. Look into possibility of intersection reconfiguration, including implementation of roundabout.	Varies.
3	Enhanced Crosswalk	Neighborhood Connector Trail and 200 South	Add curb extensions to shorten crossing and install high visibility crosswalk	\$15,328
4	Trail Crossing	Pheasant Brook Park and future Rails-with-Trail facility	Install hybrid beacon in conjunction with future trail development	\$120,500
5	Trail Crossing	Heritage Trail and Center Street (at City Center Park)	Add curb extensions to shorten crossing	\$14,000
6	Trail Crossing	Heritage Trail and Center Street (at Canal Drive)	Add curb extensions to shorten crossing	\$14,000
7	Trail Crossing	Neighborhood Connector Trail and Canal Drive	Add curb extensions and crosswalk with warning signage and yield lines	\$121,828

Cost per pedestrian hybrid beacon.....\$25,000 each
 Cost per thermoplastic x-walk marking.....\$30 per sq. ft.
 Cost per curb extension.....\$4,000 per corner

B. IMPACT FEES FACILITIES PLAN

SUMMARY

BACKGROUND

Lindon City (“City”) completed a Parks, Trails and Recreation Master Plan in February 2020. The Master Plan, along with input from the City, forms the basis for this Impact Fee Facilities Plan (IFFP) for Parks, Trails and Recreation.

The City has determined that there is one service area citywide and that there is no excess capacity in any existing park facilities. Only residential development is considered to create demand for parks, trails and recreation facilities and therefore only residential growth has been considered in the determination of impact fees.

Projections for population growth in the City are as follows:

TABLE 4.5 PROJECTED POPULATION GROWTH, 2019-2029

Year	Population
2019	11,353
2020	11,512
2021	11,673
2022	11,836
2023	12,002
2024	12,170
2025	12,340
2026	12,513
2027	12,688
2028	12,866
2029	13,046
<i>Source: Lindon City General Plan</i>	

IDENTIFY THE EXISTING AND PROPOSED LEVELS OF SERVICE AND EXCESS CAPACITY

Utah Code 11-36a-302(1)(a)(i)(ii)(iii)

The IFFP considers only system facilities in the calculation of impact fees. For the City, this has been determined to mean neighborhood and community parks. Neighborhood parks are considered project improvements and have not been included in the calculation of impact fees.

Existing service levels are based on the (2019) levels of service in the City for both parks and trails.¹ Existing and proposed service levels are shown in the table below.

TABLE 4.6 EXISTING AND PROPOSED SERVICE LEVELS

	UNITS		
	Existing	Proposed (Minimum)	Excess Capacity
Land (acres, with improvements, per 1,000 population)	6.22	6.22	0
Trail miles (trail miles per 1,000 population)	0.33*	0.33*	0
*The standard is 0.24 trail miles per 1,000 population for 10' trails and 0.09 trail miles per 1,000 population for 8' trails.			

The City intends to at least maintain existing service levels for both parks and trails.

The Aquatic Center currently serves 11,447 residents and has capacity to serve 17,000. Therefore, the Aquatic Center has excess capacity.

Parks, trails and recreation development in the City is one overall recreation system designed to meet the needs and desires of its residents for physical and leisure activities.

¹ The IFFP shows different service level than the Master Plan. This difference occurs for two reasons: 1) the IFFP cannot include project improvements (i.e., local parks), which are included in the level of service in the Master Plan; and 2) the IFFP does not include donated or gifted facilities for the purpose of establishing the impact-fee eligible level of service.

IDENTIFY DEMANDS PLACED UPON EXISTING PUBLIC FACILITIES BY NEW DEVELOPMENT ACTIVITY AT THE PROPOSED LEVEL OF SERVICE
Utah Code 11-36a-302(1)(a)(iv)

The table below shows the declining service levels that will occur in Lindon, due to population growth, if no new facilities are added. Each of these declining service levels is discussed in more detail in the body of this report

TABLE 4.7 IMPACTS TO SERVICE LEVELS DUE TO NEW DEVELOPMENT NO IMPROVEMENTS ARE MADE

Summary Table	Parks	Trails 10'	Trails 8'
Acres/Trail Miles per 1000 Population in 2019	6.22	0.25	0.09
Acres/Trail Miles per 1000 Population in 2029	5.42	0.21	0.08
Cost per Capita Investment in 2019	\$1,868.21	\$182.31	\$55.81
Cost per Capita Investment in 2029	\$1,625.77	\$158.65	\$48.57

The Aquatic Center has capacity to serve 17,000 persons. At the present time, it is serving 11,447 persons, representing 67 percent of total capacity. By 2029, the Aquatic Center will serve 12,491 persons, representing 73 percent of total capacity; therefore, 6 percent of the excess capacity will be consumed by new development between 2019 and 2029.

IDENTIFY HOW THE GROWTH DEMANDS WILL BE MET
Utah Code 11-36a-302(1)(a)(v)

In order to maintain the existing level of service, the projected new development over the next ten years will require the construction or acquisition of new park and trail facilities in the amount of \$3,566,010.65, as stated in 2019 dollars.

TABLE 4.8 NEW FACILITIES NEEDED TO MEET THE DEMANDS OF NEW GROWTH, 2019-2029

Description	Amount
Parks	\$3,162,875.93
Trails	\$403,134.72
Total	\$3,566,010.65

CONSIDERATION OF REVENUE SOURCES TO FINANCE IMPACTS ON SYSTEM IMPROVEMENTS
Utah Code 11-36a-302(2)

In order to maintain the existing level of service, the projected new development over the next ten years will require the construction or acquisition of new park and trail facilities in the amount of \$2,119,410, as stated in 2019 dollars.

UTAH CODE LEGAL REQUIREMENTS

Utah law requires that communities prepare an Impact Fee Facilities Plan (IFFP) before preparing an Impact Fee Analysis (IFA) and enacting an impact fee. Utah law also requires that communities give notice of their intent to prepare and adopt an IFFP. This IFFP follows all legal requirements as outlined below. The City has retained Zions Public Finance, Inc. (ZPFI) to prepare this Impact Fee Facilities Plan in accordance with legal requirements.

Notice of Intent to Prepare Impact Fee Facilities Plan

A local political subdivision must provide written notice of its intent to prepare an IFFP before preparing the Plan (Utah Code §11-36a-501). This notice must be posted on the Utah Public Notice website. The City has complied with this noticing requirement for the IFFP by posting notice.

Preparation of Impact Fee Facilities Plan

Utah Code requires that each local political subdivision, before imposing an impact fee, prepare an impact fee facilities plan. (Utah Code 11-36a-301).

Section 11-36a-302(a) of the Utah Code outlines the requirements of an impact fee facilities plan which is required to identify the following:

- (i) identify the existing level of service
- (ii) establish a proposed level of service
- (iii) identify any excess capacity to accommodate future growth at the proposed level of service
- (iv) identify demands placed upon existing facilities by new development activity at the proposed level of service; and
- (v) identify the means by which the political subdivision or private entity will meet those growth demands.

Further, the proposed level of service may:

- (i) exceed the existing level of service if, independent of the use of impact fees, the political subdivision or private entity provides, implements, and maintains the means to increase the existing level of service for existing demand within six years of the date on which new growth is charged for the proposed level of service; or
- (ii) establish a new public facility if, independent of the use of impact fees, the political subdivision or private entity provides, implements, and maintains the means to increase the existing level of service for existing demand within six years of the date on which new growth is charged for the proposed level of service.

In preparing an impact fee facilities plan, each local political subdivision shall generally consider all revenue sources to finance the impacts on system improvements, including:

- (a) grants
- (b) bonds
- (c) interfund loans
- (d) transfers from the General Fund
- (e) impact fees; and
- (f) anticipated or accepted dedications of system improvements.

Certification of Impact Fee Facilities Plan

Utah Code states that an impact fee facilities plan shall include a written certification from the person or entity that prepares the impact fee facilities plan. This certification is included at the conclusion of this analysis.

EXISTING SERVICE LEVELS, PROPOSED SERVICE LEVELS AND EXCESS CAPACITY

Utah Code 11-36a-302(1)(a)(i)(ii)(iii)

GROWTH IN DEMAND

Impacts on recreation-related facilities will come from residential development only. Residential growth is projected as follows:

TABLE 4.9 PROJECTED POPULATION GROWTH, 2019-2029

Year	Population	Population Growth
2019	11,353	
2020	11,512	159
2021	11,673	161
2022	11,836	163
2023	12,002	166
2024	12,170	168
2025	12,340	170
2026	12,513	173
2027	12,688	175
2028	12,866	178
2029	13,046	180
TOTAL		1,693
<i>Source: Lindon City General Plan Population projections are for 1,045 new residents between 2019 and 2029.</i>		

EXISTING SERVICE LEVELS

Existing system parks are shown in the Table below. System parks that were acquired through donations or grants have not been included in the level of service for impact fees.

TABLE 4.10 SYSTEM PARKS

Summary	Size (Acres)
Hollow Park	4.46
Pioneer Park	5.3
Fryer Park	4
Meadow Park	4.16
Pheasant Brook Park	9.78
Lindon City Center Park	11.3
Hillside Park	31.66
TOTAL	70.66

The existing level of service for parks then, for the purpose of calculating impact fees, is 6.22 acres per 1,000 residents, calculated by dividing the 70.66 eligible park acres by the 2019 population of 11,353 residents (which has been divided by 1,000).

PARK LAND AND IMPROVEMENTS

The table below summarizes the improvements, along with the costs, to determine an existing standard for park land and improvements. Cost estimates have been provided in consultation with the City.

TABLE 4.11 SYSTEM PARK LAND AND IMPROVEMENTS

	Units	Cost per Unit	Total Investment
Land (Acres)	70.66	\$201,435	\$14,233,391.24
Restrooms	5	\$150,000	\$750,000.00
Pavilions - Small	2	\$50,000	\$100,000.00
Pavilions - Medium	5	\$75,000	\$375,000.00
Pavilions - Large	3	\$150,000	\$450,000.00
Picnic Tables	91	\$1,000	\$91,000.00
Playgrounds	6	\$100,000	\$600,000.00
Exercise Equipment	0	\$9,000	\$0.00
Pickle Ball Courts	6	\$50,000	\$300,000.00
Tennis Courts	1	\$75,000	\$75,000.00
Basketball Courts	0	\$20,000	\$0.00
Soccer Fields (not incl. sod)	7	\$2,000	\$14,000.00
Ball Diamonds (not incl. sod)	4	\$20,000	\$80,000.00
Sand Volleyball	0	\$25,000	\$0.00
Riding Arena - sf	27,243	\$2.25	\$61,296.75
Lighted Paths	3,850	\$50.00	\$192,500.00
Paved Paths	1,884	\$15	\$28,260.00
Regional Trail Access	0	\$200,000	\$0.00
Asphalt (parking) sf	63,530	\$5.00	\$317,650.00
Mowed sf***	1,770,833	\$2.00	\$3,541,666.00
TOTAL Parks			\$21,209,763.99

With 67.2 existing park acres, the average cost for land and improvements is \$300,166.49 per acre. Land costs are based on the recent sale of 9.05 acres to Vineyard for a cost of \$1,822,986, or a cost of \$201,434.92 per acre.

TRAILS

The City currently has 2.8 miles of 10’ wide paved trails, or 14,784 linear trail feet. This results in an existing service level of 0.24 linear trail feet per capita, calculated by dividing the 2.8 miles by the 2019 population.

The City currently has 1 mile of 8’ wide paved trails, or 5,280 linear trail feet. This results in an existing service level of 0.09 linear trail feet per capita, calculated by dividing the 1 mile by the 2019 population.

AQUATIC CENTER

The City has currently invested \$1,864,298 in the Aquatic Center, with \$8,891,925 remaining in bond payments through 2034. With a current population of 11,447 persons, the current level of service is \$109.66 per capita, calculated by dividing the \$1,864,298 in payments made by the existing population of 11,447 persons.

PROPOSED SERVICE LEVELS

PARKS AND TRAILS

The City has determined that parks and trails have no excess capacity and that it desires to maintain existing service levels in the future, as new development occurs. This determination is based on the City’s Parks and Trails Master Plan completed in January 2020.

The proposed service level for the Aquatic Center is to serve 17,000 residents.

IDENTIFY EXCESS CAPACITY

The City has identified excess capacity in the Aquatic Center. The Aquatic Center has capacity to serve 17,000 persons. At the present time, it is serving 11,447 persons, representing 67 percent of total capacity. By 2029, the Aquatic Center will serve 12,492 persons, representing 73 percent of total capacity; therefore, 6 percent of the excess capacity will be consumed by new development between 2019 and 2029.

IDENTIFY DEMANDS PLACED ON EXISTING PUBLIC FACILITIES BY NEW DEVELOPMENT ACTIVITY AT PROPOSED LEVEL OF SERVICE AND HOW THOSE DEMANDS WILL BE MET

Utah Code 11-36a-302(1)(a)(iv)(v)

DEMAND PLACED ON FACILITIES BY NEW DEVELOPMENT ACTIVITY

PARK LAND AND PARK IMPROVEMENTS

Existing park service levels will decline, due to new development activity, from the existing service level of 6.22 acres per 1,000 persons to 5.42 acres per 1,000 residents, over the next 10 years, if no improvements are made.

TABLE 4.12 PARK LAND AND IMPROVEMENT SERVICE LEVEL IMPACTS FROM NEW DEVELOPMENT ACTIVITY, 2019-2029

Year	Population	Population Growth	Unit Service Levels per 1000 Persons if No New Facilities
2019	11,353		6.22
2020	11,512	159	6.14
2021	11,673	161	6.05
2022	11,836	163	5.97
2023	12,002	166	5.89
2024	12,170	168	5.81
2025	12,340	170	5.73
2026	12,513	173	5.65
2027	12,688	175	5.57
2028	12,866	178	5.49
2029	13,046	180	5.42

TRAILS

The existing level of service of 0.24 linear trail miles (10' wide trails) per capita will decline to 0.98 linear trail miles per capita, over the next 10 years, if no new improvements are made.

TABLE 4.13 10' TRAIL SERVICE LEVEL IMPACTS FROM NEW DEVELOPMENT ACTIVITY, 2019-2029

Year	Population	Population Growth	Unit Service Levels per 1000 Persons if No New Facilities
2019	11,353		0.25
2020	11,512	159	0.24
2021	11,673	161	0.24
2022	11,836	163	0.24
2023	12,002	166	0.23
2024	12,170	168	0.23
2025	12,340	170	0.23
2026	12,513	173	0.22
2027	12,688	175	0.22
2028	12,866	178	0.22
2029	13,046	180	0.21

The existing level of service of 0.09 linear trail miles (8' wide trails) per capita will decline to 0.08 linear trail miles per capita, over the next 10 years, if no new improvements are made.

TABLE 4.14 8' TRAIL SERVICE LEVEL IMPACTS FROM NEW DEVELOPMENT ACTIVITY, 2019-2029

Year	Population	Population Growth	Unit Service Levels per 1000 Persons if No New Facilities
2019	11,353		0.09
2020	11,512	159	0.09
2021	11,673	161	0.09
2022	11,836	163	0.08
2023	12,002	166	0.08
2024	12,170	168	0.08
2025	12,340	170	0.08
2026	12,513	173	0.08
2027	12,688	175	0.08
2028	12,866	178	0.08
2029	13,046	180	0.08

AQUATIC CENTER

The Aquatic Center has capacity to serve 17,000 persons. At the present time, it is serving 11,447 persons, representing 67 percent of total capacity. By 2029, the Aquatic Center will serve 12,492 persons, representing 73 percent of total capacity; therefore, 6 percent of the excess capacity will be consumed by new development between 2019 and 2029.

IDENTIFY THE MEANS BY WHICH THE POLITICAL SUBDIVISION WILL MEET THE GROWTH DEMANDS

The City will need to acquire additional park lands and improvements to maintain its existing and proposed service levels. Service levels will decline, due to population growth, unless new facilities are constructed or acquired. Impact fees will be used to maintain the existing service levels for parks and trails.

The figures in the following table were calculated by multiplying the existing service levels by the cost for each line item by the projected growth in demand over the next ten years.

TABLE 4.15 COST OF NEW PARK CONSTRUCTION DUE TO NEW GROWTH, 2019-2029

Year	Additional Park (Acres) Needed	Additional Investment Needed
2019		\$0
2020	0.99	\$297,045
2021	1.00	\$300,781
2022	1.01	\$304,518
2023	01.03	\$310,123
2024	1.05	\$313,859
2025	1.06	\$317,595
2026	1.08	\$323,200
2027	1.09	\$326,936
2028	1.11	\$332,541
2029	1.12	\$336,277
TOTAL	10.54	\$3,162,876

TABLE 4.16 COST OF NEW 10' TRAIL CONSTRUCTION DUE TO NEW GROWTH, 2019-2029

Year	Additional Trail Miles Needed	Additional Investment Needed
2019	-	\$0.00
2020	0.04	\$28,987.21
2021	0.04	\$29,351.83
2022	0.04	\$29,716.45
2023	0.04	\$30,263.38
2024	0.04	\$30,628.00
2025	0.04	\$30,992.62
2026	0.04	\$31,539.55
2027	0.04	\$31,904.17
2028	0.04	\$32,451.09
2029	0.04	\$32,815.71
TOTAL	0.42	\$308,650.02

TABLE 4.17 COST OF NEW 8' TRAIL CONSTRUCTION DUE TO NEW GROWTH, 2019-2029

Year	Additional Trail Miles Needed	Additional Investment Needed
2019	-	\$0.00
2020	0.01	\$8,873.64
2021	0.01	\$8,985.25
2022	0.01	\$9,096.87
2023	0.01	\$9,264.30
2024	0.01	\$9,375.92
2025	0.01	\$9,487.54
2026	0.02	\$9,654.96
2027	0.02	\$9,766.58
2028	0.02	\$9,934.01
2029	0.02	\$10,045.63
TOTAL	0.15	\$94,484.70

Total costs anticipated over the next 10 years, in order to maintain existing service levels given the projected growth in the City, is \$3,566,010.65.

TABLE 4.18 SUMMARY OF NEW FACILITIES NEEDED TO MEET THE DEMANDS OF NEW GROWTH, 2019-2029

Description	Amount
Parks	\$3,162,875.93
Trails	\$403,134.72
Total	\$3,566,010.65

CONSIDERATION OF ALL REVENUE SOURCES

Utah Code 11-36a-302(2)

The City anticipates that future trail land will be acquired through easements and grants, as it has in the past, and has therefore not included any cost for trail land in the calculation of impact fees. The City is unaware of any potential grant sources for future parks, recreation and trails development. However, should it be the recipient of any such grants, it will then look at the potential to reduce impact fees.

While the City has been gifted some park property in the past, it has no future indication of any gifts that will be received by the City. Further, the City has conservatively excluded any gifted properties, or properties acquired through grant funds, from establishing its level of service used in the calculation of impact fees.

Bonds

The City has one outstanding bond for the Aquatic Center, Series 2008, refunded in 2015, with an expiration date of 2034. Credits on this bond must be made in connection with impact fees so that double payment does not occur.

While the City could issue bonds in the future in order to fund parks, recreation or trail facilities, no bonds are currently being contemplated and therefore no costs associated with future bond issuance have been included in the calculation of impact fees.

Interfund Loans

The City has the option to purchase facilities through interfund loans but no interfund loans are currently in place.

Transfer from General Fund

To the extent that the City is able to generate net revenues in its General Fund, it may choose to transfer all or a portion of the net revenues to the City's capital fund. It is most likely that, if net revenues should be generated, they will be used to enhance existing service levels and not to offset the demands generated by new development which is anticipated to be offset with impact fees.

Impact Fees

Because of the growth anticipated to occur in the City, impact fees are a viable means of allowing new development to pay for the impacts that it places on the existing system. This IFFP is developed in accordance with legal guidelines so that an Impact Fee Analysis for Parks, Recreation, and Trails may be prepared, and the City may charge impact fees for Parks, Recreation, and Trails.

Anticipated or Accepted Dedications of System Improvements

Any item that a developer funds must be included in the IFFP if a credit against impact fees is to be issued and must be agreed upon with the City before construction of the improvements.

CERTIFICATION

Zions Public Finance, Inc. certifies that the attached impact fee facilities plan:

1. Includes only the costs of public facilities that are:
 - a. allowed under the Impact Fees Act; and
 - b. actually incurred; or
 - c. projected to be incurred or encumbered within six years after the day on which each impact fee is paid;
2. Does not include:
 - a. costs of operation and maintenance of public facilities;
 - b. costs for qualifying public facilities that will raise the level of service for the facilities, through impact fees, above the level of service that is supported by existing residents;
 - c. an expense for overhead, unless the expense is calculated pursuant to a methodology that is consistent with generally accepted cost accounting practices and the methodological standards set forth by the federal Office of Management and Budget for federal grant reimbursement;
3. Complies in each and every relevant respect with the Impact Fees Act.

C. PARKS AND RECREATION FUNDING OPTIONS

OVERVIEW OF PRIMARY FUNDING SOURCES

Multiple funding sources are available for funding parks, trails and recreation facilities. The primary options include grants, impact fees, taxes, park fees and the issuance of debt (bonds). The following sections discuss some of the advantages and disadvantages of each revenue source, along with the appropriateness of each source to various capital or operating needs.

Potential funding sources are discussed in more detail in this report and are organized as follows:

- Monthly Fees
- Sales Tax
- Recreation, Arts and Parks Tax (RAP)
- General Obligation Bonds
- Lease Revenue Bonds
- User Fees
- Recreation District
- Foundations and Donations
- Joint Funding Partnerships
- Grants and Other Funding Sources
- Impact Fees
- Special Assessment Areas

MONTHLY FEES

Several communities in Utah charge monthly fees for parks and recreation maintenance. Herriman is an example. These fees are generally added to the City's water bill and the recreation portion of the fee is the first amount to be credited when payments are made. For example, if the monthly water bill totaled \$40, plus \$5 for a recreation fee, the total bill would be \$45. If the property owner paid only \$40, rather than the full \$45, the parks fee would be credited first, leaving the property owner with a deficit of \$5 on the water bill.

The City will need to do an analysis to justify that the fee charged is reflective of its needs to cover costs of City parks, trails and recreation maintenance.

LOCAL SALES TAX

Based on Utah Code §59-12-203, any city, county or town may levy a local option sales tax of one percent on the purchase price of the same transactions for which the statewide sales tax rate of 4.70 percent is charged. The local sales and use tax was established in 1959. Historically, the rate charged associated with the local option portion of the tax changed over the years:

July 1, 1959- June 30, 1975	1/2 of one percent
July 1, 1975- June 30, 1983	3/4 of one percent
July 1, 1983- June 30, 1986	7/8 of one percent
July 1, 1986- Dec 31, 1989	29/32 of one percent
January 1, 1990- present	one percent

Currently, all counties, cities and towns in Utah have adopted ordinances to impose the maximum one percent option of the local sales and use tax. Counties may charge an additional 0.25 percent local option tax to be used for county purposes.

Because Lindon City has already enacted the full one percent local option sales tax, it does not have the ability to raise these taxes further. Therefore, the use of sales tax funds for City parks, trails and recreation projects would merely represent a "shift" in tax revenues to pay for one facility over another.

Additional sales-related taxes have been authorized by the Legislature for transportation use, as well as a "botanical, cultural, zoo tax," also known as the "recreation, arts and parks tax" or the ZAP and RAP taxes. The use of this tax is discussed in a later section. Other sales-related taxes such as the tourism taxes (such as lodging, restaurant sales, resort communities and motor vehicle rentals) have not been considered likely for City parks and recreation facilities and are not included in the discussion.

Revenue bonds payable from sales tax revenues are governed pursuant to Utah State Code Section 11-14-307. Without the need for a vote, cities and counties may issue bonds payable solely from excise/sales taxes levied by the city, county or those levied by the State of Utah and rebated to the city or county such as gasoline taxes or sales taxes.

The advantages and disadvantages of using sales tax revenue bonds are as follows:

- Advantages:
- Fairly steady revenue stream (although more volatile than property tax revenues based on economic cycles)
 - Available history of sales tax revenues on which to base projections
 - Sales tax bonds can be issued and do not require voter approval
- Disadvantages:
- Cannot raise sales tax percentage of revenues above limit allowed by Utah Legislature
 - Does not provide a new revenue stream unless tax rate is increased or sales increase
 - Used for capital costs and not operating expenses

PARKS, ARTS, RECREATION AND CULTURE TAX²

Many communities have initiated Zoo, Arts, and Parks (ZAP) or Recreation, Arts, and Parks (RAP) taxes which have been very effective in raising funds to complete parks, recreation, trails and open space projects. This tax must be put on the election ballot for voter approval and amounts to 1/10th of 1 percent of the point of sale revenue. Lindon City has already approved and enacted this tax to the full amount allowed under Utah Code §59-12-1401 so no new funds are available to Lindon City from this source.

GENERAL OBLIGATION BONDS

General obligation bonds are a common resource for funding major capital facilities, such as a recreation center or sports park, that benefits all of the community. Often, several communities will join together to join their resources (i.e., tax base) to build a joint facility that serves several communities.

General obligation bonds, commonly referred to as "G.O. bonds," are generally the least costly form of financing for capital facilities. They attract the lowest interest rates in the market because they are secured by the "full faith and credit" - the unlimited pledge of the taxing ability of the community and therefore have the least credit risk to investors. Under the Utah State Constitution, any bonded indebtedness secured by property tax levies must be approved by a majority of voters in a bond election called for that purpose.

It is our experience that if the recreation improvements being considered for funding through the G.O. bond have broad appeal to the public and proponents are willing to assist in the promotional efforts, G.O. bonds for recreation projects can meet with public approval. However, due to the fact that some constituents may not view them as essential-purpose facilities for a local government or may view the government as competing with the private sector, obtaining positive voter approval may be a challenge.

General Obligation bonds ("GO") are subject to simple majority voter approval by the constituents of the issuing entity. General obligation elections can be held once each year, in November, following certain notification procedures that must be adhered to in accordance with State Statutes in order to call the election (pursuant to Utah State Code 11-14-2 through 12). Following a successful election, it is not necessary to issue bonds immediately, but all bonds authorized must be issued within ten years. Once given the approval to proceed with the issuance of the bonds, it would take approximately 90 days to complete the bond issuance.

² Sometimes referred to as the botanical, cultural and zoo tax

General obligation bonds can be issued for any governmental purpose as detailed in Utah Code §11-14-1. The proceeds from bonds issued on or after May 14, 2013 may not be used for operation and maintenance expenses for more than one year after the date any of the proceeds are first used for those expenses. Therefore, GO bonds would not be a viable source of operating and maintenance expenses for Lindon City. If capital improvements are desired to be made, GO bonds could be used for this purpose.

The amount of general obligation debt is subject to the following statutory limitations:

- Counties are limited to two percent (2%) of the total taxable value of the County;
- School Districts are limited to four percent (4%) of the total taxable value in the District;
- Cities of the 1st and 2nd class are limited to a total of eight percent (8%) of the total taxable value, four (4%) for general purposes and four (4%) for water, sewer and lights; and
- Cities of other classes (such as Lindon City) or towns are limited to a total of twelve percent (12%) of total taxable value, four percent (4%) for general purposes and eight percent (8%) for water, sewer and lights.

Notwithstanding the limits noted above, most local governments in Utah have significantly less debt than their statutory limitations.

Pursuant to state law, general obligation bonds must mature in not more than forty years from their date of issuance. Typically, however, most GO bonds mature in 15- 20 years.

- Advantages of G.O. Bonds:
- Lowest cost form of borrowing
 - 'New' source of revenues identified

- Disadvantages of G.O. Bonds:
- Timing issues; limited date to hold required G.O. election
 - Risk of a "no" vote while still incurring costs of holding a bond election
 - Possibility of election failure due to lack of perceived benefit to majority of voters
 - Must levy property tax on all property even if some properties receive limited or no benefit from the proposed improvements
 - Can only bond for physical facilities, not ongoing or additional operation and maintenance expense

LEASE REVENUE BONDS

One financing mechanism which, until the advent of sales tax revenue bonds, was frequently used to finance recreation facilities is a Lease Revenue Bond issued by the Local Building Authority of the City. This type of bond would be secured by the recreation center property and facility itself, not unlike real property serving as the security for a home mortgage. Lease revenue bonds are repaid by an annual appropriation of the lease payment by the City Council. Generally, this financing method works best when used for an essential public facility such as city halls, police stations and fire stations. Interest rates on a lease revenue bond would likely be 15 to 30 basis points higher than on sales tax revenue bonds depending on the market's assessment of the "essentiality" of the facility.

Advantages of Lease-Revenue Bonds:

- No general vote required
- No specific revenue pledge required

Disadvantages of Lease Revenue Bonds:

- Higher financing costs than some other alternatives
- No "new" revenue source identified

As this type of bond financing does not generate any new revenue source, the City Council will still need to identify revenue sources sufficient to make the lease payments to cover the debt service.

USER FEES

User fees are generally charged for recreation facilities or, in some cases, for access to specific trails and facilities. Reasons for not including fees for trails include the difficulty and cost of collecting fees, especially with multiple access points. Another user fee option for trails is to issue permits to park in an area with trail access. Even if permits were required, rather than establishing a pay station, enforcement of permits (i.e., bike tags, parking permits, etc.) would be costly and enforcement would be sporadic at best. And, some communities have found that an unintended negative consequence is that parking spills over into adjacent neighborhoods.

RECREATION DISTRICT

Utah law allows for the creation of special districts based on Utah Code §17B. The generic term for all entities that fall under Title 17B of the Utah Code is "local district." The only type of district in Utah that is not a "local district" is a "special service district." Title 17D Chapter 1 of the Utah Code deals with the creation and administration of special service districts and is known as the "Special Service District Act."

Local districts may be created for a variety of purposes including park operations, recreational facilities and services. A special service district

created under Title 17D is a hybrid entity in that it is an independent governmental entity, except when it comes to the levy of taxes or assessments, the issuance of debt, or the holding of an election. These actions must be approved by the governmental entity that created the special service district. In reality, special service districts are still ultimately under the control of their creating entities. A special service district may be created by a city or county to provide a variety of services, including recreation.

Local districts and special service districts can only be created by cities or counties. The process is initiated either by the cities or counties themselves by resolution, or by petition from a group of citizens. In order to be created, local districts require a petition signed by 33 percent of the private property owners within the proposed district whose property values total at least 25 percent of the value of all private real property within the proposed district or 33 percent of the voters within the proposed district who voted in the last general election for Governor. Special service districts require a citizen petition to be signed by property owners within the proposed district whose property values total at least 10 percent of the taxable value of all taxable property within the proposed special service district or at least ten percent of the registered voters within the proposed special service district.

Governance options between the two types of districts differ somewhat. While both are under the jurisdiction of a local governing board, which must have at least three members, special service districts are governed by the cities or counties that create them. A local district determines, at its creation, whether board members will be appointed, elected, or a hybrid with some members appointed and others elected.

The major difference between the two types of districts is in their ability to tax. Local districts may levy property taxes but special service districts can only do so if the governing body that created the district votes to do so and the tax is approved by a majority of voters. All districts are subject to limitations on property taxes imposed to pay for operations and maintenance.

Limits are also placed on local districts and special districts for bonded indebtedness. Utah Code §11-14-310(3)(b) limits general obligation bonds to a percentage of the fair market value of all taxable property within the district. The limit for a local district is .05 and 0.12 for a special service district (unless specified in the Code for a specific type of special service district).

Liability insurance is required for all districts with budgets in excess of \$50,000. All districts must comply with most of the Utah Procurement Code

as found in Section 63G-6-104 and must adopt and implement formal purchasing policies and procedures.

If some sort of recreation district were to be created, the total taxable value of the district would be used to determine the tax rate necessary to raise the desired amount of annual operating revenues necessary to support open space.

The advantages and disadvantages of a recreation district are summarized as follows:

Advantages:

- Spread costs over a larger population
- Taxing ability that does not show up on the books of the City

Disadvantages:

- Loss of direct governance and control of recreation facilities

FOUNDATIONS AND DONATIONS

Creating a foundation could provide an additional method of generating new revenues for the City - especially for preservation and development of canyon facilities. One example of an area that has successfully done this is the Mountain Trails Foundation for Park City.

MTF has five membership options, ranging from \$10 to \$300+. The most popular options are the \$50 and \$100 a year donations. Membership results in various coupons, discounts, and gifts (depending upon the \$ level - maps, socks, vests, jackets, etc.). Memberships represent 60 percent of the Foundation's annual revenues, while races, grants, and corporate sponsorships provide the rest of the revenue.

In 2015, MTF spent \$130,000 for summer trail maintenance and an additional \$30,000 for winter trail grooming. Since its inception in 1994, MTF has been able to increase trail miles from 40 to over 400. Annual membership averages close to 4,000 members. Several members interviewed indicate that Marmot is a major sponsor and provides its "gear" at significantly lower prices to the Foundation. Therefore, many people join MTF because they are rewarded with athletic gear to offset the cost of their annual donation. Car window stickers for MTF are also viewed as something of a status symbol by some Summit County residents.

Revenue generated through memberships (representing roughly 60 percent of total revenues) is supplemented by corporate sponsorships and races. Several very popular races are held on Park City trails. Increased races sponsored by a Foundation would be another means of increasing revenue streams for City open space.

Advantages:

- Those most involved and interested contribute to the associated costs
- Creates a sense of pride and ownership in recreation-related facilities
- Partners with the private sector to increase business contributions

Disadvantages:

- Not a steady or consistent revenue source
- Cannot bond against these revenues
- May take time to build up significant membership and revenues
- Administrative costs of running the Foundation unless done by volunteers

JOINT FUNDING PARTNERSHIPS

Joint funding opportunities may also occur between municipalities and among agencies or departments within a municipality. Cooperative relationships between cities and counties are not uncommon, nor are partnerships between cities and school districts. Often, small cities in a region are able to cooperate and pool resources for recreation projects. There may be other opportunities as well which should be explored whenever possible in order to maximize recreation opportunities and minimize costs. In order to make these kinds of opportunities happen, there must be on-going and constant communication between residents, governments, business interests and others.

Advantages:

- Spreads the costs, thereby resulting in a lower burden on Lindon
- Additional revenues may provide opportunities to provide additional facilities or services using the open space

Disadvantages:

- Does not provide a steady and reliable source of revenues
- Cannot bond against these revenues

GRANTS AND OTHER FUNDING SOURCES

The following sources may serve as a supplement to, though not a replacement for, the previous funding sources. The availability of these funds may change annually depending on budget allocations.

LAND AND WATER CONSERVATION FUND

The LWCF state assistance program provides matching grants to help states and local communities protect parks and recreation resources. Running the gamut from wilderness to trails and neighborhood playgrounds, LWCF funding has benefited nearly every county in America, supporting over 41,000 projects. This 50:50 matching program is the primary federal investment tool to ensure that families have easy access to parks and open space, hiking and riding trails, and neighborhood recreation facilities. Allocation amounts have decreased over time and LCWF reports a backlog of needs for these funds. This program is administered locally by Utah State Parks and Recreation.

MAP-21 CURRENT (REPLACES SAFETEA-LU)

The Moving Ahead for Progress in the 21st Century (Map-21) includes a number of substantial changes to the transportation enhancement (TE) activities defined in Title 23. The activities are now termed “transportation alternatives” (TAs).

To qualify for funding all projects must fit into one of the following nine federal designated categories:

1. Construction, planning and design of facilities for pedestrians, bicyclists, compliance with Americans with Disabilities Act
2. Safe routes for non-drivers to access daily needs
3. Conversion and use of abandoned railroad corridors for trails
4. Construction of turnouts, overlooks and viewing areas
5. Community improvements including
 - Inventory, control or removal of outdoor advertising
 - Historic preservation and rehabilitation of historic transportation facilities
 - Archaeological activities relating to impacts from implementation of transportation projects eligible under this title
6. Any environmental mitigation activity
 - Address storm water management, control and water pollution prevention or abatement related to highway construction or due to highway runoff
 - Reduce vehicle-caused wildlife mortality or to restore and maintain connectivity among terrestrial or aquatic habitats

7. The Recreation Trails Program under section 206.
8. Safe Routes to schools under section 1404 of SAFETEA-LU
9. Planning, designing, or constructing boulevards and other roadways largely in the right-of-way of former Interstate System routes or divided highways.

UTAH WAYPOINT GRANT

The **Utah Office of Outdoor Recreation** initiated the **Utah Waypoint Grant** program in 2015. The Waypoint program makes grant monies available with a 50/50 match to communities to build outdoor recreation infrastructure which would become an enhancement in the area.

To qualify, Waypoint projects must offer an economic opportunity for the community and should have the potential to attract or retain residents and increase visitation to the region. Various types of outdoor recreation infrastructure would be eligible for the Waypoint grant including trails, trail infrastructure, and trail facilities, restroom facilities near popular recreational climbing areas, ramps and launch sites that would improve water access along rivers, whitewater parks, yurts, infrastructure for wildlife viewing areas and more. The areas for the project should be open and accessible to the public. This grant is to be used for the construction of the recreational infrastructure and cannot be used for the planning of the project. Ideally, the plans should be complete before applying for the grant. This was a pilot program and the first applications were due October 2015. Future funds will be available based on funding levels.

RECREATIONAL TRAILS PROGRAM (RTP) AND TRANSPORTATION ALTERNATIVES PROGRAM (TAP)

The federally-funded Recreational Trails Program (RTP) has helped with non-motorized and motorized trail development and maintenance, trail educational programs, and trail-related environmental protection projects. The Fixing America’s Surface Transportation (FAST) Act reauthorized the RTP for Federal fiscal years 2016 through 2020 as a set-aside of funds from the Transportation Alternatives (TA) Set-Aside under Surface Transportation Block Grant Program (STBG).

The national total is divided among States based on each State’s proportionate share of FY2009 Transportation Enhancements funding. Unless a Governor opts out, an amount equal to the State’s FY 2009 RTP apportionment is to be set aside from the State’s TAP funds for the RTP. The 2017 set-aside for Utah is \$1,561,852. Utah State Parks and Recreation administers this program locally.

PRIVATE AND PUBLIC PARTNERSHIPS

The Parks and Recreation Department or a group of communities acting cooperatively, and a private developer or other government or quasi-government agency may often cooperate on a facility that services the public, yet is also attractive to an entrepreneur or another partner.

PRIVATE FUNDRAISING

While not addressed as a specific strategy for individual recreation facilities, it is not uncommon for public monies to be leveraged with private donations often in concert with a foundation (see Foundations and Donations above). Private funds will most likely be attracted to high-profile facilities such as a swimming complex or sports complex, and generally require aggressive promotion and management on behalf of the park and recreation department or city administration.

DEDICATIONS AND DEVELOPMENT AGREEMENTS

The dedication of land for parks, and park development agreements has long been an accepted development requirement and is another valuable tool for implementing park development. The City can require dedication of park land through review of projects such as Planned Units Developments (PUDs). Many cities have received park dedications and trail easements.

URBAN RENEWAL (“REDEVELOPMENT”) AGENCY FUNDS

Generally, redevelopment agency (RDA) funds are restricted for use in redevelopment areas (unless for housing). As new RDA areas are identified and developed, tax increment funds generated can, at the discretion of the City, be used to fund park acquisition and development.

SERVICE ORGANIZATION PARTNERS

Many service organizations and corporations have funds available for park and recreation facilities. Local Rotary Clubs, Kiwanis Clubs, and other service organizations often combine resources to develop park and recreation facilities. Other for-profit organizations such as Home Depot and Lowes are often willing to partner with local communities in the development of playground and other park and recreation equipment and facilities. Again, the key is a motivated individual or group who can garner the support and funding desired.

Another potential partnership with service organizations is through an Adopt-A-Trail program where various organizations assist with maintenance of City open space and thereby reduce operating costs.

IMPACT FEES

Impact fees are one-time fees charged by communities to offset the capital costs associated with new development. These fees are especially useful in areas of rapid population growth. They help the community to maintain a current level of service as new development puts strain on existing facilities. It assures that new development pay for its proportionate share of capital costs. Impact fees can only be used for system, and not project facilities and must be capital facilities with a useful life of ten years or longer. Impact fees cannot be used for operating costs.

Based on Utah Code requirements, §11-36a, a community must prepare an impact fee facilities plan and impact document before enacting impact fees.

SPECIAL ASSESSMENT AREAS

Special Assessment Areas (“SAAs”), formerly known as Special Improvement Districts or “SID”s, are a financing mechanism that allows governmental entities to designate a specific area for the purpose of financing the costs of improvements, operation and maintenance, or economic promotion activities that benefit property within the area. Entities can then levy a special assessment, on parity with a tax lien, to pay for those improvements or ongoing maintenance. The special assessment can be pledged to retire bonds, known as Special Assessment Bonds, if issued to finance construction of a project. Utah Code §11-42 deals with the requirements of special assessment areas.

The underlying rationale of an SAA is that only those property owners who benefit from the public improvements and ongoing maintenance of the properties will be assessed for the associated costs as opposed to other financing structures in which all City residents pay either through property taxes or increased service fees. **If the boundaries of the SAA were coterminous with that of the City, the SAA would provide no advantage in terms of funding to the City. Therefore, this method is not recommended as a potential source of funding.** If the City were to joint with neighboring cities to create a special recreation district, property tax revenues would be a better source of financing than special assessments.

While not subject to a bond election as is required for the issuance of General Obligation bonds, SAAs may not be created if 40 percent or more of those liable for the assessment payment³ protest its creation. Despite this legal threshold, most local government governing bodies tend to find it difficult to create an SAA if 10-20 percent of property owners oppose the SAA.

Once created, an SAA’s ability to levy an assessment has similar collection priority / legal standing as a property tax assessment. However, since it is not a property tax, any financing secured by that levy would likely be done at higher interest rates than general obligation, sales tax revenue or utility revenue bonds. Interest rates will depend on a number of factors including the ratio of the market value to the assessment bond amount, the diversity of property ownership and the perceived willingness and ability of property owners to make the assessment payments as they come due. Even with the best of special assessment credit structure, if bonds are issued they are likely to be non-rated and therefore would be issued at rates quite a bit higher than similar General Obligation Bonds that would likely be rated. All improvements financed via an SAA must be owned by the City and the repayment period cannot exceed twenty (20) years.

Whenever SAAs are created, entities have to select a method of assessment (i.e. per lot, per unit (ERU), per acre, by front-footage, etc.) which is reasonable, fair and equitable to all property owners within the SAA. State law does not allow property owned by local government entities such as cities or school districts to be assessed.

Advantages of Special Assessment Areas:

- Bonds are tax-exempt although the interest cost is not as low as a GO or revenue bond
- No requirement to hold a bond election but the City must hold a meeting for property owners to be assessed before the SAA can be created
- Only benefited property owners pay for the improvements or ongoing maintenance
- Limited risk to the City as there is no general tax or revenue pledge
- Flexibility since property owners may pre-pay their assessment prior to bond issuance or annually thereafter as the bond documents dictate - if bonds are issued

Disadvantages of Special Assessment Areas:

- Forty percent of the assessed liability, be it one property owner or many could defeat the effort to create the SAA if they do not want to pay the assessment
- Some increased administrative burden for the City although State law permits an additional amount to be included in each assessment to either pay the City’s increased administrative costs or permit the City to hire an outside SAA administrator
- The City cannot assess certain government-owned property within the SAA
- No real funding benefit to the City since the boundary would be the same as the City.

SUMMARY OF POTENTIAL FUNDING SOURCES

Funding Source	Availability	Strengths	Weaknesses	Comments
Monthly Fees	City must enact	New revenue source; would grow annually with household growth	Additional fee on property owners	Could provide a steady revenue stream for operating costs
Local Sales Tax	Provides annual revenue stream, or if used for a bond the debt is repaid through sales tax revenues. City cannot raise existing sales tax beyond the level state legislature allows.	Flexible; no voter approval required	Not a new funding source; rather diverts existing funds. Legislature would need to approve hike in local option sales tax rate in order to increase rate	If a sales tax bond is issued, revenues should be used for capital costs; revenues can be used for any City purpose without a bond.
Recreation Zoo Arts and Parks (ZAP) Tax	Already enacted in Lindon City	No new fees or taxes required	Not a new revenue stream	
General Obligation (GO) Bond	Would need voter approval	Lowest interest rate on debt	Requires voter approval. Can be placed on ballot by City Council (referendum) or through citizen-initiated ballot measure.	Revenues need to be used for capital costs
Lease Revenue Bond	City must appropriate funds annually	Flexible; no voter approval required	Not a new funding source; City must make annual appropriations	Generally used for “essential” facilities
User Fees	“Pay to Play” User Recreation Fee (daily or annual fee).	Simple and direct; Could generate revenue from regional users;	Requires an updated fee study to show the reasonableness of the fees charged compared to the services provided; be sure collection of the fees is not more costly than fees collected	1. City would need to establish cost recovery levels for various programs 2. Could have tiered resident/non-resident fees.
Local Recreation District	City could create a special service district for parks and recreation	Can spread costs over a larger geographic area and population	Could reduce local control because the District would be shared with other cities.	Could be advantageous if combined with other communities because costs would be spread over a larger area
Foundations and Donations	Very competitive and annual allocations change	New revenue stream	Competitiveness in obtaining this resource	
Joint Funding Partnerships	An available option, could take several forms such as Intergovernmental Agreement or Local District.	Additional resources available	May lose some control of facilities or governance	
Grants and Other Funding Sources	Very competitive and annual allocations change	New revenue stream	Competitiveness in obtaining this resource	
Impact Fees	Can only be charged to new development; must have a current IFFP and IFA to meet legal requirements	Ensures that new development pays its proportionate share of the cost of new capital facilities	Developers may contest these fees; cost of having an updated IFFP and IFA prepared	
Special Assessment Area (SAA)	Can create for any size area	Assessments on property; can foreclose	Extremely difficult to obtain approval from all affected properties	High protests likely from high number of property owners; no advantage to creating district if it is coterminous with City boundaries

³ Based on the method of assessment selected, i.e. acreage, front footage, per lot, etc.

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2020 LINDON CITY PARKS MASTER PLAN

05 | APPENDIX



**Lindon City
Parks, Trails and Recreation
Impact Fee Analysis**

ZIONS  PUBLIC FINANCE, INC.

February 18, 2020



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Summary of Impact Fee Analysis (IFA)

Lindon City (“City”) has completed a Parks, Trails and Recreation Impact Fee Facilities Plan which, along with input from the City, forms the basis for this impact fee analysis. The City has determined that there is one service area citywide and that only residential development is considered to create demand for parks, trails and recreation facilities and therefore only residential growth has been considered in the determination of impact fees.

Projections for population growth in the City are as follows:

TABLE 1: PROJECTED POPULATION GROWTH, 2019-2029

Year	Population
2019	11,353
2020	11,512
2021	11,673
2022	11,836
2023	12,002
2024	12,170
2025	12,340
2026	12,513
2027	12,688
2028	12,866
2029	13,046

Source: Lindon City

This IFA is organized based on the legal requirements of Utah Code 11-36a-304.

Impact on Consumption of Existing Capacity - *Utah Code 11-36a-304(1)(a)*

The IFFP considers only *system* facilities in the calculation of impact fees. For the City, this has been determined to mean community parks. Neighborhood parks are considered *project* improvements and have not been included in the calculation of impact fees.

Existing service levels are based on the (2019) levels of service in the City for both parks and trails.¹ Existing and proposed service levels are shown in the table below.

TABLE 2: EXISTING AND PROPOSED SERVICE LEVELS

	UNITS		
	Existing	Proposed (Minimum)	Excess Capacity
Land (acres, with improvements, per 1,000 population)	6.22	6.22	0
Trail miles (trail miles per 1,000 population)	0.33	0.33	0

*The standard is 0.25 trail miles per 1,000 population for 10’ trails and 0.09 trail miles per 1,000 population for 8’ trails.

¹ The IFFP shows a different service level than the Master Plan. This difference occurs for two reasons: 1) the IFA cannot include project improvements (i.e., neighborhood parks), which are included in the level of service in the Master Plan; and 2) the IFA does not include properties that have been donated or gifted in the calculation of the impact-fee eligible level of service.



The City intends to at least maintain existing service levels for both parks and trails.

The Aquatic Center currently serves 11,353 residents and has capacity to serve 17,000. Therefore, the Aquatic Center has excess capacity.

Impact on System Improvements by Anticipated Development Activity - Utah Code 11-36a-304(1)(b)

The table below shows the declining service levels that would occur in Lindon, due to population growth, if no new facilities are added. Each of these declining service levels is discussed in more detail in the body of this report.

TABLE 3: IMPACTS TO SERVICE LEVELS DUE TO NEW DEVELOPMENT IF NO IMPROVEMENTS ARE MADE

Summary Table	Parks	Trails 10'	Trails 8'
Acres/Trail Miles per 1000 Population in 2019	6.22	0.25	0.09
Acres/Trail Miles per 1000 Population in 2029	5.42	0.21	0.08
Cost per Capita Investment in 2019	\$1,868.21	\$182.31	\$55.81
Cost per Capita Investment in 2029	\$1,625.77	\$158.65	\$48.57

Relationship of Anticipated Impacts to Anticipated Development Activity - Utah Code 11-36a-304(1)(c)

The demand placed on existing public park, trails and recreation facilities by new development activity is attributable to population growth. Lindon has a 2019 population of 11,353 persons and as a result of anticipated development activity will grow to a projected 13,046 persons by 2029 – an increase of 1,693 persons. As growth occurs as a result of increased development activity, more parks and trails are needed to maintain existing service levels and to reach proposed service levels.

In order to maintain the existing level of service, the projected new development over the next ten years will require the construction or acquisition of new facilities in the amount of \$3,566,010.65, as stated in 2019 dollars.

TABLE 4: NEW FACILITIES NEEDED TO MEET THE DEMANDS OF NEW GROWTH, 2019-2029

Description	Amount
Parks	\$3,162,875.93
Trails	\$403,134.72
Total	\$3,566,010.65

Proportionate Share Analysis - Utah Code 11-36a-304(1)(d)(i)(ii)

Costs Reasonably Related to New Development Activity

The cost of new system improvements required to maintain the service levels related to new development activity are based on the costs of system-wide park and trail facilities, and the consultant fees for the preparation of the Impact Fee Facilities Plan and the Impact Fee Analysis.

The total gross fee is \$1,788.41 per capita. However, the actual fee charged will be based on the average household size of a residential unit.



TABLE 5: CALCULATION OF GROSS IMPACT FEE

Summary of Gross Fee	
Aquatic Center	\$109.66
Parks	\$1,868.21
Trails	\$238.12
Consultant Cost	\$2.95
Fund Balance	(\$430.53)
Cost per Capita	\$1,788.41

The City may choose to either charge one fee for every type of residential unit, or it can charge different fees for single-family and multi-family units. The average household size for residential units in Lindon is as follows:

TABLE 6: AVERAGE HOUSEHOLD SIZE

Residential Unit Type	Household Size
Single-Family Units	3.90
Multi-Family Units	3.77

Source: American Community Survey 2017

The maximum impact fees for single-family and multi-family residential are shown in the table below:

TABLE 7: MAXIMUM IMPACT FEES

MAXIMUM GROSS FEE	Amount
Single-Family Residential	\$6,974.81
Multi-Family Residential	\$6,742.31

Manner of Financing - Utah Code 11-36a-304(2)(c)(d)(e)(f)(g)(h)

An impact fee is a one-time fee that is implemented by a local government on new development to help fund and pay for all or a portion of the costs of public facilities that are needed to serve new development. Additionally, impact fees allow new growth to share in the cost of existing facilities that have excess capacity.

Impact Fee Credits

The Impact Fees Act requires credits to be paid back to development for future fees that may be paid to fund system improvements found in the IFFP so that new development is not charged twice.

Extraordinary Costs and Time Price Differential

It is not anticipated that there will be any extraordinary costs in servicing newly-developed park properties.

Utah Code 11-36a

Preparation of Impact Fee Analysis. Utah Code requires that “each local political subdivision... intending to impose an impact fee shall prepare a written analysis (Impact Fee Analysis or IFA) of each impact fee” (Utah Code 11-36a-303). This IFA follows all legal requirements as outlined below. Lindon City has retained Zions Public Finance, Inc. (ZPFI) to prepare this Impact Fee Analysis in accordance with legal requirements.

Section 11-36a-304 of the Utah Code outlines the requirements of an impact fee analysis which is required to identify the following:

- anticipated impact on or consumption of any existing capacity of a public facility by the anticipated development activity;
- anticipated impact on system improvements required by the anticipated development activity to maintain the established level of service for each public facility;
- how anticipated impacts are reasonably related to the anticipated development activity
- the proportionate share of:
 - costs for existing capacity that will be recouped; and
 - costs of impacts on system improvement that are reasonably related to the new development activity; and
 - how the impact fee was calculated

Further, in analyzing whether or not the proportionate share of the costs of public facilities are reasonably related to the new development activity, the local political subdivision or private entity, as the case may be, shall identify, if applicable:

- the cost of each existing public facility that has excess capacity to serve the anticipated development resulting from the new development activity;
- the cost of system improvements for each public facility;
- other than impact fees, the manner of financing for each public facility such as user charges, special assessments, bonded indebtedness, general taxes, or federal grants;
- the relative extent to which development activity will contribute to financing the excess capacity of and system improvements for each existing public facility, by means such as user charges, special assessments, or payment from the proceeds of general taxes;
- the relative extent to which development activity will contribute to the cost of existing public facilities and system improvements in the future;

the extent to which the development activity is entitled to a credit against impact fees because the development activity will dedicate system improvements or public facilities that will offset the demand for system improvements, inside or outside the proposed development;

extraordinary costs, if any in servicing the newly developed properties; and

the time-price differential inherent in fair comparisons of amounts paid at different times.

Calculating Impact Fees. Utah Code 11-36a-305 states that for purposes of calculating an impact fee, a local political subdivision or private entity may include the following:

- construction contract price;
- cost of acquiring land, improvements, materials, and fixtures;
- cost for planning, surveying, and engineering fees for services provided for and directly related to the construction of the system improvements; and
- for a political subdivision, debt service charges if the political subdivision might use impact fees as a revenue stream to pay the principal and interest on bonds, notes or other obligations issued to finance the costs of the system improvements.

Additionally, the Code states that each political subdivision or private entity shall base impact fee amounts on realistic estimates and the assumptions underlying those estimates shall be disclosed in the impact fee analysis.

Certification of Impact Fee Analysis. Utah Code 11-36a-306 states that an impact fee analysis shall include a written certification from the person or entity that prepares the impact fee analysis. This certification is included at the conclusion of this analysis.

Impact Fee Enactment. Utah Code 11-36a-202 states that a local political subdivision or private entity wishing to impose impact fees shall pass an impact fee enactment in accordance with Section 11-36a-402. Additionally, an impact fee imposed by an impact fee enactment may not exceed the highest fee justified by the impact fee analysts. An impact fee enactment may not take effect until 90 days after the day on which the impact fee enactment is approved.

Notice of Intent to Prepare Impact Fee Analysis. A local political subdivision must provide written notice of its intent to prepare an IFA before preparing the Analysis (Utah Code 11-36a-503(1)). This notice must be posted on the Utah Public Notice website. The City has complied with this noticing requirement for the IFA by posting notice.



Impact Fee Analysis

Utah Code allows cities to include only system-wide parks for the purpose of calculating impact fees. Project-wide parks cannot be used to establish levels of service eligible to be maintained through impact fees. Based on input from the City and the consultants, a system-wide park is defined as a park that serves more than one local development area. System-wide parks in Lindon include community parks.

This IFA is organized based on the legal requirements of Utah Code 11-36a-304.

1 Impact on Consumption of Existing Capacity

Utah Code 11-36a-304(1)(a): an impact fee analysis shall identify the anticipated impact on or consumption of any existing capacity of a public facility by the anticipated development activity

Demand Placed on Facilities by New Development Activity

Park Land and Park Improvements

Lindon City currently has 70.66 systemwide park acres that qualify to be included in the calculation of impact fees. The existing level of service for parks then, for the purpose of calculating impact fees, is 6.22 acres per 1,000 residents, calculated by dividing the 70.66 eligible park acres by the 2019 population of 11,353 residents (which has been divided by 1,000).

The table below summarizes the improvements, along with the costs, that were used to determine an existing standard for park land and improvements. Cost estimates have been provided in consultation with the City.

TABLE 8: SYSTEM PARK LAND AND IMPROVEMENTS

	Units	Cost per Unit	Total Investment
Land (Acres)	70.66	\$201,435	\$14,233,391.24
Restrooms	5	\$150,000	\$750,000.00
Pavilions – Small	2	\$50,000	\$100,000.00
Pavilions – Medium	5	\$75,000	\$375,000.00
Pavilions – Large	3	\$150,000	\$450,000.00
Picnic Tables	91	\$1,000	\$91,000.00
Playgrounds	6	\$100,000	\$600,000.00
Exercise Equipment	0	\$9,000	\$0.00
Pickle Ball Courts	6	\$50,000	\$300,000.00
Tennis Courts	1	\$75,000	\$75,000.00
Basketball Courts	0	\$20,000	\$0.00
Soccer Fields (not incl. sod)	7	\$2,000	\$14,000.00
Ball Diamonds (not incl. sod)	4	\$20,000	\$80,000.00
Sand Volleyball	0	\$25,000	\$0.00
Riding Arena – sf	27,243	\$2.25	\$61,296.75
Lighted Paths	3,850	\$50.00	\$192,500.00
Paved Paths	1,884	\$15	\$28,260.00
Regional Trail Access	0	\$200,000	\$0.00
Asphalt (parking) sf	63,530	\$5.00	\$317,650.00



	Units	Cost per Unit	Total Investment
Mowed sf***	1,770,833	\$2.00	\$3,541,666.00
TOTAL Parks			\$21,209,763.99

With 70.66 existing park acres, the average cost for land and improvements is \$300,166.49 per acre. Land costs are based on the recent sale of 9.05 acres to Vineyard for a sum of \$1,822,986, or a cost of \$201,434.92 per acre.

Existing park service levels will decline, due to new development activity, from the existing service level of 6.22 acres per 1,000 persons to 5.42 acres per 1,000 residents by 2029 if no improvements are made.

TABLE 9: PARK LAND AND IMPROVEMENT SERVICE LEVEL IMPACTS FROM NEW DEVELOPMENT ACTIVITY, 2019-2029

Year	Population	Population Growth	Land Acres per 1000 Persons if No New Facilities
2019	11,353		6.22
2020	11,512	159	6.14
2021	11,673	161	6.05
2022	11,836	163	5.97
2023	12,002	166	5.89
2024	12,170	168	5.81
2025	12,340	170	5.73
2026	12,513	173	5.65
2027	12,688	175	5.57
2028	12,866	178	5.49
2029	13,046	180	5.42

Trails – 10' Width

The City currently has 2.8 miles of 10' wide trails, or 14,784 linear trail feet. This results in an existing service level of 0.25 linear trail feet per capita, calculated by dividing the 2.8 miles by the 2019 population.

The existing level of service of 0.25 linear trail miles (10' wide trails) per capita will decline to 0.21 linear trail miles per capita, over the next 10 years, if no new improvements are made.

Table 10: 10' Trail Service Level Impacts from New Development Activity, 2019-2029

Year	Population	Population Growth	Unit Service Levels per 1000 Persons if No New Facilities
2019	11,353		0.25
2020	11,512	159	0.24
2021	11,673	161	0.24
2022	11,836	163	0.24
2023	12,002	166	0.23
2024	12,170	168	0.23
2025	12,340	170	0.23
2026	12,513	173	0.22
2027	12,688	175	0.22
2028	12,866	178	0.22
2029	13,046	180	0.21



The City currently has 1 mile of 8' wide trails, or 5,280 linear trail feet. This results in an existing service level of 0.09 linear trail feet per capita, calculated by dividing the 1 mile by the 2019 population.

The existing level of service of 0.09 linear trail miles (8' wide trails) per capita will decline to 0.08 linear trail miles per capita, over the next 10 years, if no new improvements are made.

Table 11: 8' Trail Service Level Impacts from New Development Activity, 2019-2029

Year	Population	Population Growth	Unit Service Levels per 1000 Persons if No New Facilities
2019	11,353		0.09
2020	11,512	159	0.09
2021	11,673	161	0.09
2022	11,836	163	0.08
2023	12,002	166	0.08
2024	12,170	168	0.08
2025	12,340	170	0.08
2026	12,513	173	0.08
2027	12,688	175	0.08
2028	12,866	178	0.08
2029	13,046	180	0.08

Aquatic Center

The Aquatic Center has capacity to serve 17,000 persons. At the present time it is serving 11,353 persons, representing 67 percent of total capacity. By 2029, the Aquatic Center will serve 13,046 persons, representing 77 percent of total capacity; therefore, 10 percent of the excess capacity will be consumed by new development between 2019 and 2029.

2 Impact on System Improvements by Anticipated Development Activity

Utah Code 11-36a-304(1)(b): an impact fee analysis shall identify the anticipated impact on system improvements required by the anticipated development activity to maintain the established level of service for each public facility;

The City will need to acquire additional park lands and trail miles and make park and trail improvements to maintain its existing service levels of improved parks. Impact fees will be used to maintain the existing service levels for parks and trails.

The figures in the following table were calculated by multiplying the existing service levels by the cost for each line item by the projected growth in demand over the next ten years.

Total costs anticipated over the next 10 years to maintain existing service levels given the projected growth in the City, is \$3,566,011.

TABLE 12: SUMMARY OF NEW FACILITIES NEEDED TO MEET THE DEMANDS OF NEW GROWTH, 2019-2029

Summary of Needs	Acres/Miles Needed 2019-2029	Cost per Unit	Total Cost 2019-2029
Parks	10.54	\$300,166	\$3,162,876



Summary of Needs	Acres/Miles Needed 2019-2029	Cost per Unit	Total Cost 2019-2029
Trails - 10' width	0.42	\$739,200	\$308,650
Trails - 8' width	0.15	\$633,600	\$94,485
TOTAL			\$3,566,011

3 Relationship of Anticipated Impacts to Anticipated Development Activity

Utah Code 11-36a-304(1)(c): an impact fee analysis shall subject to Subsection (2), demonstrate how the anticipated impacts described in Subsections (1)(a) and (b) are reasonably related to the anticipated development activity;

The demand placed on existing public park facilities by new development activity is attributable to population growth. Lindon has a 2019 population of 11,353 persons and as a result of anticipated development activity will grow to a projected 13,046 persons by 2029 – an increase of 1,693 persons. As growth occurs, more parks and trails are needed to maintain existing service levels and to reach proposed service levels.

4 Proportionate Share Analysis

Utah Code 11-36a-304(1)(d)(i)(ii): an impact fee analysis shall estimate the proportionate share of costs for existing capacity that will be recouped; and the costs of impacts on system improvements that are reasonably related to the new development activity;

Costs Reasonably Related to New Development Activity

The cost of new system improvements required to maintain the existing level of parks, trails and recreation services related to new development activity is based on the cost of system-wide park and trail facilities, as well as consultant fees for the preparation of the Impact Fee Facilities Plan and the Impact Fee Analysis.

The City will need to acquire an additional 10.54 acres of land over the next 10 years in order to maintain its existing service level of 5.92 acres per 1,000 persons. At a cost of \$300,166.49 per acre, including improvements, the cost to the City will be \$3,162,875.93. The proportionate share per capita, for new development, is \$1,868.21.

TABLE 13: PER CAPITA COST TO MAINTAIN LOS FOR PARK LAND AND IMPROVEMENTS

Park Land and Improvements	
Investment Required, 2019-2029	\$3,162,875.93
Population Growth, 2019-2029	1,693
Cost per Capita	\$1,868.21

The per capita cost to maintain the existing level of service for 10' trails is \$182.31.

TABLE 14: PER CAPITA COST TO MAINTAIN LOS FOR 10' TRAILS

Description	Amount
Investment Required, 2019-2029	\$308,650.02
Population Growth, 2019-2029	1,693
Cost per Capita	\$182.31

The per capita cost to maintain the existing level of service for 8' trails is \$55.81.

TABLE 15: PER CAPITA COST TO MAINTAIN LOS FOR 8' TRAILS

Description	Amount
Investment Required, 2019-2029	\$94,484.70
Population Growth, 2019-2029	1,693
Cost per Capita	\$55.81

The total cost of the Aquatic Center is \$10,756,223. The Aquatic Center is funded through an outstanding bond, with \$8,891,925 remaining in principal payments. Therefore, only 17.3 percent of bond payments have been made to date. With a total capacity that will serve 17,000 residents, this represents a per capita investment of \$109.66 to date.

TABLE 16: PER CAPITA COST FOR AQUATIC CENTER

Description	Amount
Total Cost	\$10,756,223
Total Capacity – population	17,000
Total Cost per Capita	\$632.72
Bond Payments Made	\$1,864,298
Bond Amount Remaining	\$8,891,925
Current LOS - Investment Made	\$109.66

The Impact Fee Facilities Plan and Impact Fee Analysis consultant cost is \$2.95 per capita.

TABLE 17: PER CAPITA CONSULTANT COSTS

Description	Amount
Consultant Costs	\$5,000
Population Growth 2019-2029	1,693
Cost per Capita	\$2.95

The total gross Parks, Trails and Recreation Impact Fee is \$1,788.41 per capita.

TABLE 18: MAXIMUM ALLOWABLE IMPACT FEE

Description	Amount
Aquatic Center	\$109.66
Parks	\$1,868.21
Trails	\$238.12
Consultant Cost	\$2.95

Description	Amount
Fund Balance	(\$430.53)
Cost per Capita	\$1,788.41

The maximum fee per capita is then multiplied by the household size for single-family and multi-family residential units.

TABLE 19: HOUSEHOLD SIZE

Household Type	Household Size
Single-Family	3.90
Multi-Family	3.77

Source: American Community Survey 2017

Maximum impact fees for single-family and multi-family residential are \$6,974.81 and \$6,742.31 respectively.

TABLE 20: MAXIMUM IMPACT FEE

Development Type	Amount
Single-Family Residential	\$6,974.81
Multi-Family Residential	\$6,742.31

Impact Fee Credits

5

Utah Code 11-36a-304(1)(e): an impact fee analysis shall, based on the requirements of this chapter, identify how the impact fee was calculated;

There are no bonds outstanding on parks or trails facilities and therefore no credits need to be made against the parks and trails portion of the fee. However, there is a bond outstanding on the Aquatic Center. Therefore, new development has only been charged for the current principal payments made on the Aquatic Center in order to ensure that new development only pays for the current level of service that has been borne by existing residents.

6

Manner of Financing

Utah Code 11-36a-304(2)(c)(d)(e)(f)(g)(h): an impact fee analysis shall identify, if applicable: other than impact fees, the manner of financing for each public facility such as user charges, special assessments, bonded indebtedness, federal taxes, or federal grants;

An impact fee is a one-time fee that is implemented by a local government on new development to help fund and pay for all or a portion of the costs of public facilities that are needed to serve new development. These fees are usually implemented to help reduce the economic burden on local jurisdictions that are trying to deal with population and commercial growth within the area. As a matter of policy and legislative discretion, a City may choose to have new development pay the full cost of its share of new public facilities if the facilities would not be needed except to service new development. However, local governments may use other sources of revenue to pay for the new facilities required to service new development and

use impact fees to recover the cost difference between the total cost and the other sources of revenue. Additionally, impact fees allow new growth to share in the cost of existing facilities that have excess capacity.

At the current time, no other sources of funding other than impact fees have been identified, but to the extent that any are identified and received in the future, then impact fees will be reduced accordingly.

Additional system-wide park land and recreation facility improvements beyond those funded through impact fees that are desired to maintain a higher proposed level of service will be paid for by the community through other revenue sources such as user charges, special assessments, GO bonds, general taxes, etc.

Impact Fee Credits

The Impact Fees Act requires credits to be paid back to development for future fees that may be paid to fund system improvements found in the IFFP so that new development is not charged twice. Credits may also be paid back to developers who have constructed or directly funded items that are included in the IFFP or donated to the City in lieu of impact fees, including the dedication of land for system improvements. This situation does not apply to developer exactions or improvements required to offset density or as a condition for development. Any item for which a developer receives credit should be included in the IFFP and must be agreed upon with the City before construction begins.

In the situation that a developer chooses to construct facilities found in the IFFP in lieu of impact fees, the arrangement must be made through the developer and the City.

Extraordinary Costs and Time Price Differential

It is not anticipated that there will be any extraordinary costs in servicing newly developed park properties.

Certification

Zions Public Finance, Inc. certifies that the attached impact fee analysis:

1. includes only the costs of public facilities that are:
 - a. allowed under the Impact Fees Act; and
 - b. actually incurred; or
 - c. projected to be incurred or encumbered within six years after the day on which each impact fee is paid;
2. does not include:
 - a. costs of operation and maintenance of public facilities;
 - b. costs for qualifying public facilities that will raise the level of service for the facilities, through impact fees, above the level of service that is supported by existing residents;
 - c. an expense for overhead, unless the expense is calculated pursuant to a methodology that is consistent with generally accepted cost accounting practices and the methodological standards set forth by the federal Office of Management and Budget for federal grant reimbursement;
3. offsets costs with grants or other alternate sources of payment; and

4. complies in each and every relevant respect with the Impact Fees Act.

Lindon Parks, Trails, and Recreation Community Survey

Q1 What are your favorite parks, trails, or recreation facilities in Lindon and why?

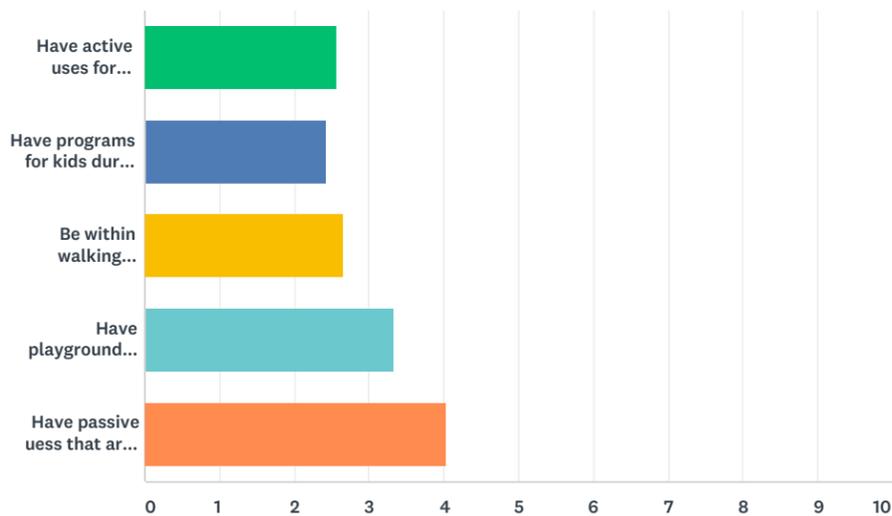
Answered: 184 Skipped: 27



Lindon Parks, Trails, and Recreation Community Survey

Q2 Please rank the following in order of priority. Lindon's parks, trails, and recreation facilities should...

Answered: 208 Skipped: 3

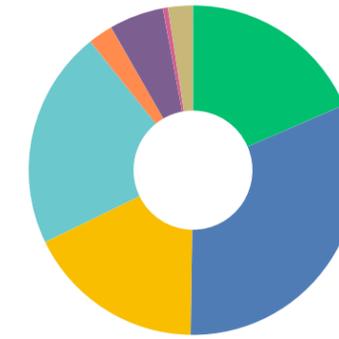


	1	2	3	4	5	TOTAL	SCORE
Have active uses for organized sports and paid programs through user fees	7.00% 14	20.50% 41	21.50% 43	26.00% 52	25.00% 50	200	2.58
Have programs for kids during summer and non-school times	6.97% 14	14.43% 29	17.91% 36	34.83% 70	25.87% 52	201	2.42
Be within walking distance of my residence	12.81% 26	19.70% 40	19.70% 40	14.78% 30	33.00% 67	203	2.65
Have playground equipment	16.08% 32	31.66% 63	29.15% 58	15.08% 30	8.04% 16	199	3.33
Have passive uses that are open to the whole community (open lawns, forested areas, picnic areas)	56.93% 115	13.86% 28	12.87% 26	8.42% 17	7.92% 16	202	4.03

Lindon Parks, Trails, and Recreation Community Survey

Q3 How often do you frequent these facilities?

Answered: 205 Skipped: 6



Several times a week (5-7) A few times a week (2-4) Once a week
A few times a month Once a month A few times a year Once a year
Never

Lindon Parks, Trails, and Recreation Community Survey

Q4 What are your favorite parks, trails, or recreation facilities outside of Lindon and why?

Answered: 172 Skipped: 39

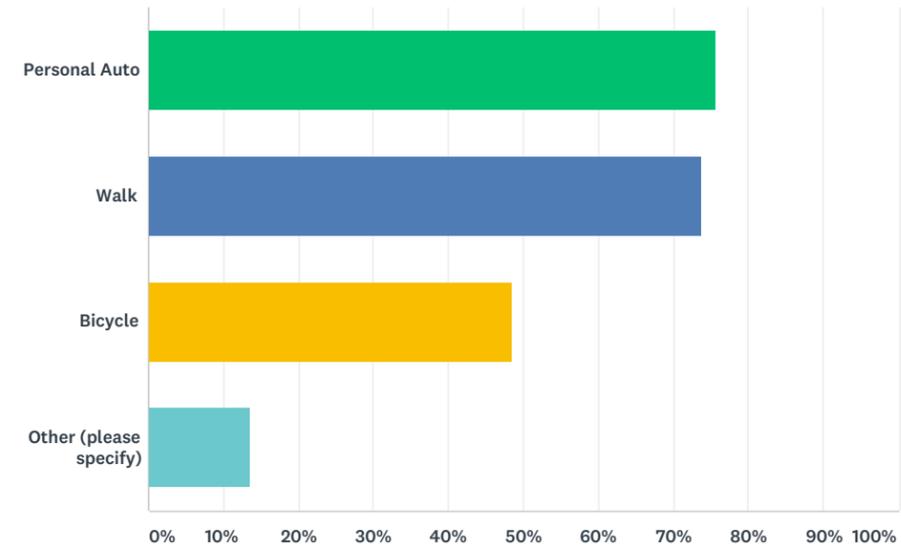


Q5 Please check all of the items that prevent you or your family from using Lindon's parks, trails, and recreation facilities.

Q7 How do you travel to Lindon's parks, trails, and recreation facilities?

Answered: 198 Skipped: 13

Answered: 206 Skipped: 5



ANSWER CHOICES	RESPONSES	
I'm unfamiliar with the facilities available	17.68%	35
Prefer to use private facilities or facilities in other cities	11.11%	22
Inconvenient locations	9.09%	18
Facilities lack the right equipment/amenities	33.33%	66
I do not feel safe	4.04%	8
Not accessible for people with disabilities	2.02%	4
Quality of facilities	10.61%	21
Nothing --I am a regular user	42.42%	84
Other (please specify)	18.18%	36
Total Respondents: 198		

ANSWER CHOICES	RESPONSES	
Personal Auto	75.73%	156
Walk	73.79%	152
Bicycle	48.54%	100
Other (please specify)	13.59%	28
Total Respondents: 206		

Q6 What would you like to improve regarding Lindon's parks, trails, and recreation facilities?

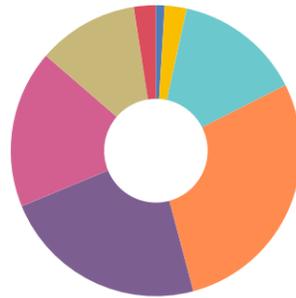
Answered: 175 Skipped: 36



Lindon Parks, Trails, and Recreation Community Survey

Q9 How old are you?

Answered: 205 Skipped: 6



ANSWER CHOICES	RESPONSES	
0-12 years old	0.00%	0
12-17 years old	0.98%	2
18-24 years old	2.44%	5
25-34 years old	14.15%	29
35-44 years old	28.29%	58
45-54 years old	22.93%	47
55-64 years old	17.56%	36
65+ years old	11.22%	23
Prefer Not to Say	2.44%	5
TOTAL		205

Lindon City
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Item 6: Industrial Building Design Standards

This item is for discussion purposes only and no motion is required. Staff and the planning commission have recently discussed reviewing the building design requirements for industrial buildings. The Planning Department has provided a number of industrial building design standards from four different Utah communities for review. Staff has also provided photos of industrial buildings that have been constructed in Lindon over approximately the past 10 years.

Comparison of Industrial Buildings

Building Orientation

- **(Lehi)** Buildings located on corner lots shall be oriented to both streets;
- **(Lehi)** Uses that are industrial in nature shall be oriented toward the public street;
- **(South Salt Lake)** Buildings oriented so the primary façade faces the street from which the building derives its street address;
- Lindon: No Requirement

Building Entrances

- **(Draper)** All building entrances shall be clearly articulated to indicate a transition from the exterior to the interior of the building. Every main entrance shall have a special emphasis when compared to the other portions of the building. This shall be accomplished through the use of at least three (3) of the following near entrances:
 1. A prominent architectural feature that is unique to the overall building design;
 2. Complementary yet differing building materials or colors;
 3. Increased use of windows or glass;
 4. Pedestrian amenities that may include patios, porches, special paving treatments, seating areas, or awnings; or
 5. Increased landscaping.
- **(Sandy)** Entries to industrial warehouse buildings should portray a quality office appearance while being architecturally related to the overall building composition
- **(Lehi)**
 1. Buildings shall have hierarchal massing at building entrances. Building entrances shall include at least one the following features: roof tower feature, pitched roof feature parapet extensions, and articulations in the façade.
 2. The hierarchal architectural features around building entrances shall also include at least two of the following features:
 - a. differing exterior material types;
 - b. awnings or canopies;
 - c. decorative lighting; and
 - d. increased amount of glass such as side or transom windows.
 3. Public entrances shall be developed on all new buildings to face the adjacent street. Building entrances may be allowed on a side of the building closest to the street if an unobstructed sidewalk connection is provided from the building entrance to the street sidewalk
- **(SSL)** Any office portion of a structure using the flex/industrial building form shall employ human-scale design along with a variety of architectural detail to break up large walls or enhance visual quality.
- Lindon: no requirement

Building Design

- **(Draper)** Façade articulation. Buildings designed with completely flat facades and monotone color schemes are not permitted. All buildings shall demonstrate articulation of all facades.
 1. Horizontal or vertical facade variations shall occur at least every thirty feet (30') or along a minimum of sixty percent (60%) of the horizontal length of buildings with facades one hundred feet (100') or greater. This shall be accomplished by using methods such as:
 - a. Variation in the surface plane that may include pop outs, bays, and recesses;
 - b. Variation in the surface pattern such as arches, banding, and paneling; or
 - c. Distinguished treatment of windows, doors, and eaves that may include molding or framing.
 2. Buildings with facades one hundred feet (100') or greater in total length shall have at least one significant facade variation from the primary wall plane whose depth is at least five feet (5') and whose width is at least twenty feet (20'). There shall be no uninterrupted facades one hundred feet (100') or greater in length.
- **(SSL)** Primary Façades. Primary façades on buildings using the flex/industrial building form shall incorporate wall recesses or projections of a minimum depth of two feet at least every sixty (60) feet or for each principal entrance, whichever is greater. In addition, at least two of the following design elements are required:
 1. Variations in roof form and parapet heights;
 2. Distinct changes in texture and color of wall surfaces;
 3. Vertical accents or focal points.
- **(Sandy)** Avoid long (over 200 feet) of unbroken building facades. Facades with varied front setbacks are required. Warehouses should avoid blank front wall elevations on street frontages and those areas visible from streets through the use of indentations and architectural details.
- Lindon: no requirement

Building Materials

- **(Draper)**
 1. Minimum Colors: A minimum of three (3) colors per elevation shall be required.
 2. Primary Materials: Brick, stone, ceramic tile, wood or concrete fiber composite siding, concrete masonry units (CMU), metal composite material, and architectural precast concrete are acceptable primary materials. CMU must be colored and feature decorative or architectural finishes such as honed, scored, offset or split face. Gray CMU block is not an acceptable finished building material and shall not be permitted on any finished building elevation with the exception of minimal foundation exposure. The following guidelines shall apply when designing a commercial or industrial building:
 - a. Each building face shall have a minimum of two (2) primary materials. At least seventy five percent (75%) of each building's finished face shall be in primary materials.
 - b. When brick is used as a primary material, it may be utilized up to one hundred percent (100%) of the facade material, in which case, no secondary primary material will be required.
 - c. Doors, glass and roofing materials shall not be included in the calculations of primary and secondary materials.

3. Secondary Materials: Secondary materials may include stucco; EIFS, including specialty finishes; metal, excluding corrugated sheet metal; exposed concrete; vinyl; shake shingles or wood siding. Materials such as awnings, wood timbers and other similar features will be considered accent and not figured into the totals of secondary materials.

4. Tilt-Up Concrete: Exposed tilt-up concrete may be used as a primary material on buildings located in all manufacturing zones and in the CBP zoning classification. Some variation in materials along the base and near the entrances of concrete tilt-ups is required.

- **(Sandy)**
 1. Preferred Materials: Full veneer brick, Architectural concrete (with recessed panels and reveal lines), Colored CMU block and architectural CMU block (i.e. split face, fluted, scored, honed, etc), Architectural metals & standing seam metal roofing, Metal walls (insulated architectural metal panels) (i.e. aluco bond)
 2. Preferred Accent Materials: Precast concrete accents, Stucco (EIFS) as an accent material (not a major building component). Limited amounts of stucco may be considered for vertical surfaces only, if the quality of the design merits such consideration. Glass accents
 3. Discourage Materials: Plain, grey, flat faced CMU block (allowed as an accent only, not as a total wall treatment), Brick tiles, Metal walls (unless it is an insulated architectural metal panel such as aluco bond), Stucco (EIFS), wood or glass, as more than an accent.

- **(South Salt Lake)** Natural or synthetic stone, Brick, Integral color CMU, High-quality pre-stressed concrete systems, Finished and treated tilt-up concrete panels, Float finish EIFS, or Glass.

- **(Lehi)**
 1. Brick, stone, split face CMU, tilt-up concrete, architectural grade metal (see Figures 126 and 127), fiber cement, and wood may be used for exterior materials.
 2. Stucco and EIFS may be used up to 40 percent of each wall area excluding windows and doors as a secondary material only. Stucco and EIFS shall not be used on hierarchal architectural features, pop-outs in a facade, or for wainscots, but shall be allowed on recessed wall areas, walls between entrances, and between hierarchal architectural feature
 3. Each building facade shall include at least three of the following features:
 - a. columns or pilasters;
 - b. decorative cornice;
 - c. awnings or canopies;
 - d. covered walkways;
 - e. decorative lighting;
 - f. string course of differing color or material;
 - g. wainscot of a minimum 30 inches in height except for under windows; and
 - h. other comparable architectural features as approved by the Zoning Administrator or Planning Commission.

- **Lindon:** 25% of the buildings must be brick, decorative block, stucco, or wood. Planning Commission can grant exceptions to move material to one side or have different exterior materials if additional architectural details are provided. Precast buildings are not required to provide additional architectural detail

Windows

- **(Draper)**
 1. Windows should be at eye level when possible and provide for transparency into the building.
 2. Where buildings are adjacent to pedestrian walkways, transparent windows shall be used to relate the scale of the building and the building's interior to pedestrians. Where transparent windows may not be desirable, tinted windows, false windows, or glass block shall be used.
 3. Window awnings are an effective way to add detail and variation to a building, emphasize pedestrian scale windows, and create an exciting and inviting environment. When placing awnings:
 - a. Quality materials shall be used which are durable and are able to withstand extremes in the weather;
 - b. Colors should be complementary to the color of the building on which the awning is located; and
 - c. Styles should be compatible with the architectural features and overall architecture of the building on which the awning is located.
- **(Lehi)** A minimum of 20 percent of the façade area on the first floor facing the street shall consist of glass. Where a building is located on a corner lot of two local streets, a minimum of 20 percent of each facade on the first floor shall consist of glass.
- **(South Salt Lake)** All façades facing a street or parking lot shall provide windows for a minimum of twenty (20) percent of the façade.
- Lindon: No requirement

Roofline

- **(Draper)** Provide roofline and parapet variations where there are long, continuous, and undisturbed rooflines fifty feet (50') or greater
- **(Lehi)** Buildings shall be designed with roofline variations in each facade over 50 feet in width
- Lindon: No requirement

Metal Buildings

- **(Sandy)**
 1. All metal buildings (where such metal materials are allowed) must be designed to have an exterior appearance of conventionally built structures. Exterior surfaces must include either stucco, plaster, glass, stone, brick, or decorative masonry. Stock, "off the shelf" metal buildings are highly discouraged.
 2. Metal buildings should employ a variety of building forms, shapes, colors, materials and other architectural treatments to add visual interest and variety to the building
- **(South Salt Lake)** Building façades utilizing metal siding, unfinished or untreated tilt-up concrete panels, or standard single- or double-tee concrete systems are not allowed on any façade visible from public or private streets, parking areas, or adjacent residential, institutional, or commercial uses
- Lindon: The Planning Commission may approve ribless, metal, flat-faced, stucco embossed, metal sandwich panel buildings when the Planning Commission finds that the building is aesthetically pleasing, adequately trimmed, contrasted with different colors, is well proportioned, blends in with surrounding property, and has a similar look to that achieved by 17.49.070(1) (25% material requirement).

Examples of Recent Industrial Building Constructed in Lindon

1396 W. 200 S.



1455 W. 200 S.



1261 W. 300 S.



225 S. 1430 W.



1020 W. 600 S



1374 W. 200 S.



158 N. 2000 W.



430 N. 1200 W.



1762 W. 20 S.



1200 E. 400 S.



96 S. 1800 W.



7 S. 1550 W.



215 N. 1800 W.



122 S. 1380 W.



128 S. 1380 W.



105 S. 1380 W.



1545 W. 200 S.



1325 W. Anderson Lane



957 W. 150 N.



950 W. 150 N.



480 N. Geneva Road



101 S. Geneva Road



142 N. 1800 W.



122 N. 1800 W.



967 W. 240 N.



64 N. 1550 W.



175 N. 1800 W.



520 N. Geneva



1507 W. 500 N.



25 W. 600 N.



31 S. 1550 W.

