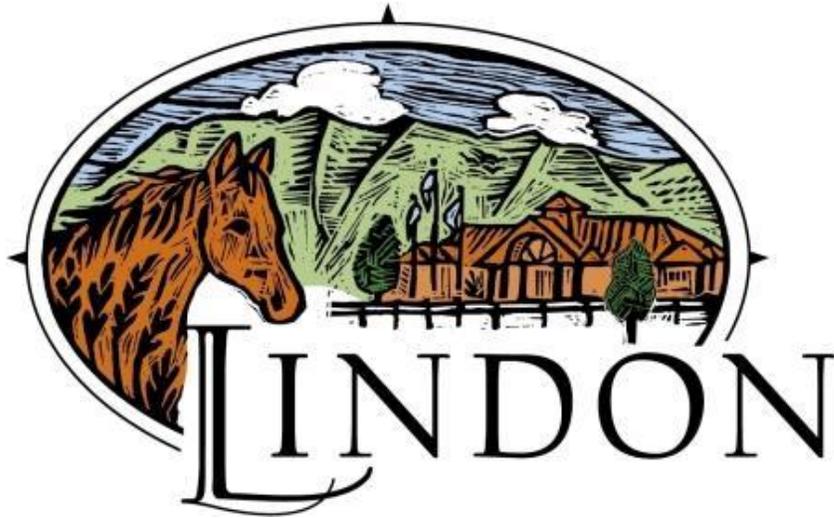


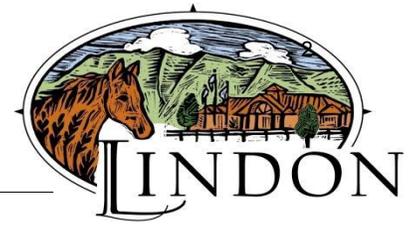
Lindon City Council Staff Report



Prepared by Lindon City
Administration

February 7, 2017

Notice of Meeting of the Lindon City Council



The Lindon City Council will hold a regularly scheduled meeting beginning at **7:00 p.m.** on **Tuesday, February 7, 2017** in the Lindon City Center council chambers, 100 North State Street, Lindon, Utah. The agenda will consist of the following:

Scan or click here for link to download agenda & staff report materials:



(Review times are estimates only)

REGULAR SESSION – 7:00 P.M. - Conducting: Jeff Acerson, Mayor

Pledge of Allegiance: By Invitation

Invocation: Jake Hoyt

1. Call to Order / Roll Call

(2 minutes)

2. Presentations and Announcements

(10 minutes)

- a) Comments / Announcements from Mayor and Council members

3. Approval of minutes: January 17, 2017

(5 minutes)

4. Consent Agenda – No Items

5. Open Session for Public Comment *(For items not on the agenda)*

(10 minutes)

6. Concept Review — Center Street & 25 South Main

(20 minutes)

The Council will hear and provide feedback to the applicant, Bryant Christensen, CL Christensen Brothers, regarding a proposed Senior Apartment and Townhome concept located at Center Street and 25 South Main St. The proposal would require a new ordinance. Portions of the property are in the General Commercial (CG) zone, Senior Housing Overlay (SHFO) zone, and the Single-family Residential (R1-20) zone. The current regulations of the SHFO zone can be found in Lindon City Code 17.75. The General Plan Land Use Map identifies this area as Commercial and Residential Low. The Planning Commission will review the concept at their first meeting in February. No motion is necessary as this item is for discussion only.

7. Discussion Item — General Plan Industrial Zone Review

(30 minutes)

The Council will receive information regarding the current Lindon City General Plan specifically industrial land use designations. No formal action will be taken at this time.

8. Public Hearing — Ordinance Changes to Commercial Design Guidelines –Ordinance #2017-I-O

Lindon City staff requests a revision to Chapter V. Architectural Character regarding massing and orientation; windows and doors; roofing; mechanical areas. Following review the Planning Commission recommended approval with a four to one vote.

(15 minutes)

9. Review and Action — Planning Commissioner Appointment

(5 minutes)

The City Council will review and consider Mayor Acerson's recommendation to appoint Mike Vanchiere of 259 North 1090 East to serve as a new Planning Commissioner for Lindon City starting February 16, 2017.

10. Council Reports:

(20 minutes)

A) MAG, COG, UIA, Utah Lake Commission, ULCT, NUVAS, IHC Outreach, Budget Committee

- Jeff Acerson

B) Public Works, Irrigation/water, City Buildings

- Van Broderick

C) Planning, BD of Adjustments, General Plan, Budget Committee

- Matt Bean

D) Parks & Recreation, Trails, Tree Board, Cemetery

- Carolyn Lundberg

E) Public Safety, Court, Lindon Days, Transfer Station/Solid Waste

- Dustin Sweeten

F) Admin., Community Center, Historic Comm., PG/Lindon Chamber, Budget Committee

- Jacob Hoyt

11. Administrator's Report

(10 minutes)

Adjourn

This meeting may be held electronically to allow a council member to participate by video conference or teleconference. Staff Reports and application materials for the agenda items above are available for review at the Lindon City Offices, located at 100 N. State Street, Lindon, UT. For specific questions on agenda items our staff may be contacted directly at (801)785-5043. City Codes and ordinances are available on the City web site found at www.lindoncity.org. The City of Lindon, in compliance with the Americans with Disabilities Act, provides accommodations and auxiliary communicative aids and services for all those

citizens in need of assistance. Persons requesting these accommodations for city-sponsored public meetings, services programs or events should call Kathy Moosman at 801-785-5043, giving at least 24 hours notice.

CERTIFICATE OF POSTING:

I certify that the above notice and agenda was posted in three public places within the Lindon City limits and on the State (<http://pmn.utah.gov>) and City (www.lindoncity.org) websites.

Posted by: /s/ Kathryn A. Moosman, City Recorder

Date: February 3, 2017

Time: 11:30 a.m.

Place: Lindon City Center, Lindon Police Dept., Lindon Community Center

REGULAR SESSION – 7:00 P.M. - Conducting: Jeff Acerson, Mayor

Pledge of Allegiance: By Invitation

Invocation: Jake Hoyt

Item 1 – Call to Order / Roll Call

February 7, 2017 Lindon City Council meeting.

Jeff Acerson
Matt Bean
Van Broderick
Jake Hoyt
Carolyn Lundberg
Dustin Sweeten

Staff present: _____

Item 2 – Presentations and Announcements

- a) Comments / Announcements from Mayor and Council members.

Item 3 – Approval of Minutes

- Review and approval of City Council minutes: **January 17, 2017**

2 The Lindon City Council held a regularly scheduled meeting on **Tuesday, January 17,**
 4 **2017, beginning at 7:00 p.m.** in the Lindon City Center, City Council Chambers, 100
 North State Street, Lindon, Utah.

6 **REGULAR SESSION** – 7:00 P.M.

8 Conducting: Jeff Acerson, Mayor
 Pledge of Allegiance: Cody Cullimore
 10 Invocation: Jeff Acerson

12 **PRESENT**

EXCUSED

12 Jeff Acerson, Mayor
 14 Van Broderick, Councilmember
 Matt Bean, Councilmember
 16 Carolyn Lundberg, Councilmember
 Jacob Hoyt, Councilmember
 18 Dustin Sweeten, Councilmember
 Adam Cowie, City Administrator
 20 Cody Cullimore, Chief of Police
 Kathryn Moosman, City Recorder

- 22
1. **Call to Order/Roll Call** – The meeting was called to order at 7:00 p.m.
 - 24
 2. **Presentations/Announcements** –
 26 a) **Mayor/Council Comments** – There were no announcements at this time.
 - 28 3. **Approval of Minutes** – The minutes of the regular meeting of the City Council
 meeting of January 3, 2017 were reviewed.

30 COUNCILMEMBER LUNDBERG MOVED TO APPROVE THE MINUTES
 32 OF THE REGULAR CITY COUNCIL MEETING OF JANUARY 3, 2017 AS
 AMENDED OR CORRECTED. COUNCILMEMBER BRODERICK SECONDED
 34 THE MOTION. THE VOTE WAS RECORDED AS FOLLOWS:

COUNCILMEMBER BEAN AYE
 36 COUNCILMEMBER LUNDBERG AYE
 COUNCILMEMBER BRODERICK AYE
 38 COUNCILMEMBER HOYT AYE
 COUNCILMEMBER SWEETEN AYE

40 THE MOTION CARRIED UNANIMOUSLY.

- 42 4. **Consent Agenda** – No items.
- 44 5. **Open Session for Public Comment** – Mayor Acerson called for any public
 comment not listed as an agenda item. There were no public comments.

46 **CURRENT BUSINESS**

2 **6. Public Hearing — Resolution #2017-4-R, FY 2017 Budget & Fee Schedule**
4 **Amendment.** Lindon City Staff will present Resolution #2017-4-R outlining
 proposed FY2017 budget and fee schedule amendments.

6 COUNCILMEMBER BRODERICK MOVED TO OPEN THE PUBLIC
8 HEARING. COUNCILMEMBER SWEETEN SECONDED THE MOTION. ALL
 PRESENT VOTED IN FAVOR. THE MOTION CARRIED.

10 Adam Cowie, City Administrator, began this discussion by explaining the City
12 adopts an annual budget in June of each year with the expectation that as the fiscal year
14 proceeds there may need to be some additions or amendments to the budget. He noted a
 budget amendment in October, January, April, and June is typical to account for these
 unexpected additions or changes to the budget. He then referenced the Resolution and
 information provided from the Finance Director and the Parks & Recreation Director.

16 Mr. Cowie noted there may be some additional outstanding items and another
18 budget amendment will be done in April when adopting the tentative budget. He then
 turned the time over to Kristen Colson, City Finance Director and Heath Bateman, Parks
 & Recreation Director for their presentation and discussion.

20 Ms. Colson began by going through each budget amendment item followed by
22 discussion:

- 24 1) Lindon City agreed to trade surplus property to Nicolson Construction in
26 exchange for them to build a drying bed for the Public Works building. Lindon’s
 surplus property is valued at \$121,253, which needs to be recorded in full as
28 revenue in the General Fund and then transferred to the Storm Water fund to
 offset the drying bed expense of \$161,176 (trade of the land for work done).
- 30 2) The \$4,000 budget for a management intern is not needed. However, the budget
 for a planning intern needs to be increased by \$1,000.
- 32 3) The Professional and Technical Services budget for Public Works Administration
 needs to be increase \$1,000 to pay for an additional GIS license for Brad
34 Jorgensen.
- 36 4) The City Council had previously advised Staff to transfer excess fund balance to
 the Road Fund. At this time, \$300,000 is being transferred and added to the
38 2017FY budget for road improvements. They are increasing the budget for this
 year for road projects from \$500,000 to \$800,000.
- 40 5) Lindon City will install lights at the Aquatics Center in order to extend the rental
 period at night (they have received bids). The cost will be about \$115,000. The
 Redevelopment Agency (RDA) State Street District will contribute \$75,000.
42 towards the project and the remaining \$40,000 will come from PARC Tax funds.
- 44 6) The Provo River Water Users Association is refinancing a balloon payment on
 one of their bonds. Lindon City opted to pay off the City’s allocation of this debt
 and save interest. Lindon’s portion is \$23,302, but the budget only needs to be
46 increased \$20,000.
- 7) The work on the 835 East pumps was budgeted for \$40,000. However, the project
 has grown and the estimated cost is now \$140,000 because of new pumps being

2 replaced and updating the electrical. The budget needs to be increased \$100,000
4 for this project. In addition, the City Council recently approved to budget \$90,000
in order to repair a portion of the North Union Canal.

6 8) The changes in revenues and expenses are balanced and offset by changes in the
use of, or appropriation to, fund balances.

8 Ms. Colson noted other upcoming items that may impact the budget as follows:

- 10 • Utilities and easements for the Ivory Development are still being determined.
- 12 • Engineers are evaluating the cost and benefit of expanding Well #3 to increase
redundancy in the City’s culinary water system.
- 14 • Engineers are designing the necessary enhancements to the well houses in order to
accommodate adding a chlorination system in each well house.

16 Mr. Bateman then went through the Fee Schedule amendments noting he is
requesting a few changes to the Fee Schedule to go into effect immediately to streamline
the pool. First they would like to remove the resident vs. non-resident designation on
open plunge punch passes and have one universal punch pass for all patrons (excluding
the Flowrider), in the amount of 25 punches and 50 punches as well as a punch pass for
exercise/walking/aerobics only. He noted that neighboring city pools (Payson, Spanish
Fork and Orem) are doing similar fees. He noted they would also like to set the pool
punch pass price and add early season pricing to the passes to encourage pass purchases
before the pool season begins.

24 Mr. Bateman then referenced the recommended punch pass fees (included in the
staff report). He pointed out that 35% of use is from residents and the remainder non-
residents. He noted that staff feels this is a good move and will keep things simple and
organized and will encourage volume. The Council suggested implementing a marketing
blast that would help get the word out early for punch pass sales to promote the pool.
They would also suggest gathering email information from patrons in order to send out an
email blast for informational and promotional purposes. Mr. Bateman noted if these
changes are not working they will stay dynamic and adjust it accordingly.

32 Mayor Acerson called for any public comments at this time. Hearing none he
called for a motion to close the public hearing.

34
36 COUNCILMEMBER HOYT MOVED TO CLOSE THE PUBLIC HEARING.
COUNCILMEMBER SWEETEN SECONDED THE MOTION. ALL PRESENT
VOTED IN FAVOR. THE MOTION CARRIED.

38
40 Mayor Acerson called for any further comments or discussion from the Council.
Hearing none he called for a motion.

42 COUNCILMEMBER BRODERICK MOVED TO APPROVE RESOLUTION
#2017-4-R AMENDING THE FY2017 BUDGET AND FEE SCHEDULE.

44 COUNCILMEMBER HOYT SECONDED THE MOTION. THE VOTE WAS
RECORDED AS FOLLOWS:

- 46 COUNCILMEMBER BEAN AYE
- COUNCILMEMBER LUNDBERG AYE

2 COUNCILMEMBER BRODERICK AYE
COUNCILMEMBER HOYT AYE
4 COUNCILMEMBER SWEETEN AYE
THE MOTION CARRIED UNANIMOUSLY.

6

7. **Recess to Lindon City Redevelopment Agency Meeting (RDA).** The Council called the Lindon City Redevelopment Agency to order, then reviewed/approved the RDA minutes.

10

COUNCILMEMBER SWEETEN MOVED TO CONVENE THE MEETING OF THE LINDON CITY REDEVELOPMENT AGENCY AT 7:26 P.M. COUNCILMEMBER BRODERICK SECONDED THE MOTION. ALL PRESENT VOTED IN FAVOR. THE MOTION CARRIED.

16

BOARDMEMBER SWEETEN MOVED TO ADJOURN THE MEETING OF THE LINDON CITY RDA MEETING AND RE-CONVENE THE MEETING OF THE LINDON CITY COUNCIL MEETING AT 8:36 P.M. BOARDMEMBER BEAN SECONDED THE MOTION. ALL PRESENT VOTED IN FAVOR. THE MOTION CARRIED.

22

8. **Review & Action — Aquatics Center Lighting Bid Award.** The Council will review and consider bids received for lighting of the Aquatics Center enabling later hours of pool use and increased rental times. Staff recommends approval of the low bid to Alliant Electric for \$112,000.

26

Mr. Bateman explained they recently solicited bids for the Aquatics Center Lighting upgrade project that was previously discussed by the Council. He noted the low bidder is Alliant Electric with a bid of \$112,000 (\$107,000 plus \$5,000 for restoration). He pointed out that Alliant Electric has previously done some big projects and are well recommended. Mr. Bateman then reviewed the project and estimated pay-back period from the increased revenues from implementing the lighting project at the pool that will include boring, trenching and concrete cutting. He stated the increased hours that the pool will be open with the new lighting will facilitate higher volume and will in turn increase revenues. Mr. Bateman concluded by stating staff feels this is the best use of funds and resources and it is their recommendation to award the bid to Alliant Electric. Following some general discussion the Council was in agreement this action will keep the pool relevant and competitive.

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Mayor Acerson called for any further comments or discussion from the Council. Hearing none he called for a motion.

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COUNCILMEMBER LUNDBERG MOVED TO AWARD THE AQUATICS CENTER LIGHTING PROJECT TO THE LOW BIDDER, ALLIANT ELECTRIC, WITH THEIR BID OF \$112,000. COUNCILMEMBER BRODERICK SECONDED THE MOTION. THE VOTE WAS RECORDED AS FOLLOWS:

46 COUNCILMEMBER BEAN AYE
COUNCILMEMBER LUNDBERG AYE

2 COUNCILMEMBER BRODERICK AYE
 COUNCILMEMBER HOYT AYE
 4 COUNCILMEMBER SWEETEN AYE
 THE MOTION CARRIED UNANIMOUSLY.

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9. **Major Subdivision — Anderson Farms Subdivision, Plat B, 330 North 1500 West.** Ken Watson, on behalf of Ivory Development, LLC, seeks preliminary plan approval of a forty-eight (48) lot (and one parcel “A”) subdivision, including dedication of public streets, at approximately 330 North 1500 West, in the Anderson Farms Planned Development (AFPD) zone.

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14 Mr. Van Wagenen led this discussion by giving some background of this item explaining this is the second phase of the Anderson Farms Planned Development which was approved by the Development Agreement between Lindon City and Ivory Development in June of 2016. He noted that Plat B consists of 48 lots in the plat and is considered Parcel B of the Anderson Farms concept plan. He explained that the development of Anderson Farms is governed by the Anderson Farms Master Development Agreement and all standards referred to tonight is a part of that Agreement. He noted Ken Watson is in attendance representing Ivory Development for this agenda item.

22

Mr. Van Wagenen then referenced the lot requirements as follows:

24

- The average lot size of Plat B is 7,426 s.f. with the largest lot being 11,509 s.f. and the smallest being 6,127 s.f. These lots are consistent with the concept plan that was presented in the Master Development Agreement.
- Parcel A is a storm water detention basin and is a non-buildable parcel (close to 3 acres).
- New roads will be built to serve the subdivision including a portion of Anderson Blvd. which will tie in to the existing 500 North and the associated round about. Curb, gutter and sidewalk will be installed along the new local streets in addition to six foot planter strips. There are several different cross sections for this phase.
- The round-about design is being finalized. The location may need to shift slightly and the diameter may be enlarged or shifted a little. If changes to the design affect the lot layout significantly, a different intersection may be considered. A recommended condition of approval is that a final design for the round-about/intersection be approved by the City Engineer. Anything major will be brought back before the Council.

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40 Mr. Van Wagenen then referenced the utility requirements as follows:

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- This second plat will require a combination sewer, ground water, and pressure irrigation system pump station with associated off-site lines to be built (they were also required for Plat A). Once built, this infrastructure will serve the remainder of the development. These systems will need to be operational before any certificates of occupancy are approved. Needs to be included as a condition of approval.
- No park improvements are required at this time.

44

46

- A six foot masonry wall will run along Anderson Boulevard and Lancaster Way on the backside of the home lots. The Council may want to consider not having the wall adjacent to Parcel A on Anderson Boulevard, the storm water detention area so it can act as accessible open space to the neighborhood.

Mr. Van Wagenen then presented an aerial photo of the proposed subdivision, concept plan, preliminary Anderson Farms Plat B, street cross sections, and off-site utility maps followed by some general discussion. He noted this is a pretty straightforward major subdivision up to this point. He then turned the time over to Mr. Watson for comment.

Mr. Watson stated they just learned from the City Engineer after reviewing the diameter of the roundabout that it will need to be enlarged somewhat but that is not a huge issue. He noted they are underway with Plat A with the utilities in the ground. They are just waiting for some good weather to do the paving and concrete work, but it will happen in the early spring. The pump station is designed and well underway noting it was quite a process to get designed and bid out; they awarded the low bidder. The offsite utilities are about 80% in. They own a 1/3 of the park now and will soon own another 1/3 of the park and once they buy property further to the north they will own all of the park and will then build the park and the detention pond (detention pond will be first).

Councilmember Lundberg mentioned there are a number of grassed in retention areas in the city that are used recreationally. She asked Mr. Watson if there is access between lots so pedestrians can walk through this area to give some access and facilitate pedestrian movement. Mr. Watson replied that is a possibility but would be something they would have to work on to see what they can do; perhaps through an easement and a walkway/path (between lots #212 and #213). The Council agreed that an easement may be an appropriate solution. There was then some general discussion regarding building the phases (Plat A and Plat B), the park, cross sections, paving and the pump station.

Mr. Watson stated they are aware of the conditions of approval and all conditions will be completed and dealt with as required and they have no issues or concerns with completing these items. Mr. Watson also thanked all involved who helped get the easement through the Schaeffer property noting the agreements are signed and recorded.

Mr. Van Wagenen recapped stating there are the four existing conditions to include in the motion and also the condition to not have the wall along the detention pond as to provide an easement from the interior to the park.

Mayor Acerson called for any further comments or discussion from the Council. Hearing none he called for a motion.

COUNCILMEMBER BEAN MOVED TO APPROVE THE APPLICANT'S REQUEST FOR APPROVAL OF A 48 LOT RESIDENTIAL SUBDIVISION TO BE KNOWN AS ANDERSON FARMS PLAT B WITH THE FOLLOWING CONDITIONS: 1. ADDRESS THE STORM DRAINAGE CONCERNS OF THE CITY ENGINEER. 2. DEVELOPER BE RESPONSIBLE TO PUMP GROUNDWATER COLLECTED BY THE LAND DRAIN SYSTEM UNTIL THE GROUNDWATER PUMP STATION IS OPERATIONAL. 3. OFF-SITE SEWER, GROUND WATER, AND PRESSURE IRRIGATION SYSTEM PUMP STATION AND OFF-SITE SEWER FORCE MAIN AND PRESSURE IRRIGATION LINE SHALL BE CONSTRUCTED

2 AND FUNCTIONAL BEFORE ANY HOMES CAN BE OCCUPIED OR CULINARY
 4 WATER SERVICE IS PROVIDED TO ANY HOMES. 4. THE ROUND-ABOUT BE
 4 DESIGNED TO ENSURE SAFETY TO THE SATISFACTION OF THE CITY
 6 ENGINEER OR SOME OTHER SATISFACTORY INTERSECTION BE APPROVED
 6 AND 5. A PATH AND EASEMENT BECOME PART OF THIS PLAT BETWEEN
 8 LOTS 212 AND 213 AS DISCUSSED AND TO BE DESIGNED WITH THE
 8 APPROVAL OF CITY STAFF WITH NO REQUIRED FENCE TO DATE.
 COUNCILMEMBER LUNDBERG SECONDED THE MOTION. THE VOTE WAS
 10 RECORDED AS FOLLOWS:

10	COUNCILMEMBER BEAN	AYE
12	COUNCILMEMBER LUNDBERG	AYE
	COUNCILMEMBER BRODERICK	AYE
14	COUNCILMEMBER HOYT	AYE
	COUNCILMEMBER SWEETEN	AYE

16 THE MOTION CARRIED UNANIMOUSLY.

18 **10. Concept Review – Legacy Plaza, 730 N. State Street.** Roy Morgan, Roy’s Auto
 20 Services, requests feedback on proposed improvements and zoning for automotive
 22 repair at 730 N. State Street. The property is currently in the General Commercial
 (CG) zone. General Auto/Vehicle Repair is not a permitted use in the CG zone.
 As a concept review, feedback is requested but no formal action will be taken.

24 Mr. Van Wagenen led this discussion by stating the applicant, Mr. Roy Morgan,
 is in attendance and is interested in utilizing the site located at 730 N. State Street for
 26 General Auto/Vehicle Repair (South of Arctic Circle). He noted Mr. Morgan specializes
 in Volvo maintenance and repair. He added that Mr. Morgan has submitted a concept
 28 review layout which includes the demolition and removal of the existing structures and
 redevelopment of the property.

30 Mr. Van Wagenen stated the Planning Commission reviewed this proposed
 concept on January 10, 2017. He noted the Commission generally felt it may be
 32 appropriate to develop conditions similar to those identified for vehicle sales lots (as
 found in Lindon City Code 17.48.200). The Planning Commission also indicated to Mr.
 34 Morgan that further discussion and approval is needed in order to take that course of
 action (application and fees would also be required).

36 Mr. Van Wagenen noted the Planning Commission also voiced the following concerns:

- 38 1) Is it appropriate to amend the land use table to allow general auto/vehicle repair as
 a permitted use or conditional use in all areas currently in the General
 Commercial (CG) zone?
- 40 2) It is not appropriate to consider rezoning only this parcel to allow the proposed
 concept.
- 42 3) Concerns with the use in general include:
 - 44 a. Outdoor storage of parts and materials
 - b. Outdoor storage of damaged, dismantled or inoperable vehicles
 - 46 c. Shift the focus off of the use and more to (favorable) site improvements and
 amenities (site must bring buildings up to meet building materials and other
 design standards)

- 2 4) Concerns with the site:
 4 a. Current conditions of the building and site
 b. Lack of landscaping (street frontage)
 6 c. Lack of parking (in front of the building)

8 Mr. Van Wagenen pointed out the goal of commercial development is to
 encourage the establishment and development of basic retail and commercial stores
 10 which will satisfy the ordinary and special shopping needs of Lindon citizens, enhance
 the City's sales and property tax revenues, and provide the highest quality goods and
 services for area residents. Objectives of this goal are to facilitate the following:

- 12 1. Expand the range of retail and commercial goods and services available within the
 community.
 14 2. Promote new office, retail, and commercial development along State Street and
 700 North.
 16 3. Encourage development of commercial facilities, such as hotels, restaurants and
 vehicle related services at transportation interchanges.

18 Mr. Van Wagenen went on to say the commercial ordinance is established to
 20 promote commercial and service uses for general community shopping. The objective in
 establishing commercial zones is to provide areas within the City where commercial and
 22 service uses may be located. These zones include the General Commercial Zones (CG,
 CG-A, CG-A8, CG-S) Lindon Village Commercial Zone and the Planned Commercial
 24 (PC-1 and PC-2) Zones.

26 Mr. Van Wagenen noted that the applicant is essentially interested in demolishing
 the buildings on site and re-developing the property to run a general auto repair business
 that specializes in Volvo's which is their customer base and their niche. He then
 28 referenced the concept plan layout. Mr. Van Wagenen then turned the time over to the
 applicant for comment.

30 Mr. Morgan stated this is will be a non-traditional automotive shop (office
 warehouse look) and they will run a nice, clean business. There will be a front and rear
 32 entrance so a future use for office warehouse is a possibility. He noted the real question
 is if this proposal is a possibility to pursue according to this property use. Mr. Morgan
 34 stated, if approved, he will re-locate his current shop from his Orem location to Lindon.
 Mr. Morgan noted he plans on buying the property with some space being sublet.

36 Mr. Van Wagenen pointed out the question tonight is if the Council has any
 interest in allowing vehicle repair along State Street and at what level (the existing auto
 38 repair businesses are grandfathered in). The other question is if the Council will allow
 vehicle repair in the existing buildings or allow it to be re-developed. Mr. Van Wagenen
 40 mentioned the majority of the Commissioners were more comfortable with it being
 contingent upon re-development of the property. There was then some general discussion
 42 including concerns with no outside storage of cars, customer parking, landscaping, tenant
 rental use, etc. Mr. Van Wagenen pointed out that this could be allowed with a new
 44 "zoning column" that would essentially allow general vehicle repair along State Street
 and apply the zone (where appropriate) which is more of a "spot zoning" and would be at
 46 the Council's discretion.

2 Councilmember Hoyt pointed out that re-development is so important and when
 4 we have an opportunity like this where there is a possible revenue generator he would be
 6 open to it and to try to find a way to make it work. Councilmember Lundberg agreed
 8 adding the owner of the property has been trying to sell this property for several years
 10 and he is having a hard time because the existing structure needs a lot of work and was
 12 “hodge podged” together in the first place. Councilmember Hoyt added if the right codes
 14 are put into place this would be an improvement to the site. Mr. Van Wagenen pointed
 16 out this is just a concept review but a lot of the layout presented looks correct and appears
 18 they could make it meet the design standards. Mayor Acerson asked the Councilmembers
 20 how they generally feel about this concept. Councilmember Lundberg commented she is
 22 open to this concept but would like to look into the parameters to do it the right way in
 24 order to avoid the reasons it was removed by ordinance in the first place. She added she
 26 has concerns of what is allowed because the State Street corridor is very limited in size
 28 and they want a good diversity.

16 Councilmember Sweeten stated he is more open to this concept following the
 18 discussion but only if we can protect certain areas and it doesn’t open it all up.
 20 Councilmember Bean commented he would also be in favor if the parking and storage
 22 was facilitated in the back. Mr. Van Wagenen stated there are currently standards in place
 24 that they would have to be in compliance and any other conditions can be added. Mayor
 26 Acerson observed, generally speaking, this concept plan is a move in a better direction
 28 but the concern overall is the multiple examples on State Street as to why general vehicle
 repair was removed in the first place. Councilmember Broderick agreed with these
 statements adding he would be open to this concept with re-development and having the
 parking where it is not visible. Mr. Morgan stated they understand these concerns and
 will put forth a good image and a good clean business. Mr. Van Wagenen stated the next
 step is a code revision on the standard land use table and any ordinance change needed
 per direction of the Council.

Following some additional discussion Mayor Acerson called for any further
 comments or discussion from the Council. Hearing none he moved on to the next agenda
 item.

**11. Review & Action — Resolution #2017-5-R; Grant Funding for North Union
 Canal Piping.** The Council will review and consider Resolution #2017-5-R
 supporting submittal of the WaterSMART: Water and Energy Efficiency Grant
 and committing \$300,000 in grant match funding for piping of significant sections
 of the North Union Canal in order to maintain this system improvement and
 reduce water loss.

Mr. Cowie stated the City has been working with the North Union Irrigation
 Company to maintain the North Union Canal because a significant water loss from the
 canal has been documented. He explained the canal is a critical water delivery
 infrastructure for the Lindon Secondary Water System. He noted the City also owns
 approximately 60%+ shares of the North Union Irrigation Company water stock. He
 further explained that maintaining and preserving the North Union Canal and the water it
 delivers is critical to the ability of Lindon to deliver secondary water to its residents and

2 businesses. He pointed out with the documented water losses in the canal, the City has
 4 been working to apply for grant opportunities to pipe the canal.

6 Mr. Cowie went on to say the WaterSMART grant through the Bureau of
 8 Reclamation (BOR) is the best opportunity so far to obtain the funding levels needed to
 10 pipe large sections of the canal. In order to best compete for the funding, a significant
 12 match is scored higher by the BOR. Mr. Cowie stated staff recommends applying for
 14 \$600,000 in funding with a commitment by Lindon City to match 50% (\$300,000) of the
 16 grant. The North Union Irrigation Company has committed to contribute \$15,000 to the
 18 project. If awarded, the funds would be needed in FY2018, with the project most likely
 20 being completed in the spring of 2018 with no change in the current fiscal year budget
 22 being required. He then referenced for discussion the DRAFT grant application and
 24 letters of support received to date. Mr. Cowie added the City Attorney has drafted the
 26 documents. Mr. Cowie stated this temporary fix would get us through several years at
 least and will dramatically increase the water retention.

18 Mayor Acerson commented that any funding mechanism that can be identified
 20 long term is the right thing to do and this would be significant. There was then some
 22 discussion of the scoring system on the grants and scoring and breakdowns looking at
 24 further funding options. Mr. Cowie stated they are trying to maximize the score. Mayor
 Acerson stated it would be prudent to look at other funding options to see what is
 available. Mr. Cowie stated he will evaluate additional funding options (up to \$500,000).
 Following discussion the Council was in agreement to match the funds towards the
 piping of the canal. Mayor Acerson expressed his appreciation to staff for taking the steps
 to facilitate the grant.

26 Mayor Acerson called for any further comments or discussion from the Council.
 Hearing none he called for a motion.

28 COUNCILMEMBER SWEETEN MOVED TO APPROVE RESOLUTION
 30 2017-5-R SUPPORTING THE WATERSMART GRANT APPLICATION AND
 COMMITTING \$300,000 MATCHING FUNDS TOWARDS PIPING OF THE NORTH
 UNION CANAL IF THE GRANT IS AWARDED. COUNCILMEMBER BRODERICK
 32 SECONDED THE MOTION. THE VOTE WAS RECORDED AS FOLLOWS:

34 COUNCILMEMBER BEAN	AYE
36 COUNCILMEMBER LUNDBERG	AYE
COUNCILMEMBER BRODERICK	AYE
COUNCILMEMBER HOYT	AYE
COUNCILMEMBER SWEETEN	AYE

38 THE MOTION CARRIED UNANIMOUSLY.

40 **12. Discussion Item — Road Maintenance Funding.** The Council will discuss
 42 current road maintenance funding sources and review options and considerations
 44 for additional sources of road maintenance funding, including a potential
 Transportation Utility Fee as has been created in Provo, Vineyard, and Pleasant
 View and contemplated by many other cities. No motion will be made on this
 46 discussion item.

2 Mr. Cowie stated the City has been discussing a possible Transportation Utility
4 Fee option for growing road maintenance funds since 2014-15. He noted staff is now
6 looking for firm direction from the Council if they choose to proceed with this fee option.
8 Mr. Cowie stated a straw poll is needed to know if the majority of the Council desires to
implement this type of fee in order to support road funding, what the approximate fee
levels may be and, if proceeding, how the Council wants to include more public input
prior to any adoption of fees (open houses, digital notification, mailers, etc.).

10 Mr. Cowie reminded the Council as to why increased road funding is needed and
12 at what funding levels are recommended. He then referenced the 2015 Pavement
14 Management Study that was adopted by the City Council. He also referenced Provo
16 City's video and ordinance explaining and supporting their Utility Transportation Fund.
18 Mr. Cowie stated it is anticipated if the Lindon City Council decides to proceed with a
transportation fee, the City would adopt something similar to Provo's model which has
been in effect since March 1, 2014. He noted the Town of Vineyard has also adopted a
more unique/simplified Transportation Utility fee calculation of \$3.50 per ERU
(Equivalent Residential unit). Mr. Cowie also presented the spreadsheet showing possible
fee calculations (residential and commercial) followed by discussion.

20 Mr. Cowie stated it is anticipated that adoption of a transportation utility fee
22 would not solve all of the funding needs of the City, but it would be a supplementary
24 funding source in addition to the Class C road funds from the State and other General
Fund contributions. He noted that ultimately, we have no long term road funding
maintenance solutions. There was then some general discussion regarding road funding
options and utility fees and the gas tax.

26 Councilmember Hoyt stated we have come a long way with road funding in the
28 past 3 years. He stated the 5 year utility rate increase we are committed to is significant
and because of that he is hesitant and is on the fence. He would like to see a comparison
of where we are at on utilities in comparison to neighboring cities. Councilmember
Lundberg commented any time a new fee is created she feels we should be transparent
30 about it and "sell" it to the residents and use same marketing that was used with the Parc
Tax to educate and inform the citizens that we are in a dire situation with roads and to
gather their support instead of legislating it.

32 Mr. Cowie stated the direction he is asking from the Council tonight is if they
34 want to go the next step of engaging the City Engineer to gather some hard numbers and
figures into something that is presentable to the public through open houses etc. as to get
36 some dialogue going. He re-iterated that the Council is limited as to what they can do for
road funding with either a utility fee, property tax increase or to supplement it on good
38 years with sales tax revenue. Councilmember Sweeten stated he will never support a
property tax increase. He also stated he feels there may not be a lot of push back from
40 residents on a utility fee but that will not be the case with business owners as it will be
higher. Mayor Acerson suggested the Councilmembers take the opportunity to talk to
42 their neighbors and prepare their thoughts on the pros and cons of a proposed utility fee.

44 Councilmember Bean commented that his position hasn't changed and feels this
46 should be looked at seriously to address the problem. He pointed out that the Council
agrees that road maintenance needs to be more important as was the sewer and water
issues several years ago that were addressed as costs out pace what we are collecting so
rates were raised over a period of time. He stated that road issues are much worse and

2 should be looked at as were the water and sewer issue. He pointed out we have examples
 4 from other cities who have stepped forward and we need to look at this as we don't have
 a sustainable model. Councilmember Lundberg agreed with Councilmember Bean's
 6 statements but pointed out this would be creating a new fee when sewer and water were
 existing fees that were bumped. However, she would be ready to get a public relations
 8 campaign going and selling it to the citizens as to get a "ground swell" of support for it;
 she feels kicking the can down the road is a far worse option. Mayor Acerson pointed out
 there is really a double focus to consider 1. Address the legislature that we are serious
 10 about this and 2. If they are not willing to step up then we as a Council need to step up to
 address this issue. Councilmember Sweeten stated he would support a fee more than a
 12 property tax increase; he really doesn't approve but if it can be justified he would be
 more in favor. Councilmember Broderick commented that he likes the plan on getting to
 14 where we need to be as we did with the water fund in working with the City Engineer etc.
 However, he would like to see the full comprehensive plan as he does have some
 16 reservations (i.e. 700 North). He wants to show that we as a city have done everything we
 can before implementing a fee.

18 At this time Mr. Cowie took a straw poll vote asking the Council if they want him
 to go to the next step and engage the City Engineers to gather some hard numbers and good
 20 estimates/data on several options to review before presenting to the public. The straw poll
 vote was recorded as follows:

22

Councilmember Broderick: Yes.

24

Councilmember Lundberg: Yes.

Councilmember Sweeten: Yes.

26

Councilmember Hoyt: Yes.

Councilmember Bean: Yes.

28

Following the straw poll vote the Council was unanimous to direct Mr. Cowie to
 30 engage the City Engineer to gather some good accurate figures with alternative costs to
 bring back to the Council for review.

32 Mayor Acerson called for any further comments or discussion from the Council.
 Hearing none he moved on to the next agenda item

34

*Mayor Acerson excused himself at 9:30 pm. Councilmember Sweeten stepped in
 36 as Mayor Pro Tem at this time. Mayor Acerson returned to the meeting at 9:40 pm.*

38 **COUNCIL REPORTS:**

40 **Chief Cullimore** – Chief Cullimore had nothing to report.

42 **Councilmember Hoyt** – Councilmember Hoyt reported that he attended the recent
 ribbon cutting for Global Marketing which was great and brought a lot of people
 44 together. He noted he is excited to have their business in Lindon and feels they will be a
 great addition to the city. He also reported the PG/Lindon Chamber luncheon was well
 46 attended and he is so proud of Lindon and noted the Mayor showcased our city well. He
 stated the luncheons are typically held the second Wednesday of every month and the

2 Council is always invited (he noted he will add them to the email list). He feels this is a
4 great opportunity to meet business owners.

6 **Councilmember Broderick** – Councilmember Broderick reported the Mayor did a nice
8 job on the State of the City report at the Chamber of Commerce meeting. He also
10 reported he plans to attend the North Union Irrigation Board meeting this Saturday.

12 **Councilmember Bean** – Councilmember Bean expressed his appreciation for the good
14 discussion by the Council tonight and he hopes to do some good things in 2017.

16 **Councilmember Lundberg** – Councilmember Lundberg reported she will be attending
18 the Tree Board meeting later this month. She also attended the Global Marketing ribbon
20 cutting noting there was a big turnout. She also mentioned she had discussion with Mr.
22 Van Wagenen about the 700 North corridor guidelines presented by the committee and
24 she would like the Council to consider those elements in the guidelines/standards to
26 improve on and perhaps adopt in the commercial zone. She noted the committee will be
28 bringing something back before the council in February. She also brought up an issue
with the Tams and Zyto buildings on 700 North. She stated that apparently the Tams
building had some additional features they were supposed to add per the site plan, and
they got occupancy, but staff determined to take them at their word and they have not
completed the items and are stonewalling. She asked the Council how they feel about
holding the contractor accountable before obtaining the certificate so they finish the items
before occupancy. Mr. Cowie stated he is hesitant to do that because it may affect other
properties/owners and he suggested a better approach and direction would be to seek a
fine instead or send a warning letter from the City Attorney. Mr. Cowie stated he will talk
to the Planning Director about this issue and get back with the Council.

30 **Councilmember Sweeten** – Councilmember Sweeten reported that the Chamber has
32 already brought in a lot of businesses to the Chamber of Commerce and Josh Walker is
doing a great job.

34 **Mayor Acerson** – Mayor Acerson mentioned the Provo State of the City report noting he
36 will send the information out to the councilmembers. He also asked for an update on the
recent drive approach issue. Mr. Cowie stated the homeowner posted a bond and they
came to an agreement that worked for both parties.

38 **Administrator's Report:**
40 Mr. Cowie reported on the following items followed by discussion.

42 **Misc. Updates:**

- 42 • January newsletter:
- 44 • March newsletter article: Councilmember Bean - Article due to Kathy Moosman
last week in February.
- 46 • City Center Elevator remodel. Architect is preparing plans for bidding by early
February.
- Public Safety Building – Hoping for completion date of January 19th. Police

- 2 Department will move over on January 25-26th. Open house most likely during
week of February 13th.
- 4 • Budget Kick-off meeting – typically a working dinner at 6pm. February 9th
(Thursday)
- 6 • Water system update – well chlorination, 835 E. tank, etc.
- FYI:
- 8 ○ Planning Commission voted Sharon Call to continue as Chair of the Planning
Commission
- 10 ○ Tattoo shop - business license
- Misc. Items:

12 **Upcoming Meetings & Events:**

- 14 • January 16th – City offices closed.

16 Mayor Acerson called for any further comments or discussion from the Council.
Hearing none he called for a motion to adjourn.

18 **Adjourn** –

20 COUNCILMEMBER BRODERICK MOVED TO ADJOURN THE MEETING
22 AT 10:00 PM. COUNCILMEMBER SWEETEN SECONDED THE MOTION. ALL
PRESENT VOTED IN FAVOR. THE MOTION CARRIED.

24 Approved – February 7, 2017

26 _____
28 Kathryn Moosman, City Recorder

30 _____
32 Jeff Acerson, Mayor

Item 4 – Consent Agenda – *(Consent agenda items are only those which have been discussed beforehand and do not require further discussion)*

- No Items.

Item 5 – Open Session for Public Comment *(For items not on the agenda - 10 minutes)*

6. Concept Review — Center Street & 25 South Main.*(20 mins)*

The Council will hear and provide feedback to the applicant, Bryant Christensen, CL Christensen Brothers, regarding a proposed Senior Apartment and Townhome concept located at Center Street and 25 South Main St. The proposal would require a new ordinance. Portions of the property are in the General Commercial (CG) zone, Senior Housing Overlay (SHFO) zone, and the Single-family Residential (RI-20) zone. The current regulations of the SHFO zone can be found in Lindon City Code 17.75. The General Plan Land Use Map identifies this area as Commercial and Residential Low. The Planning Commission will review the concept at their first meeting in February.

No motion is necessary as this item is for discussion only.

See attached information from the Planning Department.

Concept Review — Center and Main ~ 25 S. Main St.

Applicant: Bryant Christensen, CL
Christensen Bros.

Presenting Staff: Brandon Snyder

Type of Decision: None

Council Action Required: No

SUMMARY OF KEY ITEMS

1. This is a concept review to receive feedback from the Planning Commission and City Council regarding the applicant's proposal.

MOTION

No motion necessary.

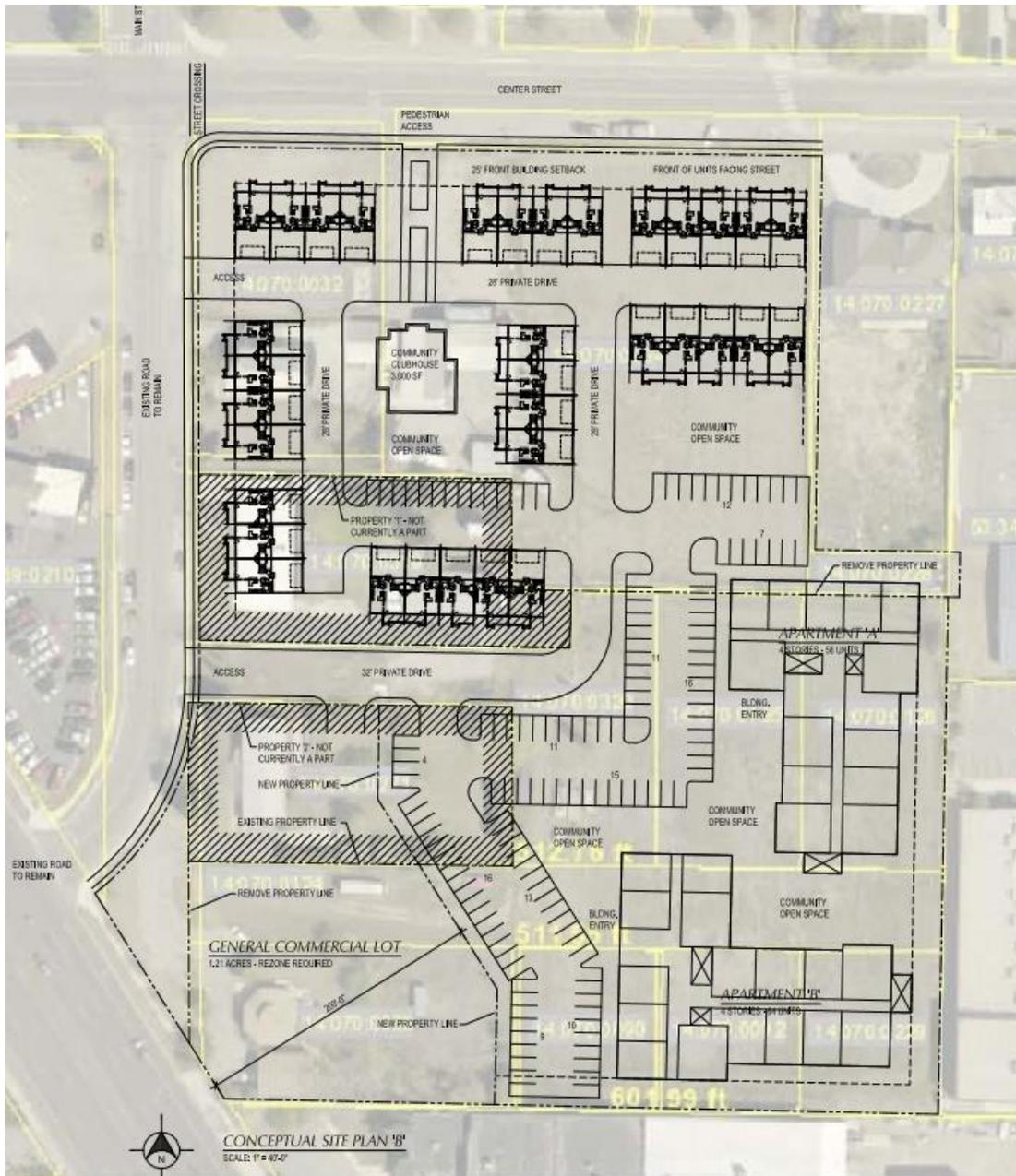
OVERVIEW

A Concept Review allows applicants to quickly receive Planning Commission and/or City Council feedback and comments on proposed projects. No formal approvals or motions are given, but general suggestions or recommendations are typically provided. Although not mandatory, a Concept Review is recommended for all large development projects.

The proposal is located at approximately 25 S. Main Street. The proposal would require a new ordinance. Portions of the property are in the General Commercial (CG) zone, Senior Housing Overlay (SHFO) zone, and the Single-family Residential (R1-20) zone. The current regulations of the SHFO zone can be found in Lindon City Code 17.75. The General Plan Land Use Map identifies this area as Commercial and Residential Low.



(Present zoning)



(Proposed concept) Please refer also to the attachment

The Lindon City General Plan indicates the following:

Community Vision

It is Lindon City's community vision to provide for a strong, positive civic image and identity within a clean and attractive physical setting which seeks to preserve a high quality, open, rural living atmosphere which is also receptive to compatible services and amenities provided by some elements of urban living.

The Objectives of this Community Vision are to:

1. Recognize and promote Lindon as a dynamic Utah County community with a distinctive rural environment consistent with its traditional, family-oriented values.
5. Maintain the quality of existing and future neighborhoods and land use areas within the City through preservation of animal rights, community beautification, improved parks & trails, and other pursuits relating to provident living, recognizing all segments of our community (age, economic status, etc.).
6. Channel future growth and development into areas that can be efficiently and effectively served by public infrastructure and facilities.
7. Ensure that new development is of high quality and reflects quality architectural and site design standards consistent with its particular use and location.

Residential Land Uses include a range of residential classifications including low, medium, and high density. Density is expressed in dwelling units per acre (DU/AC) for single or multiple family dwellings. Zoning regulations may allow in residential areas a limited number of non-residential uses, such as places of worship, neighborhood parks, schools, etc. The goal of housing and residential areas in Lindon City is to provide a housing and living environment that supports and complements the unique rural quality and character of Lindon City. Objectives of this goal are as follows:

1. Maintain and enhance the pleasing appearance and environmental quality of existing residential neighborhoods by avoiding encroachment of land uses which would adversely impact residential areas (i.e. increased traffic, noise, visual disharmony, etc.) and by providing adequate screening and buffering of any adjacent commercial or industrial development including parking and service areas.
2. Consider flexibility in housing development design and density in the R3 zone.
3. Encourage creative approaches to housing development which will maintain and protect natural resources and environmental features.
4. Ensure that new developments in residential areas (including non-residential uses) provide adequate off-street parking.
5. Provide for the unique community needs of the elderly, disabled, and children.

STAFF REVIEW

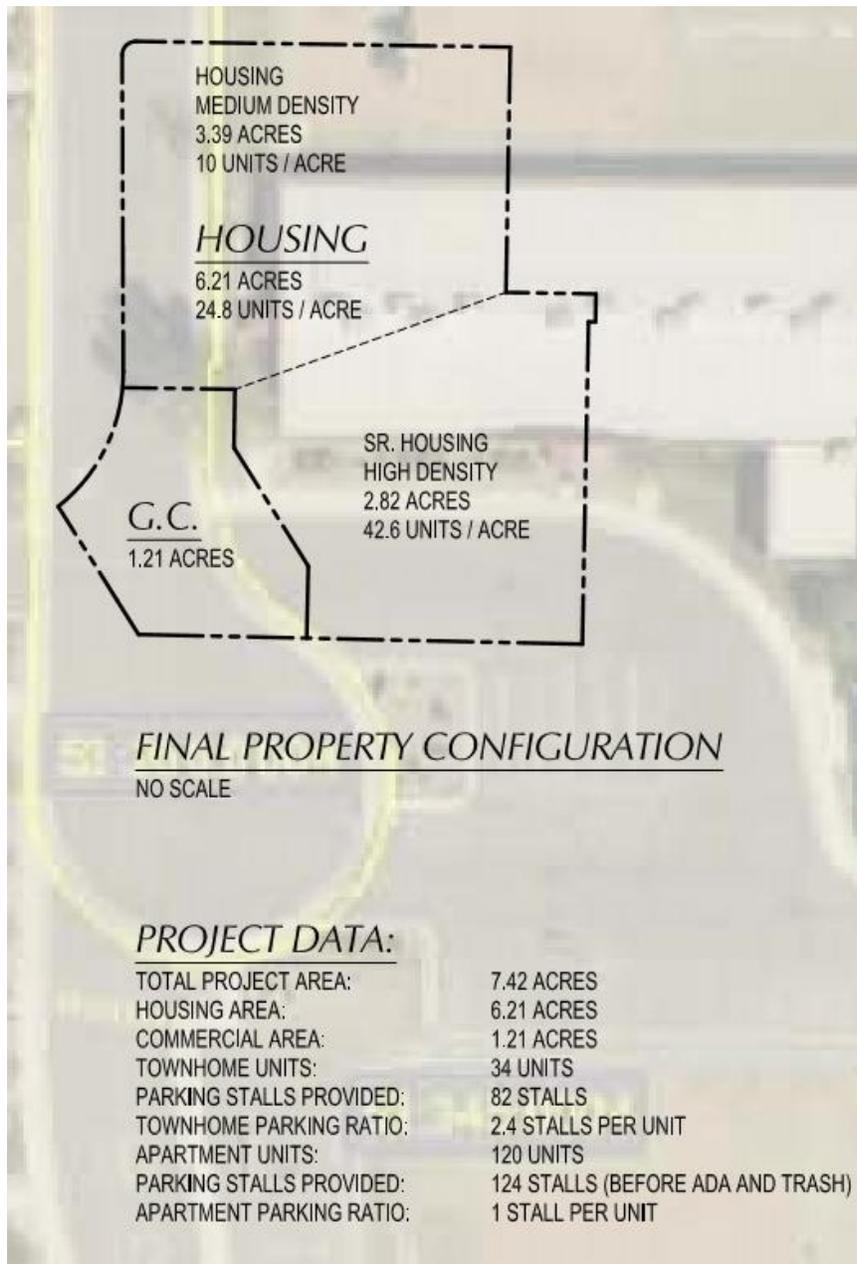
Potential benefits may include: commercial lot, compact design, efficient use of the site, central location, housing options, transition between residential and commercial properties, and front porch layout.

PLANNING COMMISSION REVIEW

The Planning Commission will review the concept at their first meeting in February.

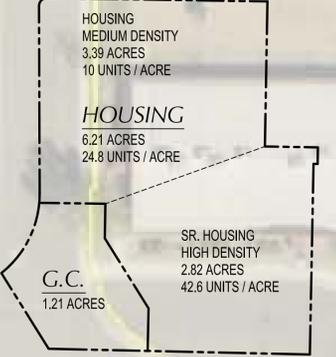
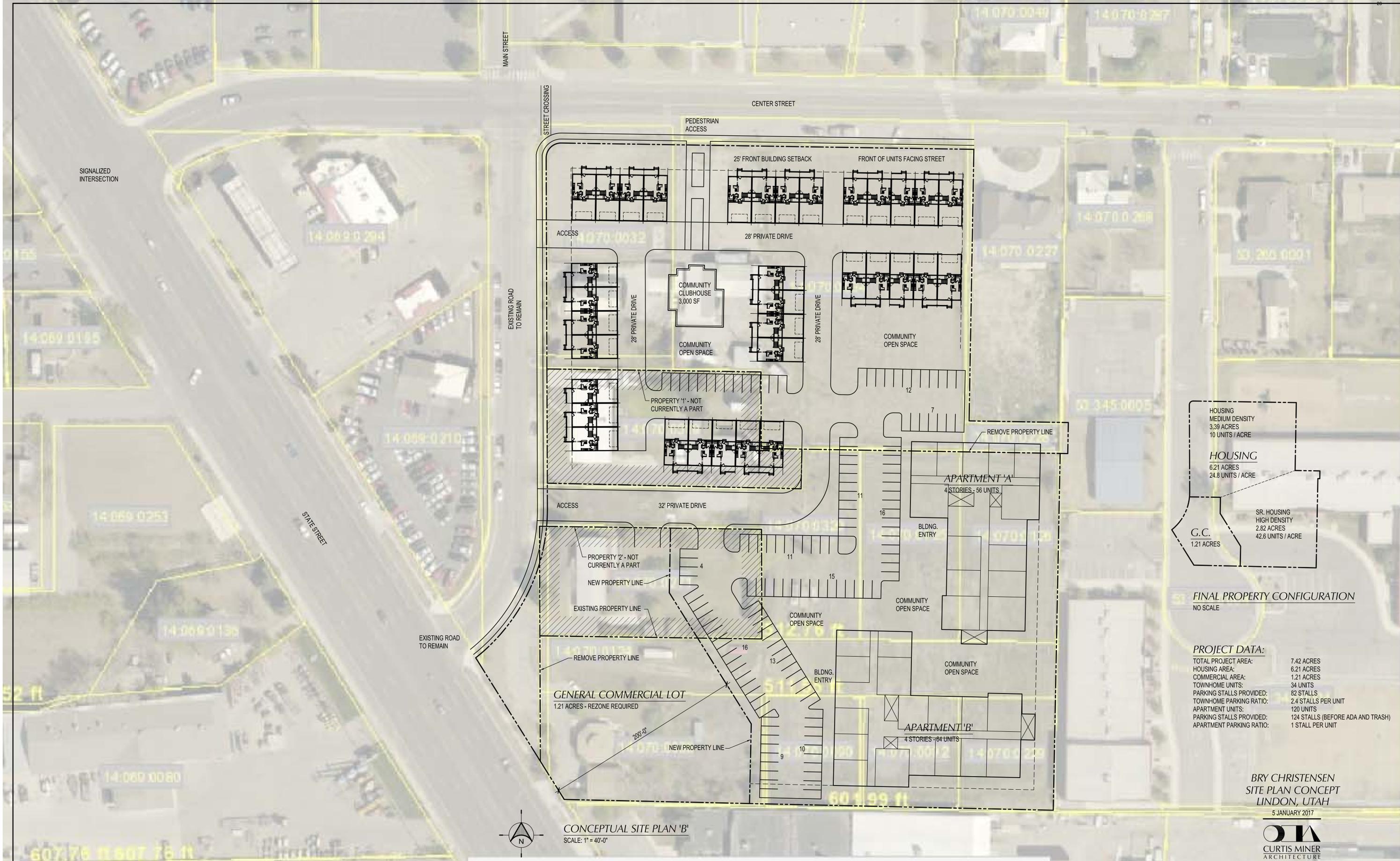
MOTION

No motion necessary.



ATTACHMENTS

1. Concept Plan



51 FINAL PROPERTY CONFIGURATION
NO SCALE

PROJECT DATA:

TOTAL PROJECT AREA:	7.42 ACRES
HOUSING AREA:	6.21 ACRES
COMMERCIAL AREA:	1.21 ACRES
TOWNHOME UNITS:	34 UNITS
PARKING STALLS PROVIDED:	82 STALLS
TOWNHOME PARKING RATIO:	2.4 STALLS PER UNIT
APARTMENT UNITS:	120 UNITS
PARKING STALLS PROVIDED:	124 STALLS (BEFORE ADA AND TRASH)
APARTMENT PARKING RATIO:	1 STALL PER UNIT

CONCEPTUAL SITE PLAN 'B'
SCALE: 1" = 40'-0"

BRY CHRISTENSEN
SITE PLAN CONCEPT
LINDON, UTAH
5 JANUARY 2017

OMA
CURTIS MINER
ARCHITECTURE
233 SOUTH PLEASANT GROVE BLVD.
SUITE #105
PLEASANT GROVE, UTAH 84062
PHONE: (801) 769-3000
FAX: (801) 769-3001
cmauth@cmautah.com

Date: 12/16/16

This plat is for reference only and no liability is assumed for any inaccuracies, incorrect data or variations with an actual survey

View the Recorder's Online Parcel Map

7. Discussion Item – General Plan Industrial Zone Review.*(30 minutes)*

The Council will receive information regarding the current Lindon City General Plan specifically industrial land use designations. No formal action will be taken at this time.

See attached information from the Planning Department.

General Discussion — Lindon City General Plan Mixed Commercial and Industrial Land Use

BACKGROUND

In preparation for updating the General Plan, City Staff will present a number of review sessions based on the existing Plan. This is intended as a review only and no updates, amendments, or changes are being presented. It is hoped that this review will lay the groundwork for discussing the General Plan update that is forthcoming.

DISCUSSION & ANALYSIS

In order to become familiar with the purpose and goals of the different non-residential land use designations in the City, please read the excerpts from the current General Plan in attachment one.

A 20 year map history of designated General Plan land uses is also attached. This will allow us to observe how the land use designations have changed with subsequent General Plan updates since 1995.

At the time of the City Council meeting, additional information will be presented regarding property taxes, sales tax, and other relevant information regarding land use impacts.

ATTACHMENTS

1. Current General Plan (2011) text on commercial/industrial land uses
2. 1995 General Plan Land Use Map
3. 2001 General Plan Land Use Map
4. 2006 General Plan Land Use Map
5. 2011 General Plan Land Use Map
6. 2016 General Plan Land Use Map

Commercial Land Uses provide a variety of goods and services to the people who visit, live, and work in Lindon. It is the purpose of the commercial area to provide areas in appropriate locations where a combination of business, commercial, entertainment, and related activities may be established, maintained, and protected. Commercial use areas should be located along major arterial streets for high visibility and traffic volumes.



Retail businesses found within the CG zone along State Street in Lindon. (2010)

Whenever commercial uses are adjacent to established or planned residential areas, special care must be taken to ensure privacy and protect personal property. Methods of protecting residential areas by providing transitions and buffers between residential and commercial areas include increased setbacks, landscaping, restricted land uses, diversion of traffic, controlled noise or light, height limitations, and transitional land uses such as research and development office uses. (Colors refer to General Plan Land Use Map)

- *General Commercial* (pink): This category includes retail and service oriented businesses, and shopping centers which serve community and regional needs. Includes area typically zoned CG, PC-1, and PC-2.
- *Mixed Commercial* (violet): This category includes general commercial, low intensity light Industrial, and research and business uses. Includes areas typically zoned MC.

The goal of commercial development is to encourage the establishment and development of basic retail and commercial stores which will satisfy the ordinary and special shopping needs of Lindon citizens, enhance the City's sales and property tax revenues, and provide the highest quality goods and services for area residents. Objectives of this goal are to:

1. Expand the range of retail and commercial goods and services available within the community.

2. Provide for adequate access, off-street parking, traffic circulation, noise buffering, and other operational considerations within commercial areas.
3. Improve the image and appearance of commercial areas by adoption of specific design guidelines and possible improvement districts, especially along State Street, Geneva Road, 200 South, 600 South, 700 North and the freeway interchange off-ramp areas.
4. Promote new office, retail, and commercial development along State Street and 700 North.
5. Encourage development of commercial facilities, such as hotels, restaurants and vehicle-related services at transportation interchanges.
6. Carefully limit any negative impact of commercial facilities on neighboring land-use areas, particularly residential development.
7. Build upon existing commercial site design and development standards, including architectural design guidelines and guidelines for landscaping and signage, to express the desired overall image and identity as outlined in the Community Vision Statement.
8. Encourage safe and convenient pedestrian access to shopping and service areas.

Industrial Land Uses provide for employment and manufacture of materials which are essential to the economy of Lindon City. It is the purpose of this industrial district to provide areas in appropriate locations where a combination of research and development, manufacturing, and industrial processing and warehousing may be conducted. (Colors refer to General Plan Land Use Map)

- *Research and Development* (dark orange): This category is for areas of very light industrial uses with the character of a high-tech research park, corporate offices, and/or commercial uses which are compatible with surrounding properties. Depending on specific business activities, this type of development is viewed as particularly helpful for buffering between residential and other uses. Includes areas typically zoned R&B.



Technology businesses located in the R&B zone in Lindon. (2010)

- *Light Industrial* (light gray): This category is for areas where manufacturing, industrial processes, and warehousing uses not producing objectionable effects may be established. Some related retail uses are appropriate for this designation. Includes areas typically zoned LI.
- *Heavy Industrial* (dark gray): This category is for areas where heavy manufacturing industrial processes necessary to the economy may be conducted. Includes areas typically zoned HI.



View of industrial office / warehouse building that has been typical of recent developments in the LI zone. (2010)

The goal of commercial and industrial development is to promote employment opportunities, quality businesses, and environmentally clean industrial and technology development which will provide a diversified economic base and will complement local retail, commercial, and industrial establishments in harmony with the community's overall country image and identity as reflected in the Community Vision Statement. Objectives of this goal are as follows:

1. Encourage the development of high quality, aesthetically pleasing business park areas incorporating major landscape features.
2. Identify those areas most appropriate for business park development in future growth areas, such as major highway access areas.
3. Establish and enforce standards with respect to environmental concerns such as; noise, air quality, odor and visual.
4. Increase the city's business base in the technology sector, building on the existing base and growing technology infrastructure, and consider expanding the Research and Development zones.

Special Use areas include land use classifications that are distinct from the other major land use classifications. These uses include open spaces, public and quasi-public uses, and areas of significant environmental constraints which impact development. The locations for these activities are interspersed within residential and non-residential areas.

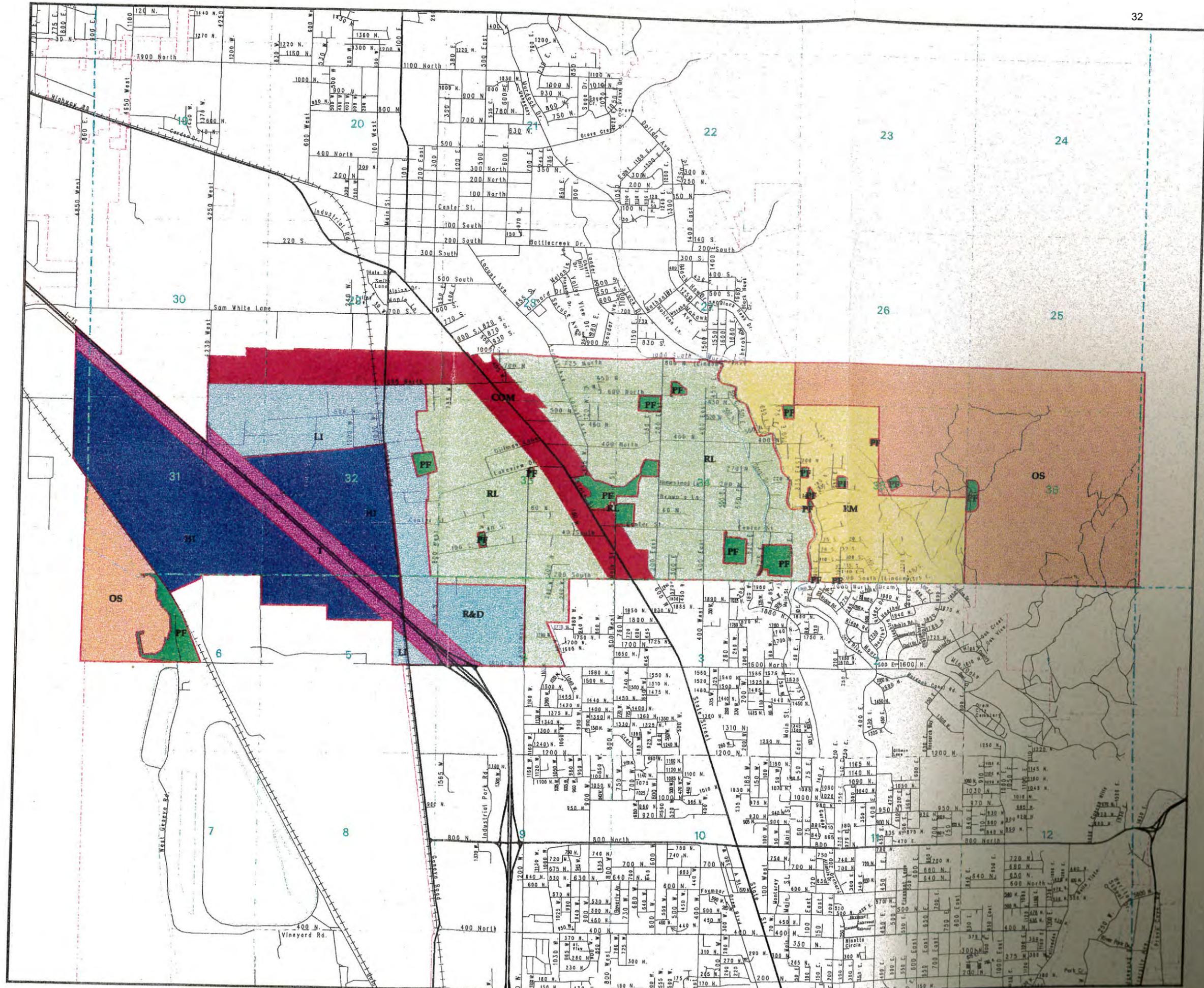
In some cases the City does not control the location of special uses, such as schools, major electrical transmission lines, and telecommunications infrastructure. The State and Federal Government can preempt local land use authority. However, the City will work with other jurisdictions and agencies on decisions regarding land

Lindon Landuse General Plan Map

No Scale
Plot Created: October 10, 1995

Map Legend

-  RM Residential Medium
-  RL Residential Low
-  COM Commercial
-  T Travel
-  OS Open Space
-  R&D Research & Development
-  LI Light Industrial
-  HI Heavy Industrial
-  PF Public Facilities



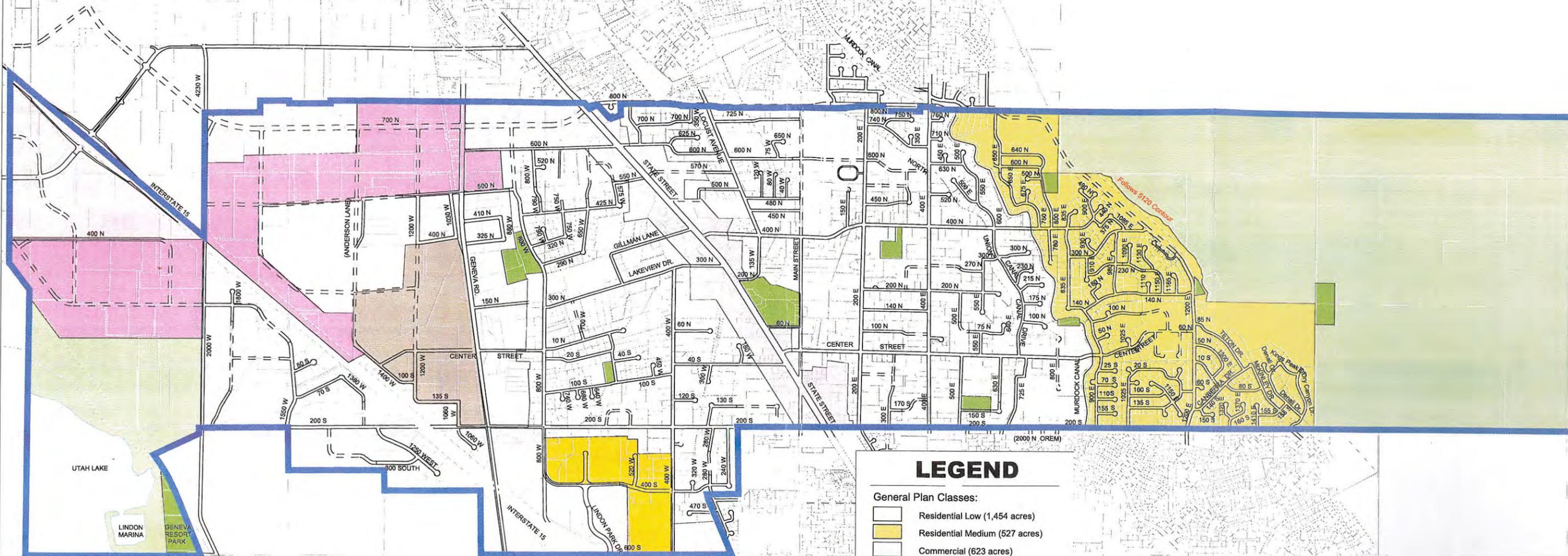
Lindon City Planning Department
383 West Lakeview Drive
Lindon, Utah 84042
(801)785-5043

8. Public Hearing — Ordinance Changes to Commercial Design Guidelines – Ordinance #2017-1-O. *(15 minutes)*

Lindon City staff requests a revision to Chapter V. Architectural Character regarding massing and orientation; windows and doors; roofing; mechanical areas. Following review the Planning Commission recommended approval with a four to one vote.

See attached information from the Planning Department.

Lindon City General Plan



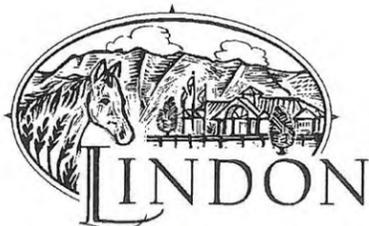
LEGEND

General Plan Classes:

- Residential Low (1,454 acres)
- Residential Medium (527 acres)
- Commercial (623 acres)
- Mixed Commercial (425 acres)
- Research & Development (85 acres)
- Light Industrial (627 acres)
- Heavy Industrial (152 acres)
- Parks/Public Facilities (73 acres)
- Open Space (1487 acres)

Streets:

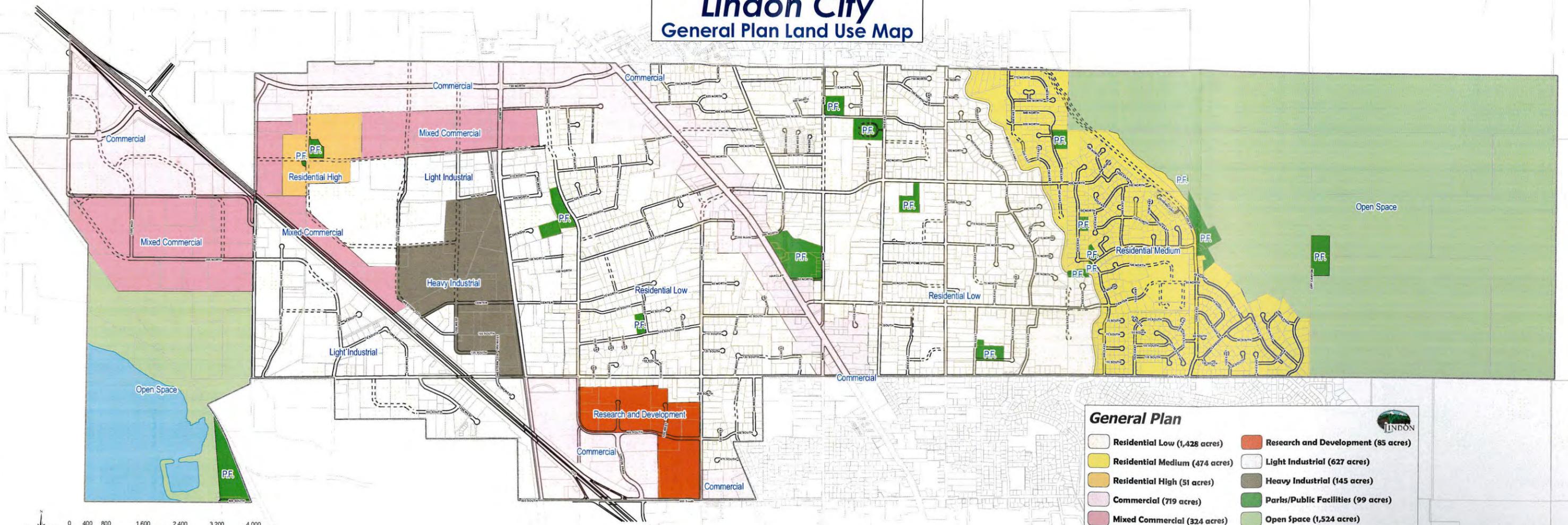
- Future Roads
- Existing Roads



Plot Date: 30 April 2001

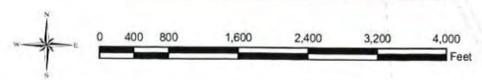


Lindon City General Plan Land Use Map

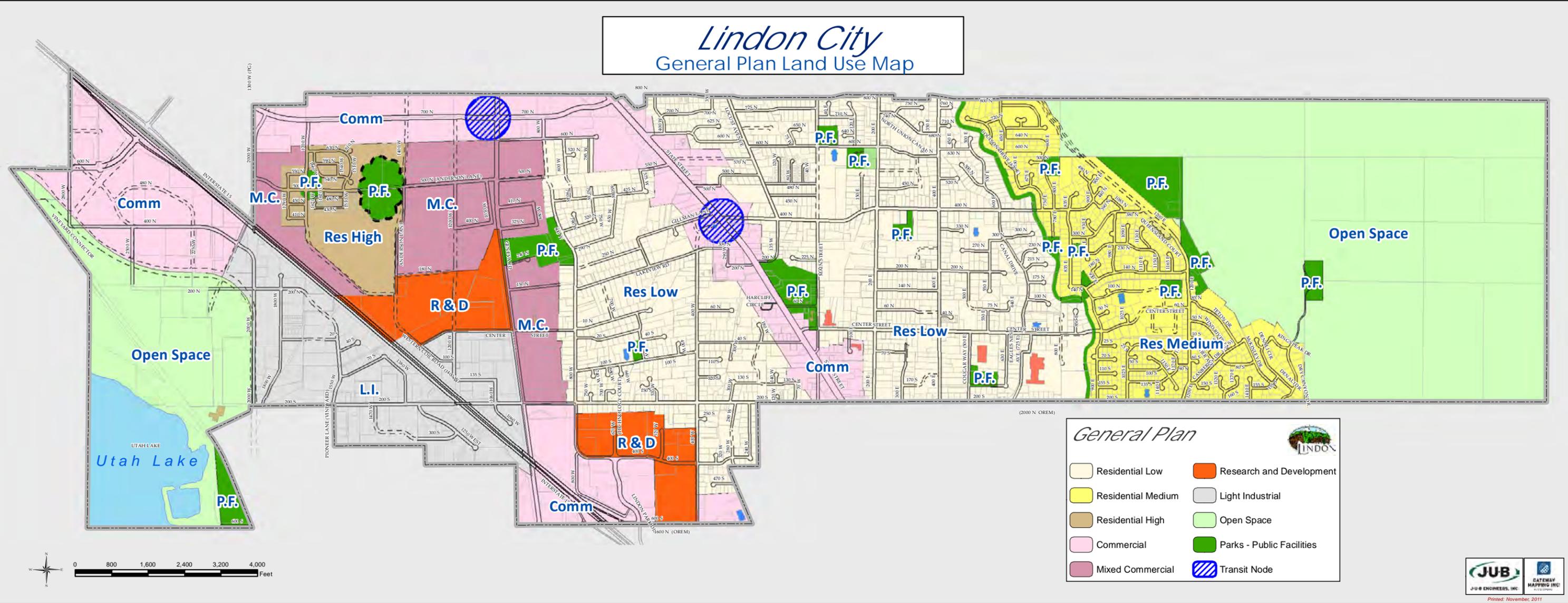


General Plan

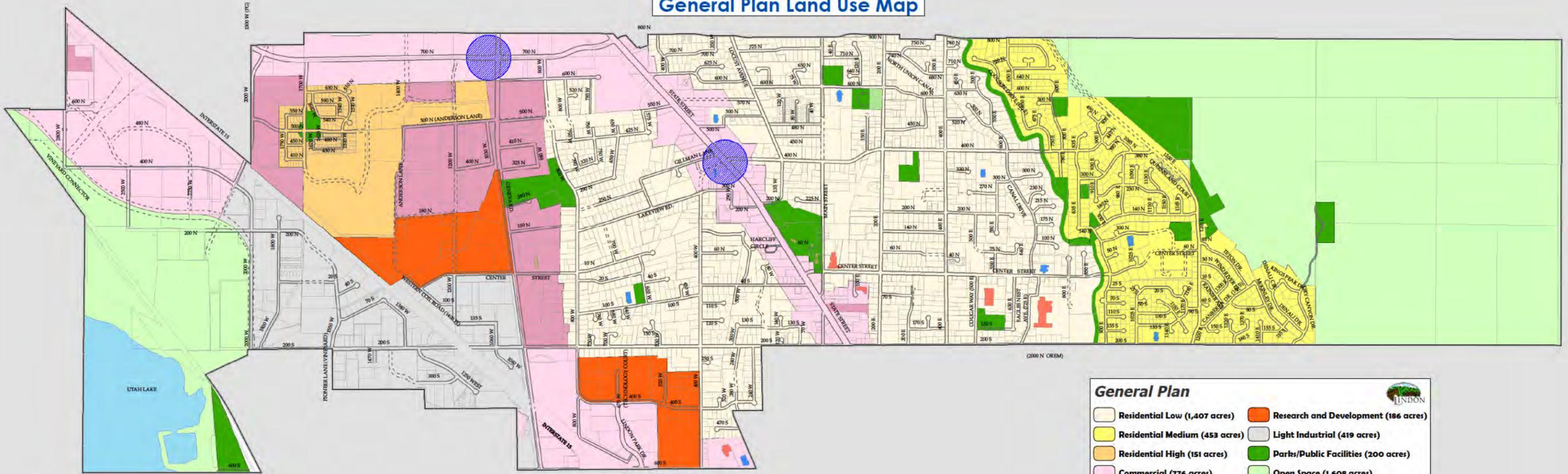
	Residential Low (1,428 acres)		Research and Development (85 acres)
	Residential Medium (474 acres)		Light Industrial (627 acres)
	Residential High (51 acres)		Heavy Industrial (145 acres)
	Commercial (719 acres)		Parks/Public Facilities (99 acres)
	Mixed Commercial (324 acres)		Open Space (1,524 acres)



Lindon City General Plan Land Use Map



Lindon City General Plan Land Use Map



General Plan	
	Residential Low (1,407 acres)
	Residential Medium (453 acres)
	Residential High (151 acres)
	Commercial (776 acres)
	Mixed Commercial (312 acres)
	Research and Development (186 acres)
	Light Industrial (419 acres)
	Parks/Public Facilities (200 acres)
	Open Space (1,608 acres)
	Transit Node (33 acres)

Two Lindon residents, Krisel and Mike Travis, were in attendance and also expressed concern over limiting creative design in Lindon and concern that the standards were too conservative in general.

ATTACHMENTS

1. Slides from Committee presentation regarding architectural standards
2. Revised Lindon City Design Standards
3. Current Lindon City Zoning Map

Public Hearing — Ordinance Amendment Commercial Design Standards

<p>Applicant: Lindon City Staff Presenting Staff: Hugh Van Wagenen</p> <p>Type of Decision: Legislative Planning Commission Recommendation: Recommended approval with 4-1 vote. Commissioner Johnson voted nay because the ordinance takes an already restrictive document and makes it more restrictive.</p>	<p><u>SUMMARY OF KEY ISSUES</u></p> <ol style="list-style-type: none"> Whether it is in the public interest to approve the proposed amendment to the Lindon City Commercial Design Standards. <p><u>MOTION</u></p> <p>I move to (<i>approve, deny, continue</i>) the proposed Commercial Design Standards amendment (<i>as presented, with changes</i>).</p>
---	---

BACKGROUND

At the encouragement of the City Council and Planning Commission, the 700 North Steering Committee was assembled to make suggestions regarding the future development of the 700 North commercial corridor. The Committee was made up of elected, appointed, and lay members of the community and began meeting in January of 2016. Recently, the Committee presented their recommendations to the Commission and Council. One such recommendation was to amend the existing Commercial Design Standards to reflect additional standards regarding building articulation and façade variation, screening of mechanical equipment, and building entryways. The changes are meant to increase the architectural quality of new commercial development within Lindon.

DISCUSSION & ANALYSIS

- In the original 700 North Steering Committee presentation, four slides regarding additional architectural standards for building articulation, building entrances, and screening of mechanical equipment were presented for consideration. Please see attachment one for those slides.
- New wording has been added to *Section V. Architectural Character* intended to reflect the suggested changes to the Standards. Please see attachment two for the redline additions to the text. Text changes can be found on pages 24, 27, and 29 of the Standards.
- The Committee suggestions included several pictures to demonstrate the desired architectural feature. No additional pictures have been added to the Commercial Design Standards, although that is a possibility.
- Although originating with the 700 North Committee, the presented changes would be applicable to all new development in every commercial zone within the City, not strictly the Lindon Village Commercial zone. See attachment three for the current zoning map.

PLANNING COMMISSION

There was a healthy discussion amongst the Planning Commission regarding this proposal. The discussion focused around whether or not these changes should be applied City wide, just in the Lindon Village Commercial zone, and whether any design standards were appropriate at all.

There was concerned voiced from Commissioner Johnson that increased design standards limit the ability of small business owners to develop smaller lots along State Street due to cost constraints.

Suggested Lindon Design Guideline Amendments

Break-up long monotonous rooflines (determine minimum feet) by dividing elevations into smaller parts. This can be accomplished by a change of plane, projection or recess, or by varying a cornice or roofline (ie. Pitched)

Require Crown-molded rooflines. Large uninterrupted expanses of a single material are prohibited.

The apparent mass of large buildings shall be reduced and a varied architectural street appearance created by manipulating the building form using:

1. Offsets
2. Recesses
3. Changes in Plane
4. Changes in Height
5. Windows
6. Trellises
7. Berming at the edge of buildings in conjunction with landscaping can be used to reduce structure mass and height along street facades.



Appropriate use of roof plane variation and materials



Innapropriate roof plane and use of materials

Require Screening of utility/mechanical units

Ground mechanical units must be screened from view with wing walls, landscaping, or a combination of both.

Appropriate vegetative buffers shall be placed to screen and buffer all utility boxes and pedestals.



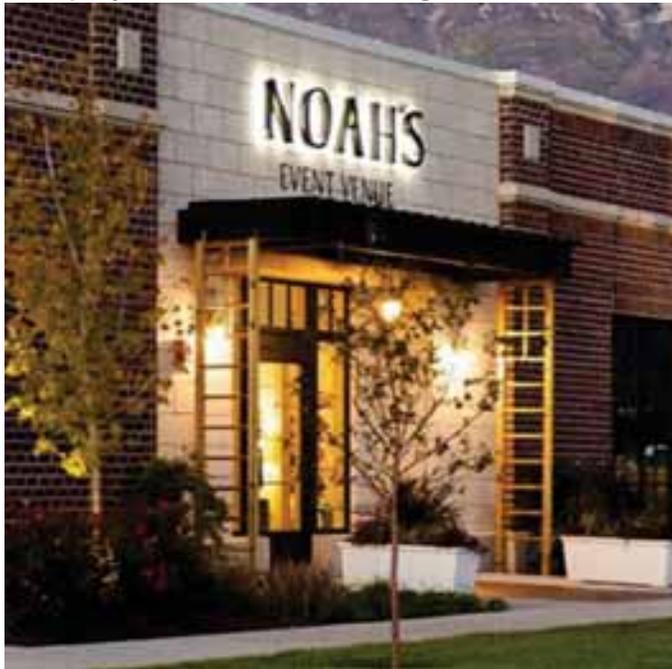
Appropriate screening of utility boxes with walls or vegetation



Innapropriate or non-existent screening

Entries - Require entries have one or more of the following:

1. Canopy, overhang or arch above the entrance (columns & pillars), Canopy or awning to create visual depth and shade
2. Recesses or projections in the building facade surrounding the entrance
3. Peaked roof or raised parapet structures over the door
4. Display windows surrounding the entrance



Articulation - enhance the visual interest of buildings

- (1) Facade modulation – stepping portions of the facade to create shadow lines and changes in volumetric spaces,
- (2) Use of engaged columns or other expressions of the structural system,
- (3) Horizontal and vertical divisions – by use of textures or materials (usually combined with facade modulation),
- (4) Dividing facades into storefronts with visually separate display windows,
- (5) Providing projections such as balconies, cornices, covered entrances, porte-cocheres, trellis', pergolas, arcades and colonnades (providing such trellis' and awnings extend outward from the underlying wall surface at least 36-inches),
- (6) Variation in the rooflines by use of dormer windows, overhangs, arches, stepped roofs, gables or other similar devices.



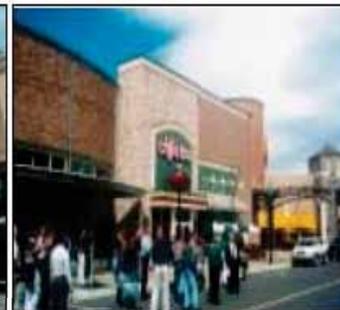
Appropriate Articulation



Inappropriate Articulation



Appropriate



Appropriate



Inappropriate building articulation

ORDINANCE NO. 2017-1-O

AN ORDINANCE OF THE CITY COUNCIL OF LINDON CITY, UTAH COUNTY, UTAH, AMENDING THE COMMERCIAL DESIGN STANDARDS OF THE LINDON CITY CODE TO MODIFY ARCHITECTURAL AND DESIGN STANDARDS IN THE COMMERCIAL ZONES AND PROVIDING FOR AN EFFECTIVE DATE.

WHEREAS, the proposed amendment will provide greater certainty and predictability in the land use application process; and

WHEREAS, the proposed amendment is consistent with the vision of the General Plan to ensure that new development is of high quality and reflects quality architectural and site design standards consistent with its particular use and location; and

WHEREAS, the proposed amendment is consistent with the goal of the General Plan to improve the image and appearance of commercial areas by adoption of specific design guidelines; and

WHEREAS, the Lindon City Planning Commission has recommended approval of an amendment to the Lindon City Commercial Design Standards; and

WHEREAS, a public hearing was held on January 24, 2017, to receive public input and comment regarding the proposed amendment; and

WHEREAS, the Council held a public hearing on February 7, 2017 to consider the recommendation.

NOW, THEREFORE, BE IT ORDAINED by the City Council of the City of Lindon, Utah County, State of Utah, as follows:

SECTION I: Lindon City Commercial Design Standards are hereby amended to read as follows:

See attached Lindon City Commercial Design Standards

SECTION II: The provisions of this ordinance and the provisions adopted or incorporated by reference are severable. If any provision of this ordinance is found to be invalid, unlawful, or unconstitutional by a court of competent jurisdiction, the balance of the ordinance shall nevertheless be unaffected and continue in full force and effect.

SECTION III: Provisions of other ordinances in conflict with this ordinance and the provisions adopted or incorporated by reference are hereby repealed or amended as provided herein.

SECTION IV: This ordinance shall take effect immediately upon its passage and posting as provide by law.

PASSED and ADOPTED and made EFFECTIVE by the City Council of Lindon City, Utah, this _____ day of _____, 2017.

Jeff Acerson, Mayor

ATTEST:

Kathryn A. Moosman,
Lindon City Recorder

SEAL



Commercial Design Standards

Adopted Aug. 1, 2006. Revised Nov. 21, 2006
 Revised Feb. 2014 (Sections 2.5.2; 4.2; VI Appendices)
 Guidelines to Standards September 2015
Revised 2017 by Ordinance 2017- -O



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I. Introduction

1.1 Lindon History

The Lindon City area was settled in 1861, and was originally known as “Stringtown” because of the line pattern of homes built along the route between Pleasant Grove and Orem. The town incorporated in 1924 with 458 residents, encompassing an area of approximately 3 and one half square miles. Commercial and public buildings were also built during the early years of settlement and the incorporation of the town. However, unlike neighboring Pleasant Grove and many other small Utah towns in the late 1800's and early 1900's, Lindon did not develop a town center or higher intensity core of commercial and public buildings.

The city has experienced significant residential growth in the last few decades with the 2005 population nearing 10,000, and commercial uses have expanded to meet the needs of Lindon's growing population. Lindon wishes to accommodate this commercial growth, yet retain the rural character of the community, as well as preserve the historical and architectural attributes that contribute to the character of Lindon. Several historic residential structures remain in Lindon, with some being used in a commercial capacity. Two of these residential structures, along with the historic Cullimore Mercantile building stand at the intersection of State Street and 400 North. These remaining structures, along with other preferred building styles, provide the historical basis upon which these standards have been developed.



Fig. 1 View of Lindon City, Utah Lake, and Oquirrh Mountain Range.

1.2 Intent of Standards

This document is intended to be applied to all areas zoned General Commercial (CG). The Lindon City commercial areas, consisting primarily of the State Street and 700 North corridors, are dynamic areas and are progressing forward. The object of these guidelines is to guide future development that will enhance the character of Lindon City.

In order to respect and reference their heritage, Lindon has chosen to draw primarily upon the historical building types and styles of the town for new commercial structures. *See figure 2.* City officials and residents also find craftsmen and alpine type construction very appealing.

Fig. 2 Historical residence.



Input from the public, city staff, and elected officials has led to the establishment of guiding principles to promote appropriate development standards (*See figure 3*) that will create a vibrant commercial corridor as well as small commercial districts that respect and enhances the heritage of the community's identity. Although Lindon does not have a historic town center, there is a desire to create that kind of small town Main Street district or gathering place in Lindon.

Figure 3: Community input.



New structures should affect the area in a positive manner, signifying continued growth, and not be detrimental through use of inappropriate massing, scale, or materials. These standards utilize approaches that intend to encourage a sense of place and a sustainability of the area. The standards are not meant to preclude making exception in the case of innovative design, and modifications to the standards are allowed at the discretion of the Planning Commission and the City Council.

Each of the three subsections in the document provides guidance for various design aspects of building in the Lindon City Commercial District:

- Streetscape
- Site Design
- Architectural Character

Photographs, drawings, and diagrams included in each section illustrate desirable characteristics that describe the general intent of these standards. Strict adherence to the standards may require some flexibility depending on specific site conditions. Such flexibility, however, should not be contrary to the general intent for each section, as described.

Several useful tools to assist the city and developers are included in the appendices, including a color palette of Utah-based earth tones, a glossary of architectural terms, and a building materials guide.

II. Streetscape

2.1 General Intent / Introduction

Streets are important public spaces that contribute to the character and identity of a commercial area. The intent of streetscape standards is to create a collective streetscape of buildings,

landscaping, and other site design elements that identify the commercial zones of Lindon City as a cohesive commercial district, rather than a conglomeration of individual commercial structures. The overall streetscape design will be visually attractive, as well as safe and comfortable.

2.2 Layout & Spatial Form

The general pattern of buildings should help define streets as public open spaces. The following standards can be used to enhance spatial definition of the commercial area. Special consideration should be given to further enhance the streetscape and public amenities of key intersections, such as State Street and 400 North, and future key intersections along 700 North. The use of a special overlay district may be used to implement a special streetscape enhancement approach.

- Buildings located on corner lots should orient to both streets. In order to define these corner pad sites, a setback of 20-50 feet is recommended on both street-facing facades as far as sites permit.
- Orient and align the street-facing façade of buildings to the street to help define and shape the street.
- Orient primary entrances to streets and other public spaces, such as plazas, courtyards, and pathways, that have higher levels of pedestrian activity.
- Where possible, maintain and restore contributing historic buildings to conserve historic character in the Commercial District.
- Consolidate driveways and entrances to minimize the amount of breaks, maximize safety and support the continuity of the streetscape design.
- Locate a landscaped parking strip between street and walkway to provide a buffer from traffic. Parking strips are recommended be a minimum of 6 feet wide. A width of 10 feet may be allowed to accommodate a meandering style of walkway/sidewalk.



Fig. 4 When possible use landscaping to maintain a buffer between sidewalks and parking lots.

2.3 Amenities (Trees, Landscaping, Furnishings)

- The use of amenities, such as street trees and planter boxes, are important to an overall streetscape design and can greatly help define a wider street. A consistent landscape and amenity design and theme along the length of a street or block can strengthen the association of unrelated buildings.
- In addition to street trees, other landscaping such as lawn, shrubs, or ground covers provide a buffer between people and cars, as well as providing seasonal colors. Proper maintenance is essential to keep the benefits of these areas continuous.



Fig. 5 Planter boxes/pots add to the color and attractiveness of the streetscape.

- Where a landscaping strip does not exist, install planter boxes and/or plant trees along sidewalk.
- Hanging baskets or large pots are also encouraged to enhance the façade and provide color and create a more pedestrian friendly atmosphere.
- Coordinated street furnishings, such as fencing, trash receptacles, bollards, bicycle racks, and seating, can be an important component in creating a unified, attractive look to a commercial streetscape. Maintenance, safety, and durability are the main considerations regarding choice and placement of furnishings.



Fig. 6 Planters, benches, parking strips, pots, and trees create an attractive commercial streetscape.

2.3.1 Trees

Street trees can be a critical element in defining the edges of a street. To realize the effect, the correct type of street trees must be installed in a well-designed manner and well maintained over time. Refer to the [“Lindon City Tree Planting Guide”](#) for detailed information on selecting trees for a site.

- Provide a parking strip of street trees between street and walkway, with trees spaced a minimum of every 30 feet. Trees should be placed a minimum of 40 feet from street corners to allow visibility at intersections.
- Select trees and other plant materials based on appearance, hardiness, and appropriateness to site location, solar orientation, and climate. Low-water, low maintenance, and adaptable varieties are desirable. Consult water conservation programs and the Lindon City Tree Planting Guide for recommendations of appropriate tree varieties.
- Keep the choice of street tree(s) consistent for each corridor. Establish a pattern or design that will continue the length of the corridor for greatest effect in defining the space.
- Select trees that will provide a large canopy while maintaining a suitable height to keep unobstructed passage of vehicles and pedestrians.



Fig. 7 Trees with a large canopy provide shade and help to define a specific corridor.

- Space trees appropriately from each other and from buildings and other structures to allow for full canopy growth.
- Street trees should have a consistent, continued spacing without omissions. Design driveways, lamp poles, and other elements around the spacing of the trees.
- Careful consideration should be given when selecting the type and location of trees in front of businesses so as to not obstruct business signage or building identification (See fig. 8).
- Street trees planted along the 700 North corridor shall be specified by the City. Type and location of the required trees can be obtained at the Planning Dept.



Fig. 8 Trees should be appropriately spaced and located so as not to overly obstruct the view of buildings, signage, or entryways (as shown above).

2.3.2 Landscaping, Pedestrian paving, & Entry Markers

- Landscaping along streets, easements, and public corridors should be consistent to reinforce the overall identity of the commercial district.
- All landscaped areas should be regularly maintained in a neat and orderly appearance as appropriate to the plant types. Leaves, clippings, and other debris should be immediately cleared when accumulation occurs.
- A parking strip of lawn may be most appropriate in streetscapes with a large area between the sidewalk and the street, or where a low pedestrian volume exists. Turf should be used in areas where there is a minimum of 4 feet available, in order to accommodate irrigation systems and mowing.
- Pavers and stamped or colored concrete are desired effects around pedestrian foot traffic areas.
- Parking strips should use a system that is permeable, in order to sustain and enhance the survival of street trees.
- Acceptable paving materials include brick, flagstone, or concrete pavers. Colored, scored, or stamped concrete may be considered.
- Identifying a beginning and end of a corridor or district can enhance the definition of the street. Use well-designed entry monuments, statues, or other means to mark the entrance into the Commercial District of Lindon City.
- Use district gateway markers throughout the commercial zone to define the district. The scale of the markers should relate to the street width and size of buildings nearby. Markers should be effective both for the pedestrian and vehicular traffic (See fig. 10).



Fig. 9 Planted medians are desirable ways to define the streetscape, especially on wider streets.



Fig 10 Marked entrances define commercial districts.

2.4 Paths & Walkways

Paths and walkways are used to provide proper separation of pedestrian and vehicular movement in a manner that encourages pedestrian activity, comfort, and safety. Paths and walks within the commercial areas are encouraged to be linked in some way to the overall trail system of Lindon City.

- Crosswalks should be of a paving material different from the rest of the street or drive to emphasize their location and increase the safety of pedestrians.
- Walkways and sidewalks should be separated from travel lanes by either on-street parking or landscape treatments.



Fig. 11 Pedestrian corridors should be marked by landscaping and other treatments. Corridors provide access between buildings and parking areas. Delineating crosswalks using a different paving material or painted stripes enhances pedestrian safety and the overall streetscape.

- Walkway widths will vary depending on intensity of adjacent uses. Recommended minimum requirements are 10 feet for primary walkways in high pedestrian traffic areas (i.e., stores, restaurants, etc.), and 4 feet for secondary walkways in lower traffic areas (i.e., service oriented businesses, public buildings, etc.). All high traffic walkways shall have a minimum of 6' unobstructed walking space (with respect to overhanging of parked vehicles, landscaping, seating, etc.).
- Use wider sidewalks or patios to create additional space for more intensive sidewalk uses such as outdoor dining, rather than greatly encumbering the sidewalk for such uses.
- Provide overhead weather and sun protection, such as canopies, awnings, balconies, or other overhangs, at building entrances.
- Provide pedestrian circulation and access to buildings adjacent to pedestrian corridors.
- Periodically interrupt large blocks and development parcels exceeding 200 feet in length periodically with pedestrian paths, alleys, or driveways. These routes should be provided with appropriate lighting and amenities such as landscaping and seating.
- Develop pedestrian corridors to connect activity centers and blocks throughout the business district and to surrounding residential neighborhoods. This promotes foot traffic and creates a more vibrant commercial district.
- Use walkways between neighboring developments to enhance the flow of pedestrians.
- Where on-street parking is not practical, other types of buffering such as landscaping, street trees, seating, etc., should be used to improve perception of pedestrian safety.

- Articulate and enhance pedestrian ways with furnishings, waste and recycle bins, lighting, paving materials, public art, and landscaping.



Fig. 12 Providing open space for seating creates a more friendly and inviting environment for walking, dining, and lounging.

- Provide for proper collection and drainage of water, snow, and ice from roofs, balconies, etc., to avoid standing water on walkways that may freeze and create a slipping hazard.
- Drainage grates should allow safe passage by bicycles and pedestrians, and should be designed with some redundancy to reduce the possibility of clogging by leaves and other debris.

2.5 Lighting & Fencing

Coordinate streetscape lighting is required throughout the Commercial District, including type of light source, style of poles and fixtures. Lighting styles should be harmonious and complement the architectural and landscape features of the district.

2.5.1 Street Lighting

Street lighting is an important component of the overall character of a commercial district, as well as improving the quality and safety of the street. Street lighting should be consistent throughout the district. Street lighting can also be placed in planted and paved medians.

- If on street parking is provided, street light poles should be located at least 2.5 feet from the curb to avoid contact with car doors and bumpers.
- Light poles should be placed a minimum of 100 feet apart.



Fig.13. Decorative lighting with hanging baskets increases safety and enhances the streetscape.

- Street lights are required along streets in commercial zones. The approved lighting in Lindon is the Holophane Washington Postlite Luminaire on a black post with a total height of 19'-9" (See fig. 13).
- Light fixtures used in parking areas should not exceed 25 feet in height.
- Single globe luminaries are recommended. Multiple globe luminaries may be considered for entryway points or special locations.

2.5.2 Fencing

- A white two rail fence is encouraged in all CG, MC, and PC-1-2 zones to enhance the character and consistency of the commercial area of Lindon City.
- Fences should not block access of pedestrians from the sidewalk to a commercial structure(s).
- Fencing height along public street frontages shall not exceed 36 inches.



Fig. 14 Two rail white fence adds continuity and uniformity to the commercial district.

2.5.3 Pedestrian Scale/Pathway Lighting

Pedestrian scale lighting plays an important role in the overall character of a commercial district. This type of lighting, such as lower poles and bollards, should be used along walkways, public plazas, and other pedestrian areas to illuminate and identify routes and provide safety at night.

- Align lights with street trees where possible.
- Lights should be spaced 100 feet apart to avoid excess glare and provide room for street trees and other furnishings.
- Lights should be properly located to avoid glare into second story windows.
- Single globe luminaries are recommended. Multiple globe luminaries may be considered for entryway points or special locations.



Fig. 15 Lighting adds charm to a shopping district and encourages shopping after dark.

III. Site Design

3.1 Setbacks

Front, street-facing setbacks should be compatible with the pattern of Lindon's historic structures being used in a commercial capacity, such as those located at the 400 North and State Street intersection.

- A setback of 20-50 feet is generally desirable on State Street and 700 North.
- Avoid setbacks greater than 50 feet. Buildings that are located too far back from the street generally do not contribute in a positive manner to the overall streetscape of the area.
- A maximum front setback of 50 feet with no more than one row of parking stalls is recommended to avoid a suburban atmosphere of large parking lots fronting the street. A landscaped setback should be located between the sidewalk and the parking area. For large 'big box' buildings, the city may consider increasing the maximum setback.
- Large 'big-box' stores are encouraged to locate towards the rear of a property and provide smaller pad sites closer to the primary public street. These pad sites can consist of secondary buildings with more pedestrian oriented amenities.
- Utilize the front and side setbacks to create usable public gathering spaces, such as plazas or patio/outdoor seating areas, or for landscaping or public art.
- Avoid placing parking in the front setback between building and street; the majority of parking shall be located to the side or rear of a building to maintain the connection between building and street.
- Greater setbacks may be considered for buildings that propose a public park/plaza area in front of the primary, street-facing façade.



Fig. 1 Where street parking exists, wide sidewalks are desirable to provide an uninhibited walkway for pedestrians.



- Side and rear setbacks for structures within a commercial zone shall not be required.
- Setbacks for structures abutting residential uses or zones shall be a minimum of 40 feet.
- Corner sites shall have a recommended setback of 20-50 feet on both street facing facades in order to properly define the intersection if site permits.
- Include a minimum landscaped front setback of 20 feet along 700 N and State Street to retain a sense of openness and small, rural atmosphere. Front landscaping on side streets may include a hard-scaped public plaza, large sidewalk or outdoor patio dining area.
- For narrower side streets off of the main commercial corridors (State Street and 700 North), no minimum landscaped setback is required, but there cannot be a net loss in landscaping. This will allow for a pedestrian scale of buildings that are built closer to the sidewalk, enhancing the sense of a commercial district for an area.
- A 20' setback should be considered between the sidewalk and parking lots that are located to the side of a building.

3.2 Parking

- Surface parking should be located so as to minimize the break in streetscape character and design, yet have sufficient visibility for safety and convenience.
- The use of shade trees, landscaping, and low screen walls can help diminish the dominant and often negative visual impact of parking lots, especially near adjacent residences and parks. A minimum of 20% of the parking lot area is required to be landscaped (See fig. 2).



Fig. 2 Landscaping beautifies the area and conceals large parking lots as well as provides shade - thus cooling the urban environment.

- On-street parking provides an effective buffer for the pedestrian as well as easy access to surrounding businesses and reduces the amount of surface lot parking needed. Spaces on side streets provide a positive perception of parking availability.
- Locate the majority of surface parking to the rear of buildings. Side parking lots are allowed, but should be minimized to allow more continuity between adjacent structures. Big box buildings may require surface parking to be located in the front.
- Encourage the use of shared parking lots that provide more efficient parking patterns. Shared parking with all businesses in an area can help reduce the overall amount of surface parking needed in the commercial district.

- On-street parking may be considered where street width and traffic patterns/speed limits allow. Angled parking may be effectively utilized on side streets.
- Parking should be visible from an entrance to the building.
- Avoid access to parking from/through residential areas.
- Design primary access points to avoid traffic conflicts. Wherever possible, they should be located across from existing access drives and streets.
- Minimize the number of access points from the street by encouraging shared/common driveways for multiple buildings or a building complex.
- Encourage the use of side streets or drives for access to parking areas.
- Make parking areas visible enough to discourage crime and vandalism and utilize CPTED (Crime Prevention Through Environmental Design) principles in the design and layout of the parking (resource: <http://www.cpted-watch.com/>).
- Provide perimeter and interior islands throughout parking lots to break up hard-surfaced areas. Islands should be landscaped with shade trees that will provide a canopy as well as other lower level landscape elements and plantings (Refer to Lindon City's Tree Guide for recommended varieties).
- Interior islands should be minimum 6' to 10' in width to allow adequate drip line for trees and landscaping. This minimizes visual impact of expanses of asphalt and controls cross traffic through the lot.
- Locate parking lots back from buildings to allow for pedestrian space, such as walkways benches, and landscaping.
- Separate parking from pedestrian walkways, using landscaping elements.
- Include other amenities such as public art near or within parking areas to add visual interest.



Fig. 3 In smaller spaces, smaller varieties of trees are required as to not destroy the sidewalk or create extensive maintenance costs.



Fig. 4 This short brick fence separates the sidewalk from the parking lot and obscures the cars from the street view.



Fig. 5 Shaded on-street parking is desirable on small side streets.

3.3 Driveways & Circulation

- Encourage shared driveways, cross easements, and automobile entrances to minimize vehicle trips and conflicts between automobiles and pedestrians. Use a coordinated and shared system to access parking and delivery areas at the rear of buildings where possible.

- Walkway materials perpendicular to the drive shall continue across the drive apron to help alert drivers to possible pedestrian activity.
- Interior circulation drives should be articulated and reinforced with other site design features such as lighting standards, trees and other plantings, special paving and walkways. Include an interior circulation system that clearly defines the route to parking areas.
- Minimize conflicts between pedestrians, service vehicles, and customer vehicles through proper design and layout of the parking lot.
- Reduce traffic impacts to neighboring residential areas with appropriate landscape buffers between the uses and by proper location and design of all parking areas.
- Clearly delineate crosswalks from parking areas to surrounding businesses/residences with the use of contrasting pavers and/or striping.



Fig 6 Shaded/covered walkways are desirable, especially to promote use in extreme weather.

3.4 Site Lighting

Lighting styles within individual developments should complement the architecture and landscape design as well as the overall Commercial District streetscape lighting scheme. Avoid selecting different types of lighting for individual developments.



Fig. 7 Lighting enhances architectural characteristics.

3.4.1 Building Lighting

- Lighting may be used to highlight and articulate building facades.
- Building facades should be lit primarily at street level.
- Above the first floor, light should only be used to selectively highlight unique building features without lighting the entire structure.

3.4.2 Landscape Lighting

- Lighting can be used to accent and highlight plantings and landscaping elements.
- Direct accent lighting upward into trees to achieve appropriate light levels and pleasant accent effects. This provides for a low intensity that offers dramatic illumination of nearby pedestrian areas.
- Reserve special architectural lighting for individual plaza areas to emphasize focal points.



3.4.3 Lighting Levels

- Lighting levels should be sufficient to produce a safe, visible nighttime environment, without producing excess light and glare.
- Lighting levels should not be less than 0.5 footcandles at 5 feet above the ground plane, with an average of approximately 3 footcandles at 5 feet above the ground plane throughout parking areas and pedestrian walks.
- Consider ambient lighting from indoors when determining lighting levels.
- Outdoor building lights and pole lights should not produce obtrusive off-site glare. Use full or partial cut-off fixtures that eliminate direct light pollution.

3.5 Signage

- The signs covered in this section cover mainly pedestrian oriented signs. Refer to Title 18 of the Lindon City Code for more details.
- Signs should be used sparingly to provide information, identify businesses, and assist pedestrians and drivers with way-finding. Signs should be compatible with structures and storefronts, and should be simple and straightforward to avoid visual clutter.
- Signs should be located closest to the ground floor of buildings, where pedestrians and drivers most easily see them. Signs should be easy to read.

3.5.1 Commercial Signs

- A variety of shapes, sizes, and materials are possible for most signs. Sign materials and colors should be complementary to the materials, colors and architecture of the related structure. Excessively bright colors should be avoided.
- Simplicity in design, style, and shape is preferred over complex or fancy signs.
- Signs should be large enough to be visible and read with ease, yet not dominate the structure or streetscape by an overly large scale.
- Fully backlit signs are not recommended. Individual backlit or neon letters, or front- or side-lit signs are preferred. Lighting fixtures for signs should be consistent with the architecture and lighting scheme for the building/development.
- Signage or wording is not permitted on any part of awnings.
- Sign materials should be of high quality, durable materials that will maintain their beauty and appearance for many years. Consider the use of materials such as bronze, brass and copper, that patina naturally, are suitable.



Fig. 9 Signs can add architectural character and be used to enhance the streetscape.



Fig. 10 Bright colored signs that distract from the façade are not desirable.

- Signs on historic structures should be designed and attached in such a way that they do not damage or destroy elements of the building.
- Signs within a development should have a common element, such as type of sign, color scheme, or lettering to provide a sense of continuity.

3.5.2 Sign Types and Location

Some of the types of signs recommended may be appropriate for use as a primary sign for a business entity. Others may be more appropriate for use as a secondary or pedestrian-scale sign that is better seen while walking by or through a development.

Monument signs

- A free-standing, two-sided sign, generally placed in the front setback area between the building and the street
- Appropriate at entry drives or paths for building complexes, and may include identification for multiple businesses.
- Suitable for use with historic structures to avoid unnecessary damage to the structure, which often can occur with sign installation.



Fig. 11 Monument signs are the most desirable sign type.

Blade/Bracket Mounted signs

- A two-sided sign, usually mounted by a metal bracket and projecting from a building's façade. Blade/bracket signs do not conform to the current code but may be considered as part of a pedestrian orientated development.
- Can be well suited for both pedestrians and drivers, since they can be viewed from far down a sidewalk or street depending on the size/scale.
- Can also be located on the corner of a building where they can be visible from two directions.
- Often shaped to mimic an architectural element of the building to reinforce the style of the building.
- Simple mounting brackets should be used, so as not to detract from the sign itself.



Fig. 12 Bracket signs should be moderately sized, simple, and easy to read.

Signboards/Flush Mounted signs

- Usually a long, narrow panel, located just above the main entrance on a storefront. Sometimes, individual lettering is used directly on the building instead of attached to a signboard panel.
- Generally most suitable as a pedestrian-scale sign, or at an intersection, where signs can be viewed most easily at oblique angles.

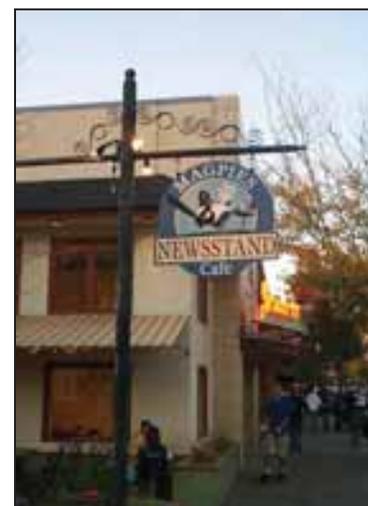


Fig. 13 Small pole signs may be permitted in pedestrian-oriented developments.

Pedestrian-scale, artistic pole signs

- Pedestrian-scale artistic pole signs are not allowed under current code but may be considered in a pedestrian-oriented development.
- Usually a wooden or metal pole with an extended arm to attach a hanging signboard that is catered to pedestrian traffic.
- Height should be such that the hanging signboard does not interfere with pedestrian traffic. Generally, height does not exceed 8 to 10 feet.
- Suitable at the front of a yard or plaza where businesses may be set back from the street or are not visible.
- Suitable for use with historic structures to avoid unnecessary damage to the structure, which often can occur with sign installation.

Window and Door Signs

- Simple lettering or motifs that are placed on storefront display windows, glass panels of entry doors, or upper floor windows.
- Traditionally, these were painted signs, but the same look may be achieved through the application of thin, vinyl appliques; another alternative is to hang a sign placed on clear glass or acrylic in the window or door.

Plaques

- Wall mounted plaques located near an entry or recessed vestibule; often used to direct patrons to upper level offices or businesses.

Wayfinding Signs

- Directional signs should be low, highly visible, and integrated with other graphic and design systems throughout the district. Directional signage for cars and people on the street should be consistent with any signage within the interior of a development.
- Locate signs to avoid blocking important views for pedestrians or drivers.
- Larger retail developments or complexes may include a single monument at public drive entries noting the names of businesses within the complex.
- Each building within a complex or development should have a legible address sign, visible both day and night. Numbers should be a minimum of 8" high.



Fig. 14 Reader board signs are adaptable and can be used in many different ways to display information and advertisements.

IV. Historical Building Forms and Architecture

4.1 Historic Architectural Styles of Lindon

Architectural styles and the details associated with them are considered secondary characteristics of a structure. Each building form has traditionally accommodated a range of architectural styles, and can also accommodate more contemporary styles of architecture. The architectural styles commonly represented in Lindon's historic structures are Classical, Victorian, and Bungalow, with most buildings using a vernacular version of these styles. Vernacular architecture is basically defined as the regional and local manifestations of a style. It is the architecture that most people build in a given time and place. Vernacular buildings utilize the basic conventions of a style, but are often scaled-down and simplified, and used on a smaller scaled structure. Excessive ornamentation, even in the Victorian-styled buildings, is not commonly seen in Lindon's vernacular architecture. Thus, simple detailing is encouraged for all building types and styles, with an emphasis on enhancing the structure, rather than trying to achieve a "period look" or style through the use of excessive ornamentation.



Fig. 1 Old central passage home

4.2 Lindon Design Theme

The basis of the following standards is respect for Lindon's historic building forms. Accordingly, the design of future development along State Street and 700 North should incorporate, as much as possible, these historic building forms. Craftsman and alpine style developments are similar to these historical buildings and are also acceptable. Individual buildings with smaller footprints better fit this historic theme than larger buildings and "big box" retailers. The historic feel of Lindon can be further enhanced through site design. Streetscapes should include sidewalks and street trees to create an attractive and safe environment for pedestrians. Locating most off-street parking on the sides and rear of buildings will help preserve the traditional, small-town feel of Lindon. As the Lindon City slogan "a little bit country" expresses, the community has a desire to preserve its rural heritage. The design of the State street and 700 North corridors should reinforce this desire.

Multiple options are given for recommended building forms to provide flexibility and variety in design and avoid the development of too many similar structures. Certain forms are more suited for smaller structures, while other forms may accommodate a wide range of building sizes. Care should be taken by developers and the city to work with a building form that is appropriate for the massing and scale of the proposed structure. Variations and adaptations of these basic building forms is expected, although the approach of tacking on different elements to a boxy building to achieve a "period/historic look" should be avoided. Major variations should be considered on a case-by-case basis.



Fig. 2 Historic cross wing home now converted to a commercial use.

4.3 Building Form / Type

The primary characteristic of a structure is the basic building form or type. Most of Lindon's historic structures are residential buildings, with some currently being re-used in a commercial capacity. Thus, the recommended building forms in these standards, based on those commonly found in Lindon, include both commercial and residential types. Listed below are the general characteristics of these building forms.

Building type refers to the basic form or shape of a building, and is not always associated with a particular architectural style, though some types are more commonly seen in some styles of architecture than others, especially residential buildings. Some building forms are residential in nature, yet may be appropriately adapted for commercial structures. Listed are those most commonly seen in Lindon or are variations of those seen.

4.4 Preferred Building Forms

There are three primary styles of historical homes built in Lindon City. They include Central Passage /Hall-Parlor, Cross Wing/ Gabled Ell, and Bungalow. All three forms are characterized by

pitched roofs. These building forms are preferred over flat roofed "block" building forms.

A. A hall-parlor or central passage structure was most commonly used in a residential manner, but may be successfully used for smaller commercial or office structures.

As symmetry is an essential component of this form, it is not recommended for uses that may require a covered drive-through area.

B. The cross wing or gabled ell replaced the hall-parlor as most common house type in the years after 1880. This form was most commonly used in a residential manner, but is also seen historically in institutional or public uses and is similar to the central block form in some ways. It may be successfully used for commercial or office structures in a range of sizes. Its asymmetrical form is a departure from the hall parlor/central passage form, and was often seen in association with Victorian styles, which embraced asymmetry and the enhancement of irregular massing forms.

C. The bungalow was the most popular house type in the first quarter of the 20th century. It was also used for smaller civic buildings, such as schools, libraries, city halls, and for small churches. It can be well adapted for use in smaller commercial structures, especially those that are situated near residential areas. Some bungalows often had a porte cochere attached to the side of the house, which would be an ideal form for businesses, such as banks, that require a drive-through area.

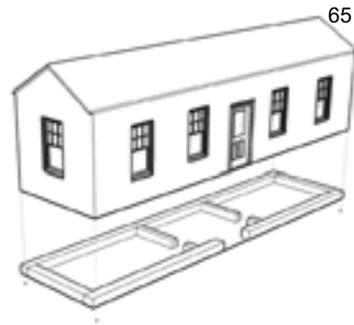


Fig. 3 Hall Parlor or central passage.



Fig. 4 Cross wing or gabled ell



Fig. 5 Bungalow



Fig. 6 Historic Lindon home (gabled ell)

4.5 Additional Building Forms (Block forms)

Commercial - Block Forms

Some of the historical commercial buildings in Lindon are One-part Block (Cullimore Mercantile; Walker's Service Station) and Central Block with Wings (Old South School).

These buildings are flexible in that they can have a variable number of bays or wings, large plate-glass display windows and are easy to build and replicate. While Lindon City would consider such designs, modern architectural improvements would need to be made to dress up the structure.

A. The one-part commercial block is suitable for a wide range of commercial uses, and can also house multiple businesses. One-part commercial blocks are an attached or freestanding, single street-level structure that is a simple box or rectangular plan. The defining feature of the one-part commercial block is the storefront.

B. Similar to the one-part block, the two-part block can be utilized by a wide range of uses. Two-part commercial blocks are an attached or freestanding structure of a simple box or rectangular plan separated into two distinct zones: the street level and upper. The street-level zone is for public uses, such as retail, while the upper stories are for more private uses such as residential, office, or meeting areas. This type of building form is ideal for incorporating offices or residential above one or more businesses on the street level.

C. A central block with wings is more commonly used for banks or public and institutional buildings, but may be used for commercial and office structures. As symmetry is an essential component of this form, it is not recommended for uses that may require a covered drive-through area.



Fig. 7 One-part block

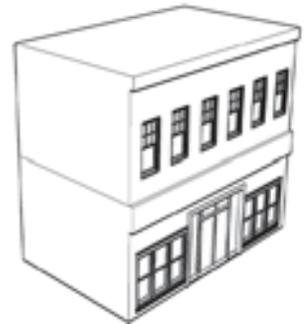


Fig. 8 Two-part block



Fig. 9 Central block with wings

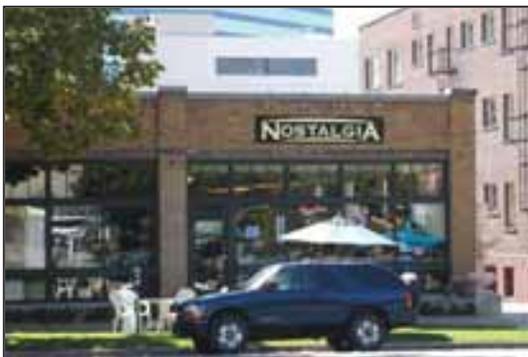


Fig. 10 One-part Block



Fig. 11 Two-part block

V. Architectural Character

5.1 General Intent / Introduction

The character of Lindon City should be positively conveyed through the appropriate use of massing, form, and materials in new commercial structures. In this chapter, general standards for all development are presented first, followed by standards specific for each recommended building form.

New commercial development should be sensitive and complementary to the heritage of Lindon City yet be balanced with present objectives to encourage development diversity and establish a vibrant commercial area.

The architectural standards are designed to promote development that is compatible and complementary to the historic built environment of Lindon. However, direct imitation of historic architectural styles and specific details is not recommended, but historical design principles should be incorporated into the design.

Rhythms and proportions of existing buildings should be identified and incorporated into new construction. These include such things as window to wall or solid-void ratio, bay division, proportion of openings, entrance and porch projections, and site coverage.

Exterior surfaces should be compatible with those of historic structures or the collective character of Lindon in regard to scale, type, size, finish, texture, and color. Finishes should complement the existing scheme of Lindon's historic structures. Roof form and style should be similar to or replicate those found in historic buildings and be appropriate for the selected building form.

Contemporary design and architectural expression that follows the basic principles of the standards is appropriate. The standards are not meant to preclude making exception in the case of innovative design.

5.2 General Standards

5.2.1 Massing and Orientation



Fig. 1 Massing of a building can be broken up by the variation of depth, texture, and color.



Fig. 2 Pitched roofs and rustic architectural features are preferred design characteristics that are consistent with the historic character of Lindon.

- Utilizing appropriate massing and orientation can allow new development to complement the heritage of Lindon. New structures should use massing and orientation similar to that of historic structures. Building placement and orientation should also reinforce the connection to primary and secondary streets, contributing in a positive manner to the streetscape of the commercial area.



Fig. 3 Even very large buildings can be broken up into smaller sections and avoid large blank walls.

- Small, individual developments are preferred. Several small developments contribute a greater degree of diversity than a few large developments.
- Where large buildings are unavoidable, they should be located at the rear of a development parcel or staggered with adjacent developments, with smaller individual developments along the street to preserve a consistent streetfront.
- Breaking up large buildings with multiple bays is required, and each façade should provide a meaningful purpose such as individual entrances to the larger building. On large buildings the façade should be broken up every 30' to 40' with color, change of building materials, depth, height, or other architectural characteristics. On smaller buildings, the break in façade should be every 15' to 25'. Appropriate detailing, scale, and proportion area elements that can be addressed through facade design.
- Orient buildings to the main street, either parallel to the street or at a maximum angle of 45 degrees. If a building is on a corner lot, it may have a corner orientation. This is not to preclude entrances or façade detailing to other orientations, such as a side parking lot.
- The perceived width of buildings should be consistent with smaller developments. Divide wider buildings into modules to convey a sense of more traditional construction, yet remain true to the interior layout/programming of the building. This is especially recommended for a series of adjacent businesses built in one development.



Fig. 4 Artistic design & architectural features can decrease the impact of 'big-box' size and massing.

- Use courts and atria to help vary the mass of buildings with large floor plates and introduce natural light to the interior.
- Differentiate between the ground floor and upper floors by providing for depth and variation in a façade through the use of different colors, materials, and other details.

- Building articulation is important to visually engage the populace. This can be accomplished through façade modulation, use of engaged columns or other expressions of the structural system, horizontal and vertical divisions through differing textures and materials. Variation in rooflines is also appropriate through the use of dormer windows, overhangs, arches, stepped roofs, gables or similar devices.



Fig. 5 Color changes & variation in material and depth are good tools to break up the massing of a building.

- Avoid flat looking walls/facades and large, boxy buildings. Break up the flat front effect by introducing projecting elements such as wings, porticos, bay windows, trellises, pergolas, port-cocheres, awnings, recessed balconies and/or alcoves, cornices, or other offsets, changes in plane, and changes in height. Staggered bays will also contribute to a greater definition of a façade. Specific standards for different building forms are given in the recommended building forms section of this chapter.
- Berming at the edge of buildings in conjunction with landscaping can be used to reduce structure mass and height along street facades.
- Give the greatest consideration in terms of design emphasis and detailing to the street facing façade (or façades if a corner site). Clusters of buildings in a single planned development may utilize common or compatible building forms and/or architectural styles, with a secondary emphasis on the internal relationships of buildings around a shared parking facility, interior court, landscaped yard, or plaza.
- Buildings on corner sites shall orient to both streets. These buildings are encouraged to have an entrance situated at or near the corner.
- Use sculpture, fountains, monuments, and landscape to enhance the three-dimensional quality of outdoor spaces.

5.2.2 Height

Building heights shall comply with the limits as established in the city code for the underlying zone. Building heights of one to three stories are considered desirable and appropriate to the scale of Lindon City.

5.2.3 Mixed Use Housing

Since Lindon strives to create an attractive & vibrant shopping district along the 700 North corridor, maintaining commercial uses on the ground floor is essential. The second and third floors of commercial buildings can be used for office space, retail and in some cases housing (if specific amenities are provided). Second and third story housing may be feasible along the 700 North corridor if additional public pedestrian amenities are provided such as open space, pocket parks, plazas, sitting areas, extra landscaping, fountains, etc. A starting recommendation is that for every two square feet of amenities that provide a public benefit, one square foot of livable housing may be added on upper floors. This housing must be utilized as part of a mixed use

development, therefore street level commercial elements are required to be part of the project. The Planning commission & City Council must approve any mixed use developments.



Fig. 6 Mixed used developments can be architecturally pleasing, satisfy housing needs, and also create vibrant shopping districts.

5.2.4 Exterior Walls and Surfaces: Building Materials

- Materials for exterior walls and surfaces should be selected based on durability, appearance, timelessness as well as compatibility with those used for the historic structures found in Lindon.
- Several notable historic buildings in Lindon are constructed of a locally quarried honeycomb limestone. The limestone was often transported then cut on the construction site, into large blocks. Most of the other notable historic buildings in Lindon are constructed of brick. To complement and be compatible with the character of Lindon, masonry building materials, such as brick, stone, and colored decorative concrete block, are highly preferred for use as the primary building material (85% or greater) of commercial development. Fenestration can also be used to count toward the 85% of the recommended building materials. Many varieties and colors of brick or stone are available and acceptable for use. While use of the same historic honeycomb limestone is not feasible, other masonry materials, such as decorative concrete block and other types of stone may be formed and used in a manner similar to the limestone. Other materials may be considered for use as a primary building material, based on review by the city.



Fig. 7 Emphasis on the detailing of the street-facing façade creates a pleasing experience for the pedestrian as well as the overall character of a commercial district.

- Secondary building materials may include brick, stone, colored decorative concrete block, stucco, wood/cement fiber siding & timbers. These materials are highly desirable over metals, plastics, vinyl, and faux siding materials including synthetic stucco-type materials.



Fig 8 Wainscotting is encouraged to break up the façade and inhibit a color or material from dominating the building façade.

- Scale, texture, detailing, and fenestration should be greatest at the ground floor, where the level of visibility and adjacency to pedestrian activity is greatest.
- Use materials in a manner that is consistent and visually true to the nature of the building material. (See Appendix A for additional materials standards.)
- Use primary building materials for facades that front onto public ways. Secondary building materials may be used as accents on these facades or on less visible facades.
- Use natural building breaks (such as inside corners) for changes in materials, rather than abrupt changes or changes at outside corners to avoid the appliqué look of a material.
- Avoid the use of synthetic materials.
- Innovative use of other materials may be considered.
- Consider durability and life cycle in the selection of materials.



Fig. 9 Secondary building materials (such as the timbers over this entryway) are encouraged as accents to a primary building design.

5.2.5 Texture, Colors, Finishes

- Design elements such as color and materials should reinforce the scale and character of the Commercial District and the heritage of Lindon. Avoid large areas of the same color and/or materials with no relief. Conversely, avoid the use of too many materials and/or colors, which may create busy or incongruous facades. Use materials that have a modular pattern closest to pedestrian ways to add scale, texture and visual interest.
- Earth tones are generally preferred over harsh or



Fig. 10 Individual businesses can share a larger building but stand out by changing the color or material of the facade.

loud colors, except where more vibrant colors are used to create a special effect that is harmonious with the adjacent context. *A color palette of Utah earth tones has been provided in the appendix for use as a reference guide to color selections in developments.*

- The use of color schemes should be compatible with the surrounding areas.
- Simplicity is encouraged regarding color. Excessive amounts of different colors should not be used. Brighter colors are recommended for use as accents only.
- The texture and finish of a structure should convey a modern, yet timeless, building.
- Vary colors and materials to break up the monotony in larger developments.



Fig. 11 Arches, sills, trim, muntins, and other architectural features can enhance windows and entryways..

5.2.6 Windows and Doors/Fenestration

- Windows and doors make important contributions to the appearance of any building and should be of a similar design and style to the general character of Lindon's historic buildings.
- Building entries shall have one or more of the following treatments: canopy, overhang, awning or arch; recesses or projections in the building façade including display windows surrounding the entrance; peaked roof or raised parapet structures over the door.
- Facades that front on to public ways should contain functional windows and doors, with a balance of solids and voids.
- Windows at the ground level should generally be of clear glass, and placed at a height that relates visual connection of indoor and outdoor environments.
- Avoid blank facades with no fenestration.
- Avoid the use of dark-tinted or reflective glass windows. Where possible, awnings, balconies, eaves, arbors, landscaping, and other shading devices are effective, and can be far more visually interesting.
- Materials for framing windows shall be compatible to the primary exterior material. Aluminum or similar framing materials that do not match are discouraged.
- Consider the use of canopies or awnings on windows that directly abut pedestrian walkways to provide protection from the elements.
- Sun and glare can be controlled with awnings, canopies, balconies, trellises, foliage, and other shading devices that also protect pedestrians from inclement weather.



Fig. 12 Architectural characteristics that do not blend in, such as these 'pasted on timbers', should be avoided.



Fig. 13 Covered entrances or porches create a more attractive façade.

- The ground floor of the primary façade shall be 60% fenestration at the pedestrian level.
- A significant amount of the primary ground story façade facing public streets, easements and other right-of-way corridors should be transparent glazing, to enhance the pedestrian environment, to connect the building interior to the outside, and to provide ambient lighting at night.
- Dark and obscure glazing should not be used at the ground level, except where harsh solar conditions cannot be controlled with other devices.

5.2.7 Architectural Styles: Exterior Trim and Decorative Detailing

While building form is the primary identifying characteristic of a structure, architectural style, represented by the use of exterior trim and detailing, is a secondary characteristic. Different architectural styles can be used on the same basic building form.

Many of the historic structures are of a vernacular architecture - smaller residences that use a scaled-down version of styles popular at the time. Thus, in general, most detail is simple in form and application, while still being attractive. This simplified approach to trim and detail should also be utilized for new construction.

- Use details and features that work well with the chosen primary and secondary building materials.
- Design details to be visually true and consistent with their materials of construction.
- The use of details can break up uninspiring solid surfaces and helps to avoid the box-like appearance often seen in new construction.
- Trim and details should be simple in material and design. A classic, timeless style should be used.
- Materials for trim and details shall be compatible with the primary exterior material. Detailing should be authentic with the characteristics and capabilities of the materials.
- Excessive ornamentation is not recommended.
- The use of details such as timbers, stones, and beams should be considered.
- Avoid use of pasted on details that do not reflect internal pattern of building or are not proper use of materials (see figure 12 on pg 27).
- Avoid façade appliquéés as a method to modulate the façade. Exterior materials, massing, modulation, etc., should relate to the indoor function and use of the structure.



Fig. 14 Variation in rooflines, slope, and plane provide character to a commercial structure.

5.2.8 Roofing

Roofing is a significant design feature. The form, height, color, pattern, materials, configuration and massing of the roof contribute to the success of a structure. Roof mass and form should be consistent with the scale and proportions of the building as well as the architectural character (see Fig 14 above). Use roof materials and patterns that are appropriate to the overall character and form of the building.

- Use no more than two roof types in a single structure i.e. a primary and secondary roof type.
- Variations in rooflines through dormer windows, overhangs, arches, stepped roofs, gables or other similar devices promote visual interest.
- Parapets require cornice treatments.
- Roof materials visible from the street (i.e. sloped roofs), should be harmonious in texture, color, and material with other building materials.
- Sloped roofs should be carefully designed to shed snow away from all pedestrian ways.

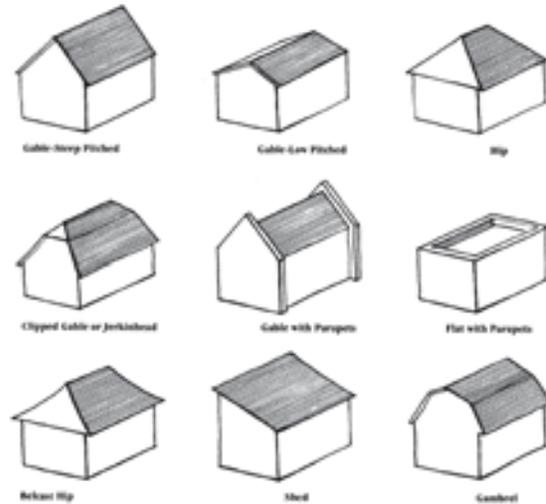


Fig. 15 Variety of roofing styles

5.2.9 Mechanical and Service Areas

Mechanical, electrical, and communications equipment such as heating and cooling units, transformers, control boxes, and antennas should not be located on primary facades.

- Rooftop mechanical units are desirable where possible, and should be screened from view with integrated architectural elements (walls, parapets, etc.).
- Ground mechanical units must be screened from view with wing wall, landscaping, or a combination of both.
- Appropriate vegetative buffers shall be placed to screen and buffer all utility boxes and pedestals.
- Meters, stacks, and service pipes should be located conveniently for service and use, but not on primary façades.
- Loading docks should be located near parking facilities, in alley ways or on side streets, and designed or screened in a way that minimizes their visual impact.

5.3 Specific Standards for Recommended Building Forms

The following recommended building forms for new commercial structures in Lindon are based on those common and/or similar to historic structures in Lindon, as depicted in the introduction. Some building forms have a residential basis, yet may be appropriately adapted for commercial structures.

For each building form, standards are given for the following elements:

1. Massing and Form
2. Height and Scale/Size

3. Roofing
4. Exterior Walls and Surfaces (Building Materials)
5. Fenestration (Windows and Doors)
6. Exterior Trim and Decorative Detailing

5.3.1 Building standards: Massing & Form, Height & Scale, Roofing, Exterior Walls & Surfaces, Fenestration, and Exterior Trim & Detailing.

1. Massing and Form

A. One and two-part Commercial Block

- Large plate-glass display windows shall be used to distinguish the front façade or storefront.
- The number of bays can range from one to five when building a One-part Commercial Block (Cullimore Mercantile; Walker's Service Station).
- The façade need not be symmetrical, although bays should be of the same or similar widths.
- If the structure is used for a business requiring a drive-through area (such as a bank or restaurant), use an extension of the roofline detail and supportive elements on the facade to encompass a covered drive-through area that is consistent with the building.
- A linear series of adjacent businesses may be incorporated into one block, utilizing separate bays for each business. Or, a series of adjacent blocks may be used more successfully if the size of the development would exceed five bays in width.

B. Central block with wings (Old South School)

- Use a symmetrical composition of a central block with identical wings.
- The central block will project from the wings and should be accentuated by size and/or height, as well as decoration.
- The wings will generally be lower and recessed from the central block.

C. Hall-Parlor/ Central Passage

- Use a symmetrical building composition, with the long side of the building being the primary façade.
- May be either three or five bays across on the front façade.
- Depth of the building should be one or two rooms deep.



Fig. 16 Two-part commercial block building.



Fig. 17 Gabled EII with historic western theme and cupola.



Fig. 18 Bungalow with a covered entryway.

- A covered front porch may be used.

D. Cross Wing/ Gabled Ell

- The form will have two or more wings placed at right angles to each other.
- The basic building may take the form of a “T”, “L”, or “H”
- The form/shape could be repeated or mirrored for larger structures or a connected series of stores.

E. Bungalow

- Use a square or rectangular floor plan.
- Use the form for the entire structure, rather than just a bungalow entrance on the front of a block building.
- A variety of form types may be used to create a ‘small gabled cottage’ style of building.
- Narrow end to the street with a hip or gable roof.
- Broad gabled roof that projects out over a front porch, usually with a top half story that has a centrally placed gabled or hip dormer.

2. Height and Scale/Size

A. One and two-part Commercial Block

- Scale and Size may vary from a small building (such as the Cullimore Mercantile) to larger structures.
- Bays should generally range from 15' - 25' in width for small buildings; large buildings 30' - 40'.
- For corner buildings, articulation of the corner with additional height may be considered.
- Scale and Size may vary from a small two-story building of one or two bays to larger structures that would encompass up to five bays.



Fig. 19 Bays & entryways break up this façade.

B. Central block with wings

- Buildings may be one to three stories in height.
- The height of the central block should be higher than that of the wings.

C. Hall-Parlor/ Central Passage

- Buildings may be one, one and a half, or two stories in height.
- This building type is best suited for smaller buildings (a footprint of less than 6,250 square feet)



Fig. 20 A simple building with appropriate features can create a unique appearance.

D. Cross Wing/ Gabled Ell

- Buildings may be one, one and a half, or two stories in height.
- A range of sizes may be accommodated with this form by utilizing additional sections of the projecting and flanking wing form.

E. Bungalow

- Buildings may be one, one and a half, or two stories in height.
- This building type is recommended for smaller buildings (a footprint of less than 6,250 square feet)

3. Roofing

A. One and two-part Commercial Block

- Sloped roofs should be the primary roof form and should use a material that is compatible in material and color with the exterior material of the building and any flat roof material. Wood or faux wood shingles, or architectural asphalt shingles are preferred.
- The flat roof can be used, but should always be used with a parapet and/or decorative cornice. Secondary roof forms, such as gabled, hip, or shed roofs may be considered for use to break up larger structures or for use on the corner of buildings situated on a corner.
- A wide range of roofing materials can be used for the flat roofs.



Fig. 21 Possible variation of rooflines

B. Central block with wings

- The entire structure may have flat roof with parapet or cornice detailing or the central block may be gabled with a flat roof used on the wings. For a structure that has a gabled center block and gabled wings, refer to the cross wing form in the residentially influenced building form section.

C. Hall-Parlor/ Central Passage

- A steeply pitched gable roof (8:12 to 14:12) is the preferred roof form. A hip roof may also be used.
- If a building is more than one story and dormers are used, the roof of the dormer should be of the same form as the primary roof.

D. Cross Wing/ Gabled Ell

- A steeply pitched gable roof (8:12 to 14:12) or a hip roof are the preferred roof forms.
- All sections of the roof should have the same height for the peak.

- If a building is more than one story and dormers are used, the roof of the dormer should be of the same form as the primary roof.

E. Bungalow

- Use a low-pitched gable or hip roof that projects out over the eaves.
- Dormers, if used, may have gable, hip, or shed roofs all work well with either primary roof form.

4. Exterior Walls and Surfaces (Building Materials)

- Brick, Stone, or Colored Decorative Block should be utilized as the primary building material (85% or greater of the building), especially on street-facing facades (Refer to Appendix 6.2).
- If using Stone or Decorative Concrete Block, details such as the texture of the block and the mortar joints should be similar to that of the historic structures of Lindon (Refer to Appendix 6.2).
- All of the above, as well as Cement Stucco, Wood, Architectural Metals, colored or decorative concrete, and cement board siding may be used as secondary (less than 40%) building materials, and on less visible facades.
- Foundation ribbons may be created from a material complementary to the primary building material.



Fig. 22 Stucco is common but other materials should be used to enhance the appearance.

5. Fenestration (Windows and Doors)

- If using muntins to create the look of paned glass, use an exterior application to create a visible shadow line, lending to an authentic look. Coordinate with an interior application of muntins.
- Avoid center pressed muntins, which lack a look of authenticity

A. One and two-part Commercial Block

- Large, transparent storefront windows are an essential component of the one-part commercial block.
- Storefront windows should be framed with a material complementary to the primary building material(s). Wood or metal are framing materials that work well with brick or stone.
- If storefront windows do not reach to the ground, a projecting sill should be used at the bottom.



Fig. 23 Large open windows add to the façade

- Transom windows should be used above storefront windows. These are often also transparent, but clear, decorative colored glass may be used to add detail to the building façade. Transoms may be either single or multi-paned.
- Awnings, if used, may be either metal or fabric. These should generally be mounted just above or below the transom windows when used on the storefront.
- Use of recessed entries that are flanked by the storefront display windows is encouraged, however flush entries may also be used (See fig. 23).
- Upper levels use more traditional windows with a vertical emphasis. These windows should be inset, with a sill and lintel, and may incorporate some simple detailing to add definition to the upper zone.
- If the building is free-standing, windows on the sides of the building should be vertical in orientation and proportional to the size of the building. These windows should be inset, with a sill and lintel. Simple window shapes should be used, although windows may be enhanced with details such as paned glass divided by muntins.
- Panes are also encouraged to add character to windows.



Fig. 24 Window trim and awnings add to the visual character of the fenestration.

B. Central block with wings

- Locate the main entrance to the building in central block section.
- Additional entrances are allowed in the wings.
- High, vertical windows should be used in the central block.
- The same style of windows used in the central block should be used in the wings, but usually of a smaller scale. These windows should continue around to the sides and potentially the back of the building, depending on the interior use of the structure.
- Windows should be inset, with a sill and lintel, and should incorporate some simple detailing in the molding or casing elements to add definition to the building.
- Simple window shapes should be used, although windows may be enhanced with details such as paned glass divided by muntins.



Fig. 25 Addition of muntins, color variations, trim, sills and arches create attractive windows.

C. Hall-Parlor/ Central Passage

- Windows should be vertical in orientation and proportional to the size of the building. Windows should be inset, with a sill and lintel.
- Simple window shapes should be used, although windows may be enhanced with details such as paned glass divided by muntins, or detailing in the lintel and/or molding.

- Molding or trim around the windows can be used to enhance a simple window shape.
- Entries may be accented with a covered porch area. Roofing of the entry porch should be of the same form as the main roof.



D. Cross Wing/ Gabled Ell

- Entrances should be located in side facing/flanking wing. These entrances may be protected by a porch or awning, which will add detail to the façade.
- Windows should be vertical in orientation and proportional to the size of the building. Windows should be inset, with a sill and lintel.
- Simple window shapes should be used, although windows may be enhanced with details such as paned glass divided by muntins.
- Molding or trim around the windows can be used to enhance a simple window shape.

E. Bungalow

- Windows should be vertical in orientation and proportional to the size of the building. Windows should be inset, with a sill and lintel.
- Simple window shapes should be used. Windows and doors may be enhanced with geometric patterns, created in stained or leaded glass, or by the use of wooden muntins. These are often seen only in the top half of windows and doors.

6. Exterior Trim and Decorative Detailing

- Some form of detailing or fenestration should be used every 15 to 25 feet along each side of small building (every 30'to 40' for large buildings). Windows, doors, art or architectural detailing at the first floor level are all options for a blank wall.
- Enhance buildings with usable details and accents, such as a covered porch or walkway.
- Avoid trying to incorporate multiple styles in one structure, instead use consistent, continuous detailing.
- Utilize colors, textures, and changes in building material to give definition to the façade.

A. One and two-part Commercial Block

- Use simple decorative detailing to enhance the features of building rather than using excessive decoration or pasted on details.



Fig. 26 These block form buildings have an attractive, modern, clean look achieved through detailing.

- Detailing should be focused on the primary, street-facing façade of the building.
- Utilize colors, textures, and changes in building material to give definition to a building's façade.
- Avoid trying to *excessively* break up a building's façade. Instead use consistent detailing along the façade.
- Two-part commercial blocks often incorporate more detailing than the one-part block. Simple, decorative detailing that evokes an architectural style should be used.
- Most detailing should be focused on the street-level.
- Upper level(s) feature less detailing than the street-level.

B. Central Block with Wings

- The central block portion should be further accentuated through use of detailing.
- Stylistic influences may be incorporated in both the central block and wings, with a greater emphasis on the central block.
- Detailing should be consistent on all sides of the building, although the primary, street-facing façade may have a greater emphasis of detail.



Fig. 27 This older two-part block building has been restored to maintain a modern appearance.

C. Hall-Parlor/ Central Passage

- Use simple detailing that highlights the structural elements of the building, such as the eaves, windows, and doors.
- Use simplified versions of historic elements seen in Lindon.

D. Cross Wing/ Gabled Ell

- Decoration and detailing should be consistent between the façade of the facing wing and the porch/front of the flanking wing
- Use simple detailing that highlights the structural elements of the building, such as the eaves, windows, and doors.

E. Bungalow

- Use sparse and simple detailing that highlights or exposes the structural elements of the building, such as the eaves, windows, and doors.
- Detailing should be more reserved in decoration and rely on the exposed elements, such as partially exposed framing members in the end of the roof, rather than adding on details.
- Use simplified versions of historic elements seen in Lindon bungalows.



Fig. 28 Gabled Ell with historic western theme.

VI. Appendices

I. Preference Lists: These architectural features are considered desirable and are suggested as “recurring themes” for buildings within the City.

- a. Cupolas
- b. Arched windows with muntins
- c. Exposed Timbers
- d. White two-rail fences along streets & walkways
 - i. Where white two-rail fencing is used, the following standards are encouraged: A continuous three (3) foot tall fence with post dimensions of five (5) inches by five (5) inches with rail dimensions of two (2) inches by six (6) inches. The posts should be installed eight (8) feet on center with two (2) rails between posts. The fence should be placed adjacent to any dedicated streets so as to generally appear in a continuous fashion. Placement of the fence should typically be two (2) feet behind the sidewalk within the required landscaping strip.
- e. Pitched roofing styles
- f. Stone wainscot and other stone or brick accents
- g. “Country Accents” in line with the Lindon theme, “A Little Bit of Country.”

II. Building Materials Standards

The use of details and features that work well with the chosen primary and secondary building materials are most effective. Design buildings such that details are visually true and consistent with their materials of construction. Provided are standards for:

- A. Brick
- B. Timbers
- C. Stone
- D. Stucco
- E. Siding/Cement fiber panels
- F. Concrete Block Masonry- split faced or decorative.
- G. Concrete Tilt Up Building
- H. Wood / cement fiber siding

A. Brick

Brick is a modular material and should be used in a manner that achieves a sense of permanence and quality.

- Dimensions of facades and openings should course out with brick modules where possible, to avoid small, cut pieces of brick.
- Trim with appropriate water table detail.



Fig 1. Brick is a classic and timeless building material.

- Brick should appear self-supporting and three-dimensional. Avoid wide spans (over 10') at openings.
- Use some form of header or lintel at all openings. These should be deeper for wider spans.
- Use inset windows, brick jamb returns, and projecting sills at windows. Recess windows.
- Avoid pieces of wall that are less than one brick wide between openings, or less than two bricks wide at a corner.
- Use the range of decorative patterns brick offers. Use combinations of soldiers, headers, stringers, etc. to form patterns that create cornices, wall caps, water tables, and other details. Use patterns in a manner consistent with the material.

B. Timbers

Wood beams or exposed logs can be used to add to the architectural appeal of the building. This type of treatment is often seen in Craftsman & Alpine style construction.

- The use of timbers helps to maintain the historical & western feel of the city.
- Used as decoration over porches and windows and other overhangs.



Fig. 2 Timbers dress up a building front and entrance and add to the character of Lindon.

C. Stone

Stone is a substantial material rooted in the land. Stone and stone panels must be used in a manner that appears self-supporting and three-dimensional in order to feel genuine.

- Avoid wide spans (over 10') at openings.
- Avoid narrow pieces of stone wall less than 1' wide between openings, and less than 2' wide at corners.
- Take care in the detailing and construction to create a believable corner.
- Keep mortar joints consistent in width to match apparent breaks between stones with breaks in modules.



Fig. 3 This entryway is oriented to the corner and is emphasized by the use of stone.

- Show some form of header or lintel at all openings. Wider spans should utilize deeper headers and lintels.
- Use inset windows, stone jamb returns, and projecting sill at windows.

D. Stucco

Stucco is traditionally a rough 'plaster type' finish coat over masonry walls. Use in a simple manner over large planar wall surfaces.



- Stucco turns corners without need of trim, so keep clean lines at the corners.
- Emphasize the material with broad overhangs, deep recesses at openings, and delicate details such as thin metal rails.
- Avoid narrow pieces of wall at corners or between windows.
- Create points of emphasis to provide contrast to stucco walls. Use slight changes in plane, changes in texture (walls vs. trim), or inset panels of contrasting finish and color (ceramic tile).
- Use appropriate scoring joints to create smaller panels that allow for natural expansion and contraction without unnecessary cracking. Joints should tie in with natural breaks or openings where cracks might naturally develop.

E. Wood Siding / Cement Fiber Panels

Siding is traditionally a lightweight material over a frame structure. If using a cement-based product, choose those with a genuine appearance and use an authentic manner when detailing.

- Avoid vinyl, plywood, or pressboard siding.
- Critical details, both visually and functionally, are joint sealings at corners, soffits, openings, and between siding pieces.
- Combinations of trim, fascia, subfascia, soffits, eaves and rakes protect the vulnerable joints in a building that is sided. Work to create a functional, unified, and harmonious family of these details.
- Ensure that the scale of details is appropriate to the function.
- Apply details consistently on all sides of the building.

- Stagger vertical joints in horizontal pieces of siding.
- Avoid small sections of siding between openings and at corners.
- Use a base to protect sided walls from the elements. Masonry bases are preferred.
- Provide an attractive and functional transition to the base.

F. Decorative Concrete Block Masonry

A modular material used in a similar manner to brick or cut stone. Many different textures and sizes are available.

- Dimensions of facades and openings should course out with the block modules where possible, to avoid small cut pieces of block.
- Use a stain or color finish for visible areas.
- Consider the use of patterns to enhance the building; create cornices, wall caps, water tables, and other details using patterns.
- Accent with detail blocks of different texture/finish.
- Use inset windows.
- Avoid pieces of wall that are less than one block wide between openings, or are less than two blocks wide at corners.
- Large sections of smooth faced, plain block in highly visible areas is not permitted.

G. Concrete tilt up buildings

A Tilt-up is a building constructed of concrete panels, commonly used for commercial or industrial facilities, such as; office buildings, warehouses, retail centers, manufacturing facilities, etc. Large concrete panels are typically poured on site and raised by a crane in place to form the exterior walls of the building.

- It is strongly recommended that stamped, stained, or textured panels be used to add character and appeal to the building. Plain, blank panels over large expanses of wall will not be permitted.
- Wainscot is also recommended to break of the façade.
- Use change of color or change of material to break up large walls of cement.
- *Plain cement panels without decorative features are not permitted.*
- Windows can also be used to add character and break up the wall area.

III. Glossary of Architectural terms

Readers of this book may find several descriptive architectural terms with which they are not familiar. This glossary of terms is provided to give simple definitions of words used throughout this text.

ADOBE - A large, unfired brick made of clay-based mud and straw binder, handpicked in a form and dried in the sun.

ARCADE - A range of arches supported by piers or columns. A passageway, of which one side is a range of arches supporting a roof.

ARCH - A structural element designed to support the weight above an opening. A true arch consists of wedge-shaped stones or bricks that make a curved bridge spanning an opening.

ASHLAR - Textured, rough-hewn stone; or the simulated appearance of rusticated stone in concrete blocks.

ASTRAGAL - A molding of half-round profile, especially the strip covering the joint between a pair of doors or casements.

BALCONET - A decorative balcony that is too small to stand on.

BALUSTRADE - A railing consisting of a handrail supported on balusters, often built on a base.

BALUSTERS - Lathe-turned or straight spindles that support a handrail as part of a balustrade.

BARGEBOARD - Ornamental trim board along the face of the incline of a roof gable.

BATTERED WALL - A wall that slopes inward as it rises; a tapering pier. Common on Pueblo walls and Bungalow porches.

BAY WINDOW - A window that projects from the outer wall, extending the floor space and creating an alcove in the interior space.

BELCAST ROOF - A roof slope with a convex profile creating a distinctive curve, associated with some Victorian and Bungalow styles.

BELT COURSE - A slightly raised horizontal band marking a division in wall surfaces.

BOARD-AND-BATTEN - Vertical plane siding with joints covered by narrow wood strips.

BRACKET - A supporting member, often L-shaped or triangular, for a projecting roof cave, balcony or shelf.

BROKEN SCROLL - A Colonial decorative motif placed over doors or windows featuring a central ornament flanked by interrupted gable moldings.

CANALE - A waterspout extending beyond the plane of an exterior wall or parapet.

CANTILEVER - Construction in which a beam or structure extends beyond the face of a wall, being supported only at the one end.

CASEMENT - A window with the sashes opening outward on vertical hinges.

CASING - Decorative trim encasing a window or door opening.

CHICAGO STYLE - With reference to windows, a symmetrical, flat-arched, tri-partite gang of windows with a large, fixed picture window in the center, flanked on both side by narrower, operable windows.

CLASSICAL ORDERS - In classical architecture, the design of a column and entablature relating to a specific style and time period, including: Doric, Ionic, Corinthian, Composite and Tuscan.

COPING - The sloped capping or top course of a wall made of stone, metal, wood, or some other material for the purpose of protecting the wall from weather.

CORBEL - A projection of successive level of masonry beyond the wall surface producing a bracket form.

CORNICE - The projecting member at the top of a wall or roof trim.

COLUMN - A vertical round shaft that supports, or appears to support, a load.

CREEPING DAMP - (Sometimes called rising damp) The vertical movement of water through a substance by capillary action. Common on lower levels of masonry buildings

CROSS WING - A house form involving two intersecting rectilinear shapes, one recessed.

CROWN MOLDING - A curved molding used to terminate the trim on cornices, walls, casings and cabinets.

CUPOLA – A small structure built on top of a roof & used as architectural treatment or observation post.

CURVILINEAR PARAPET - The multiple-curving, ornamental motif on the center of the top of a parapet wall, especially in Mission Style architecture.

DEAD LOAD - The uniform, fixed weight inherent in any structure (as opposed to LIVE LOAD).

DECKING - The material used to cover the floor of a porch, balcony or other flat exterior walking surface.

DENTILS - A classical ornamental molding consisting of a horizontal series of block-like projections thought to have been based on the appearance of rows of teeth.

DORIC - The simplest of the classical orders.

DORMER - A projecting gable in a pitched roof with a window or windows on its front vertical side.

DOUBLE HUNG - A window in which both the upper and lower sash are independently operable in vertical movement within the same frame.

EAVE - The edge of a roof that projects over the outside wall.

ELEVATION - A "head-on" drawing of face of a building or object, without any allowance for the effect of the laws of perspective.

ENTABLATURE - In classical architecture, the horizontal member immediately above the columns consisting of the architrave, the frieze and the cornice.

FACADE - The front or principal face of building: any side of a building that faces street or other open space.

FANLIGHT - A semi-elliptical or semicircular window, usually over a door.

FASCIA - A flat board with a vertical face that forms the trim along the edge of a flat roof or along the eaves of a pitched roof.

FEDERAL - A classical American architectural style. Dating from 1780 to the mid-1800's.

FENESTRATION - The arrangement and design of windows in a building.

FINIAL - A terminal form at the top of a spire, gate-post, pinnacle, or other point of relative height.

FLASHING - Metal sheets at the junction of roofs and walls or chimneys used to prevent leaking.

FLUSH - Being even with or in the same plane or line as.

FLUTING - A decoration consisting of long, rounded grooves in columns or casings.

FOOTPRINT - A popular term for the shape of an area within the perimeter of a floor plan.

FRAME - The part of an encasement of an opening supporting a door or window. Also, a method of building construction employing a skeletal system of several repetitive structural components, as in wood-frame or steel-frame, or the work of constructing such a system.

FRONTISPIECE - A classical, ornamental projection, including windows, around a major door. Sometimes refers to a wing extending forward from the facade.

GABLE ROOF - A ridged roof forming a gable at each end. A roof with a single peak.

GABLE - The upper (usually triangular shaped) terminal part of a wall under the eave of a pitched roof.

GAMBREL ROOF - A roof with two slopes on each of two sides, the lower steeper than the upper.

GLAZING - Glass set in windows.

GREEK REVIVAL - A classical American architectural style, or individual components of that style, generally dating from 1820 to 1860, but also used during later decades in the west. Architectural style, or individual components of that style, generally dating from 1820 to 1860, but also used during later decades in the West.

HALF-TIMBER - A form of Medieval construction using exposed wood framing with the intervening spaces filled with stucco or masonry. Ornamental trim that reflects the internal structure.

HIGH STYLE - Common terminology for the most elaborate and formal versions of major architectural styles.

HIP ROOF - A roof with sloping ends and sides, usually with four sides terminating in a ridge or point.

HOOD - A protective, often ornamental cover over doors or windows.

HUE - Generally, color or a particular shade or tint of a given color.

IN-KIND - Matching the original material.

ITALIANATE - An architectural style from the mid-to-late nineteenth century which derived its designs and forms from mansions and villas of the Italian Renaissance.

JACOBEOAN - A seventeenth century English architectural style, revived in America in the early twentieth century, characterized by red brick Wells, and steep, coped, cast concrete gable trim.

JERKINHEAD - A gable roof with the ends of the gables clipped off to form small hips.

JOINERY - The hand-crafted intersecting joints in ornamental woodwork; associated with woodwork, eaves and cabinetry.

LACE WORK - Fine wooden or metal ornamental screens or scrollwork.

LANCET ARCH - A tall, thin, three-centered or pointed arch surrounding a window opening or vent.

LINTEL - A supporting beam placed over a door, window or other opening; usually visible and of a contrasting material from the wall surface.

LIVE LOAD - A moving or inconstant structural load or weight (such as people) that a building's structure carries in addition to its own weight.

LOGGIA - A covered second-story porch, typically cantilevered and framed by a balustrade. Square posts or turned columns usually support a shed roof.

MANSARD ROOF - A roof that slopes in two planes, the lower of which is usually steeper. Typical of the French Second Empire style.

MILL FINISH - The raw, unfinished color and texture of an aluminum or other metal product, such as a window or door frame, as it comes directly from the mill or factory.

MUNTIN - A small piece of wood or metal in a window sash holding in place and separating one piece of glass from another.

ONE-OVER-ONE (1/1) - A double-hung window with one pane of glass in the top sash and one pane in the bottom. 2/2 has two panes over two panes. A likewise 4/4, 6/6, 12/9 and other window patterns.

ORIEL - A projecting corner window supported by brackets.

OXIDATION - In rusting or burning, the chemical union of a substance with oxygen.

PALLADIAN WINDOW - A tri-partite window consisting of a large, central, round headed window flanked by two smaller, rectangular windows.

PARAPET - A low wall at the edge of a roof, porch, or terrace.

PATINA - A thin coating or color change resulting from natural oxidation during aging; for example, the changing of copper to a greenish-blue color over time.

PENDANT - A hanging ornament.

PERGOLA - An arbor or colonnade with columns or posts supporting open roof timber.

PIECE-IN - To add a piece or pieces matching the original in order to repair.

PILASTER - A pier or half-column of shallow depth applied to a wall.

PINNACLE - A terminal ornament or protecting cap, usually tapered upward to a point or knob and used as a high point of a roof.

PITCH - The degree of slope or inclination, as in the steepness of a roof.

PLASTER - A wall finish material, usually made of lime gypsum or cement, sand and water, applied in a plastic state with or without a heavy texture, to exterior or interior surfaces.

PLINTH BLOCK - A small, slightly projecting block at the bottom of the casing around a door opening.

PLUMB - The degree to which a wall is perfectly vertical.

POLYCHROMATIC - Featuring several colors, as opposed to monochromatic or one color.

PORTAL - A principal entrance, usually recessed and arched.

PORTE COCHÈRE - An open-walled but covered structure attached to the side of a building through which a carriage or automobile may pass or under which they may park. Also a roof and supporting projection over a driveway near the entrance to a house; later referred to as a carport.

PRESERVATION - The process of preserving the existing form, character and appearance of a structure through techniques designed to arrest or slow the deterioration of a structure, or to improve structural conditions.

PROJECTING BAY - Typically a three sided extension from the main facade of a building, containing windows and ornamental elements; sometimes called a "pent" or "slanting" bay or **BAY WINDOW**.

QUARREL - A small, diamond-shaped pane of glass, one of many in a window. Associated with English styles.

QUOINS - An ornamental element, usually of masonry, on the corners of buildings that expresses the structural interlocking of the corner.

RAISED PANEL - In wood millwork, a door, cabinet or furniture with beveled panels inset in flat wooden frames. Doors will usually have several raised panels, as opposed to slab or flat panel doors that may have only one panel per door.

RAFTER - A wooden frame member stretching from the ridge to the eave of the roof.

RENOVATION - The introduction of new elements to a building to replace old worn parts.

RESTORATION - To employ treatments aimed at returning a building to its original appearance and condition.

REHABILITATION - To take corrective measures to make a building usable or livable again.

RIDGE - The horizontal top line formed by the meeting of two sloping roof planes.

RIDGE CAP - The wood, tile or metal cap covering the ridge of a roof.

ROMAN ARCH - A semi-circular or "round" arch, invented by the Romans.

ROOF CRESTING - A decorative metal element placed along a ridgeline.

ROOF PITCH - The relative angle of the roof slope.

SASH - The movable frame holding glass in a window opening.

SCONCES - Decorative wall fixtures or lamps. Wrought iron sconces are common to the Spanish Colonial and Mediterranean Revival styles.

SCUPPER - An opening through a wall that allows for roof drainage. Term also refers to the metal funnel which catches runoff water and directs it into the downspout.

SECOND EMPIRE - An American architectural style from the mid-to-late nineteenth century, employing the Mansard roof and related elements from the reign of Napoleon.

SEGMENTAL ARCH - A gently curving arch having the shape of the uppermost segment of a circle.

SHAKE - A thick, wavy, rough, shingle made of wood, used in Ranch Era architecture.

SHED ROOF - A single sloped roof.

SHINGLE STYLE - A turn of the century American architectural style characterized by the use of shingles on most wall surfaces, often paired with Colonial Revival ornamentation.

SIDELIGHTS - Tall, narrow windows with small glass panes flanking a doorway, or picture window.

SOLID CORE - With reference to doors, a slab door made of solid wood rather than several panels with a hollow interior.

SPINDLEWORK - Delicate ornamentation of turned wood spindles, typically from the Victorian Era, often found on porches and as ornamentation for doorways.

SQUARE - Forming a right angle.

STUCCO - Plaster for exterior walls.

SURROUND - Ornamental trim or casing surrounding a door or window opening.

TERRA COTTA - Cast and fired clay units, usually larger and more intricate in form and detail than brick.

THATCH - A Medieval roofing material consisting of matted or woven straw; imitated by undulating wood shingles in Period Revival architecture.

THREE-POINTED ARCH - An elliptically shaped arch with its curve established by three perspective points beneath the arch.

TRANSOM - A window opening over a door.

TRI-PARTITE - Consisting of three similar, joined components, such as windows or ornament.

TRUNCATED - Having the top of a hip roof cut off by a flat plane.

TUDOR ARCH - An English arch which slopes gently upward to a point. Associated with English Revival styles.

TURRET - A small tower, sometimes corbelled from the corner of a building & extending above it.

VENTS - Ventilation openings, pipes or shafts.

VESTIBULE - A small entrance room or enclosure situated at an exterior entry to a building.

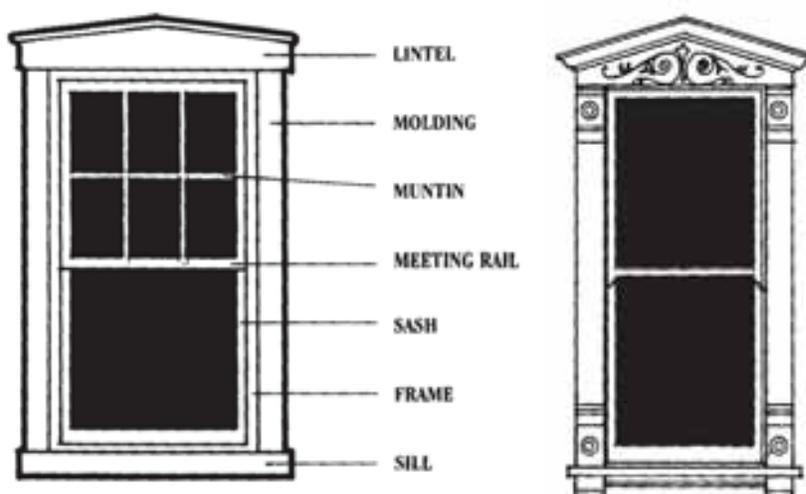
VIGA - A horizontal roof beam, usually a wood log exposed and extending beyond the plane of a wall or parapet.

VERANDA - A long, roofed, gallery-like arcade or porch that spans the width of a facade.

VERNACULAR - Indigenous architecture characteristic of a certain locale.

WAINSCOT - Wood, stone, brick or stucco paneling or some other decorative material that is applied to the lower section of a wall and may extend around the entire facade.

WINGWALL - A non-structural ornamental wall extending out to the side of a building.



IV. Utah Mountain Desert Color Palette



Utah Mountain Desert Color Palette

Primary Colors

A large beige rectangular area containing the text "Utah Mountain Desert Color Palette" and "Primary Colors". Below the text are seven rows of five color swatches each. The rows are: 1) light blue to cyan; 2) grey to light grey; 3) brown to tan; 4) dark green to medium green; 5) olive green to light green; 6) yellow to light yellow; 7) brown to light beige.

Lindon City Zoning Map



Lindon City Planning Zones

CF	Commercial Farm	PF-HSO	Public Facilities -- Hillside Overlay
CG	General Commercial	PRD	Planned Residential Development Overlay
CG-A	General Commercial A	R&B	Research and Business
CG-A8	General Commercial A8	R1-12	Residential Low Density
CG-S	General Commercial Storage	R1-12-H	Residential Low Density -- Hillside Overlay
HI	Heavy Industrial	R1-20	Residential Very Low Density
LI	Light Industrial	R3	Residential High Density
MC	Mixed Commercial	RMU-E	Recreational Mixed Use -- East
PC-1	Planned Commercial - 1	RMU-W	Recreational Mixed Use -- West
PC-2	Planned Commercial - 2	SHFO	Senior Housing Facility Overlay
PF	Public Facilities	SPOD	Utah Lake SPOD



9. Review & Action — Planning Commissioner Appointment*(5 minutes)*

The City Council will review and consider Mayor Acerson's recommendation to appoint Mike Vanchiere of 259 N 1090 E to serve as a new Planning Commissioner for Lindon City starting February 16, 2017.

See attached letter of appointment.

Lindon City
100 North State Street
Lindon, UT 84042-1808



TEL 801-785-7687
FAX 801-785-7645
www.lindoncity.org

Mike Vanchiere
259 North 1090 East
Lindon, Utah 84042

February 8, 2017

Mr. Vanchiere,

On February 7, 2017, the Lindon City Council approved the recommendation of Mayor Acerson to appoint you as a member of the Planning Commission for Lindon City. We hope to have you start participating at meetings beginning on February 16, 2017. It is anticipated that you will serve a full three-year term which will expire the last day of February 2020, or until your respective successor has been appointed.

Meetings are typically held on the 2nd and 4th Tuesday of each month, except for meetings that may fall on or near a holiday. A schedule of meetings for 2017 is attached.

We are excited to work with you and appreciate your willingness to serve the City of Lindon and represent the citizens in our community. As a new Planning Commissioner, I would like to meet with you to answer any questions you may have and to review your responsibilities. Please contact me at 801-785-7687 or by email at hvanwagenen@lindoncity.org to set up a quick meeting before your first Planning Commission meeting.

Sincerely,

Hugh Van Wagenen
Planning Director

Jeff Acerson
Mayor

10. Council Reports: (20 minutes)

- A) MAG, COG, UIA, Utah Lake, ULCT, NUVAS, IHC Outreach, Budget Committee- Jeff Acerson
- B) Public Works, Irrigation/water, City Buildings - Van Broderick
- C) Planning, BD of Adjustments, General Plan, Budget Committee - Matt Bean
- D) Parks & Recreation, Trails, Tree Board, Cemetery - Carolyn Lundberg
- E) Public Safety, Court, Lindon Days, Transfer Station/Solid Waste - Dustin Sweeten
- F) Admin., Community Center, Historic Comm., Chamber of Com., Budget Comm. - Jacob Hoyt

10. Administrator's Report: _____ *(10 minutes)*

Misc. Updates:

- February newsletter:
- March newsletter article: **Matt Bean** - Article due to Kathy last week in February.
- City Center Elevator remodel. Architect is preparing plans for bidding by early March.
- Public Safety Building – Open House Saturday February 25th at 11 am.
- Budget Kick-off meeting – February 9th at 6pm.
- Misc. Items:

Upcoming Meetings & Events:

- January 16th – City offices closed.

Adjourn